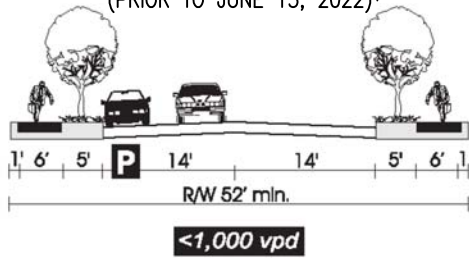
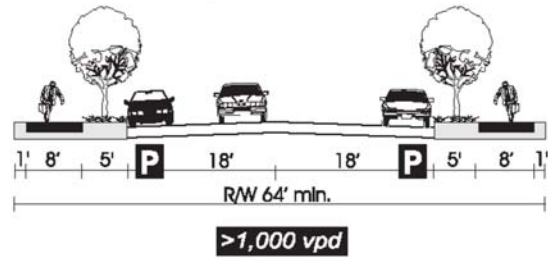


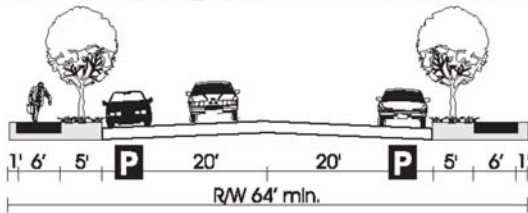
**28' Standard Residential\***  
(PRIOR TO JUNE 13, 2022)\*



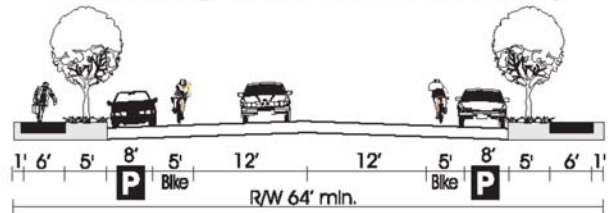
**36' Neighborhood Route**



**40' Standard Commercial/Industrial Not Exceeding 3,000 Vehicles Per Day**



**50' Standard Commercial/Industrial Exceeding 3,000 Vehicles Per Day**



**Local Street Design Characteristics**  
(typically minimums unless stated otherwise)

Characteristic	Neighborhoods	Locals	Comm/Ind
Vehicle Lane Widths (Bus Route - 11 ft.)	10 ft.	10 ft.	20 ft. *
On-Street Parking	8 ft.		
Sidewalks (minimums)	8 ft.	6 ft.	6 ft.
Landscape Strips	Required	Required	Required
Neighborhood Traffic Management (NTM)	Acceptable	Should Not be Necessary	Acceptable
Transit	Special Circumstances	Not Appropriate	Acceptable

\* Combined travel/parking lane.

**Legend**

**P** - On-street Parking Lane

**\*** - Parking can be provided on both sides if it can be demonstrated that curb cuts make up at least 40% of street frontage.

ORDINANCE 2022-005\*

**Figure 16D**  
**LOCAL/NEIGHBORHOOD STREETS**  
**SHERWOOD**  
**STREET CROSS SECTIONS**



STANDARD DRAWING TITLE

DRAWING NUMBER

LOCAL & NEIGHBORHOOD STREET SECTIONS

RD-1 A

Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.

SCALE

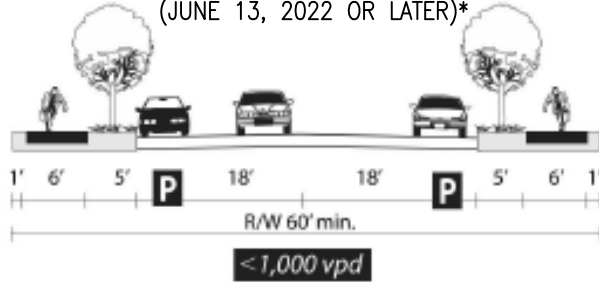
REVISION DATE

N.T.S.

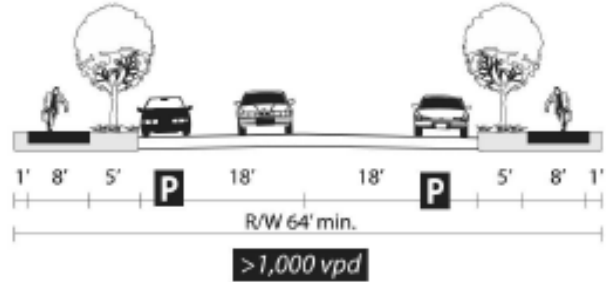
2023.03.17

### 36' Standard Residential

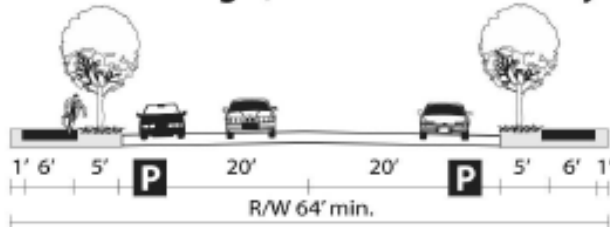
(JUNE 13, 2022 OR LATER)\*



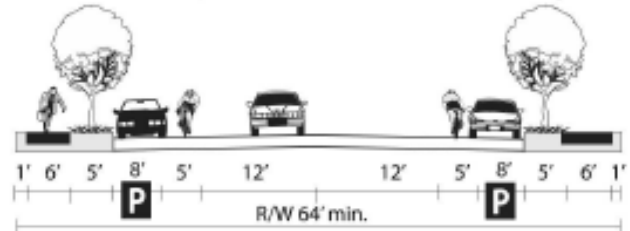
### 36' Neighborhood Route



### 40' Standard Commercial/Industrial Not Exceeding 3,000 Vehicles Per Day



### 50' Standard Commercial/Industrial Exceeding 3,000 Vehicles Per Day



#### Local Street Design Characteristics (typically minimums unless stated otherwise)

Characteristic	Neighborhoods	Locals	Comm/Ind
Vehicle Lane Widths (Bus Route - 11ft.)	10 ft.	10 ft.	20 ft.*
On-Street Parking	8 ft.		
Sidewalks (minimums)	8 ft.	6 ft.	6 ft.
Landscape Strips	Required	Required	Required
Neighborhood Traffic Management (NTM)	Acceptable	Should Not be Necessary	Acceptable
Transit	Special Circumstance	Not Appropriate	Acceptable

\* Combined travel/parking lane

#### Legend

**P** - On-Street Parking Lane

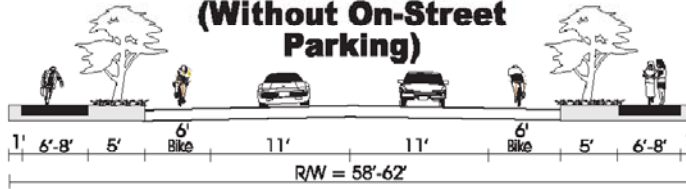
\* ORDINANCE 2022-05

**Figure 16D  
LOCAL/NEIGHBORHOOD STREETS  
SHERWOOD  
STREET CROSS SECTIONS**

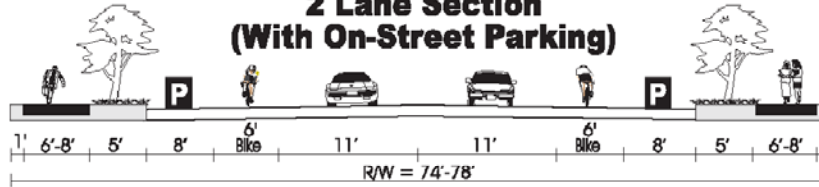


STANDARD DRAWING TITLE		DRAWING NUMBER
LOCAL & NEIGHBORHOOD STREET SECTIONS		RD-1 B
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	REVISION DATE
	N.T.S.	2023.03.17

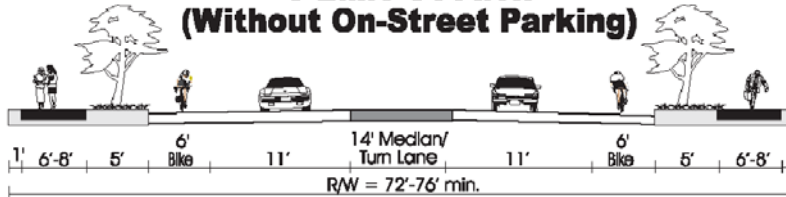
### 2 Lane Section (Without On-Street Parking)



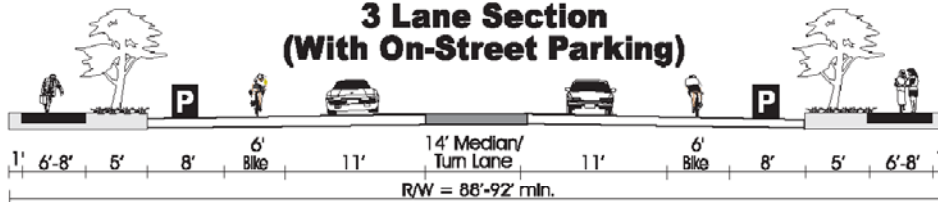
### 2 Lane Section (With On-Street Parking)



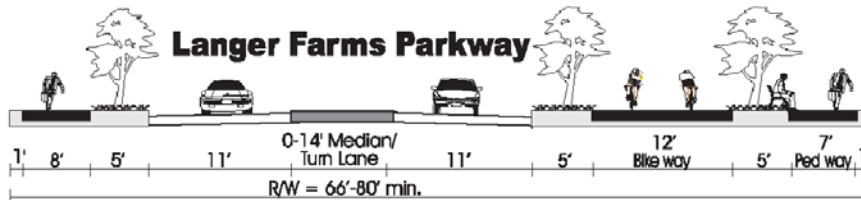
### 3 Lane Section (Without On-Street Parking)



### 3 Lane Section (With On-Street Parking)



### Langer Farms Parkway



A variance requires demonstration of hardship or other exceptional circumstances resulting from conditions of the property. Variances must meet Sherwood Development Code and TPR criteria.

**P** - On-street Parking Lane (except at intersections)

**Notes:**

- In constrained conditions on collectors a minimum width of 10 feet may be considered (i.e. for intersection turn lanes). 14-foot is desirable for continuous two-way left turn lanes.
- 8 feet for residential streets, 6 feet in commercial/industrial areas.
- Turn lane warrants should be reviewed using Highway Research Record No. 211, NCHRP Report No. 279 or other updated/superseding reference.

#### Collector Street Design Characteristics

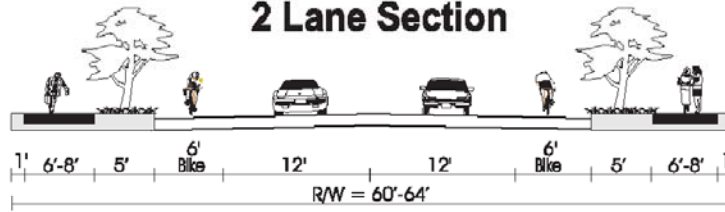
Characteristic	Collectors
Vehicle Lane Widths (Turn Lane - 12-14 ft.) *1	11 ft.
On-Street Parking	8 ft.-Optional
Bicycle Lanes (minimums)	6 ft.
Sidewalks (minimums)	6-8 ft. *2
Landscape Strips	Required
Raised Medians	Optional (Required where 3-lane section used)
Neighborhood Traffic Management (NTM)	Under Special Conditions
Transit	Appropriate
Turn Lanes	When Warranted *3
Access Control	See Later Discussion

**Figure 16C  
COLLECTOR STREETS  
SHERWOOD  
STREET CROSS SECTIONS**

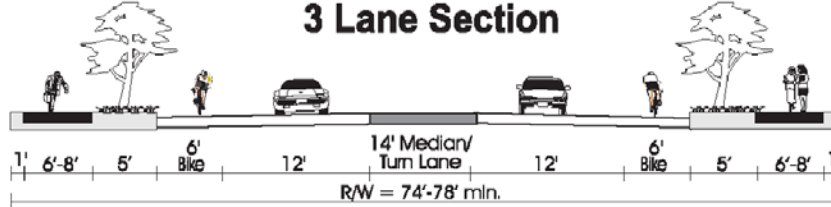


STANDARD DRAWING TITLE		DRAWING NUMBER
COLLECTOR STREET SECTIONS		RD-3
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

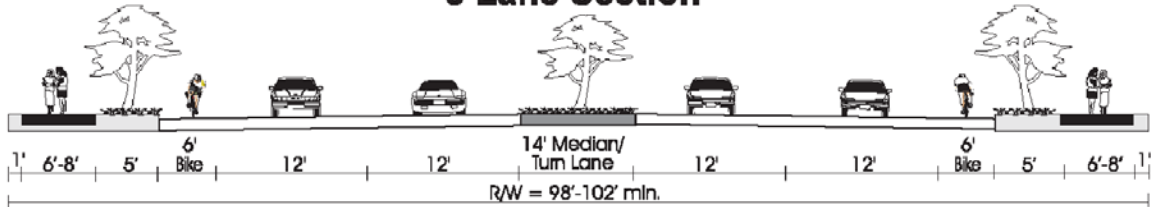
## 2 Lane Section



## 3 Lane Section

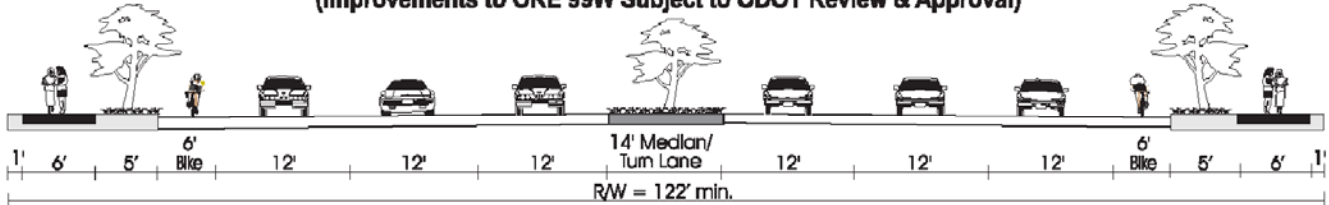


## 5 Lane Section



## 7 Lane Section

(Improvements to ORE 99W Subject to ODOT Review & Approval)



### Arterial Street Design Characteristics

A variance requires demonstration of hardship or other exceptional circumstances resulting from conditions of the property. Variances must meet Sherwood Development Code and TPR criteria.

Characteristic	Arterials
Vehicle Lane Widths (Turn Lane - 12-14 ft.)	12 ft.
On-Street Parking	Only in (i.e. downtown) (8 ft.)
Bicycle Lanes (minimums)	6 ft.
Sidewalks (minimums)	6-8 ft. *1
Landscape Strips	Required
Raised Medians	Required
Neighborhood Traffic Management (NTM)	Prohibited
Transit	Appropriate
Turn Lanes	When Warranted *2
Access Control	See Later Discussion

**Notes:**

- 8 feet for residential streets, 6 feet in commercial/industrial areas.
- Turn lane warrants should be reviewed using Highway Research Record No. 211, NCHRP Report No. 279 or other updated/superseding reference.

**Figure 16A  
ARTERIAL STREETS  
SHERWOOD  
STREET CROSS SECTIONS**



STANDARD DRAWING TITLE

DRAWING NUMBER

STANDARD ARTERIAL STREET SECTIONS

RD-4

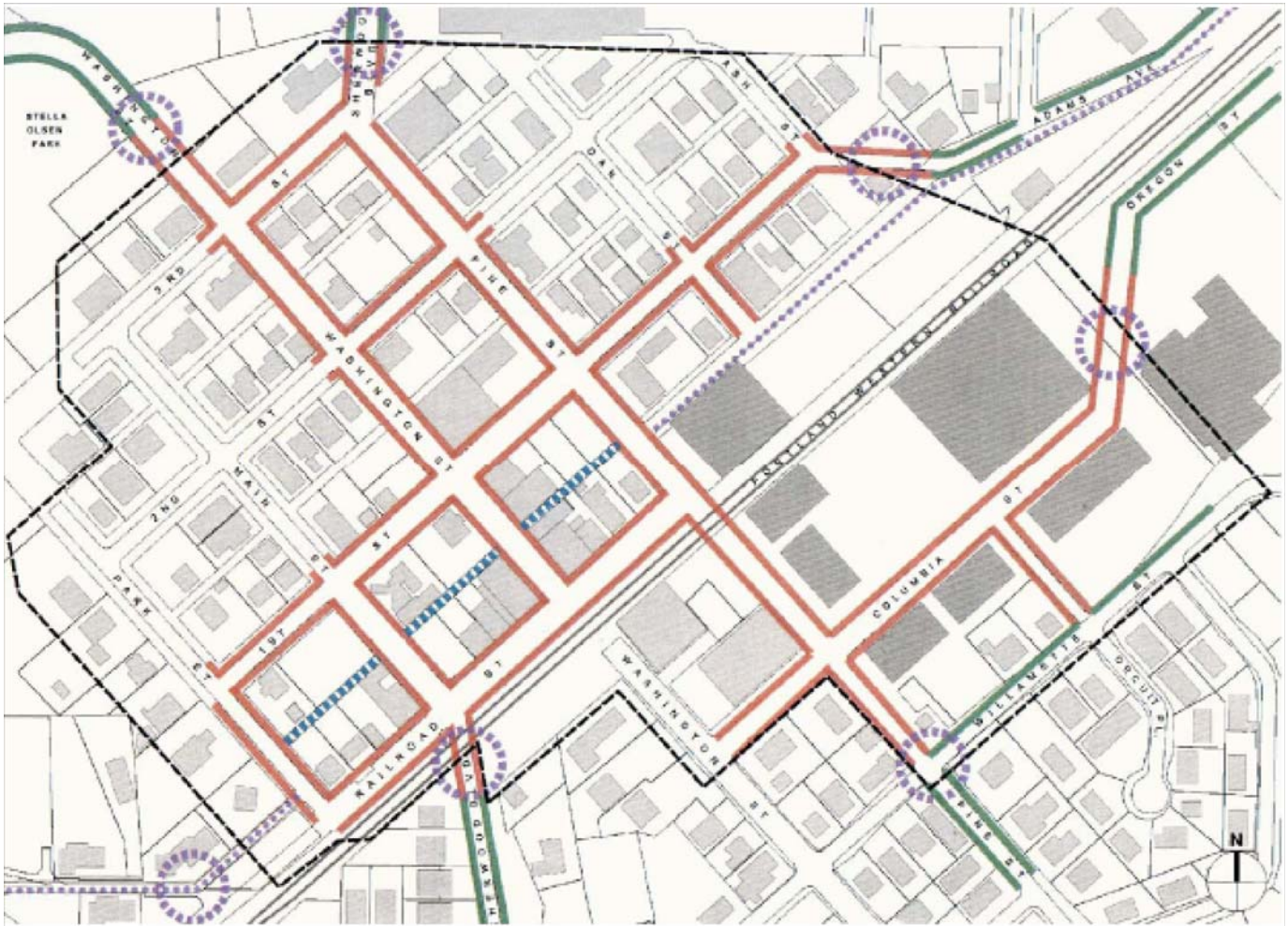
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.

SCALE

DATE

N.T.S.

MAR '16



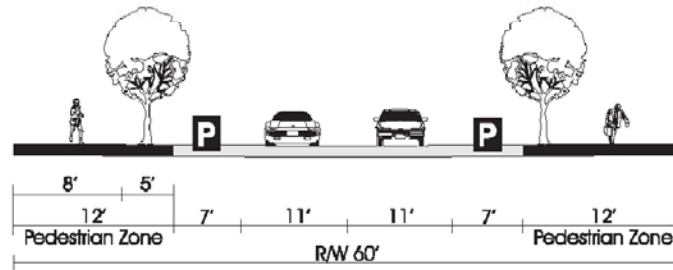
**Legend**

-  Concrete sidewalk w/planting strip
-  Concrete sidewalk adjacent to curb
-  Curbless street w/site furnishings
-  Pedestrian/bike route
-  Alley improvements
-  Gateway



STANDARD DRAWING TITLE		DRAWING NUMBER
DOWNTOWN STREETS STANDARDS DOWNTOWN OVERLAY DISTRICT		RD-6
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

## Pedestrian Streets\*\*



**\*\* No Curbs**

**Legend**

**P** - On-street Parking Lane (except at intersections)

**Figure 16E**  
**DOWNTOWN STREET STANDARDS**  
**RESIDENTIAL/PEDESTRIAN**



	STANDARD DRAWING TITLE	DRAWING NUMBER
	DOWNTOWN STREET STANDARDS PEDESTRIAN "WOONERF" STREET SECTION	RD-7
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

# DOWNTOWN STREET STANDARDS

## NOTES:


1. THE DOWNTOWN STREETS STANDARDS APPLY TO THE AREA IDENTIFIED IN STANDARD DRAWING RD-6.

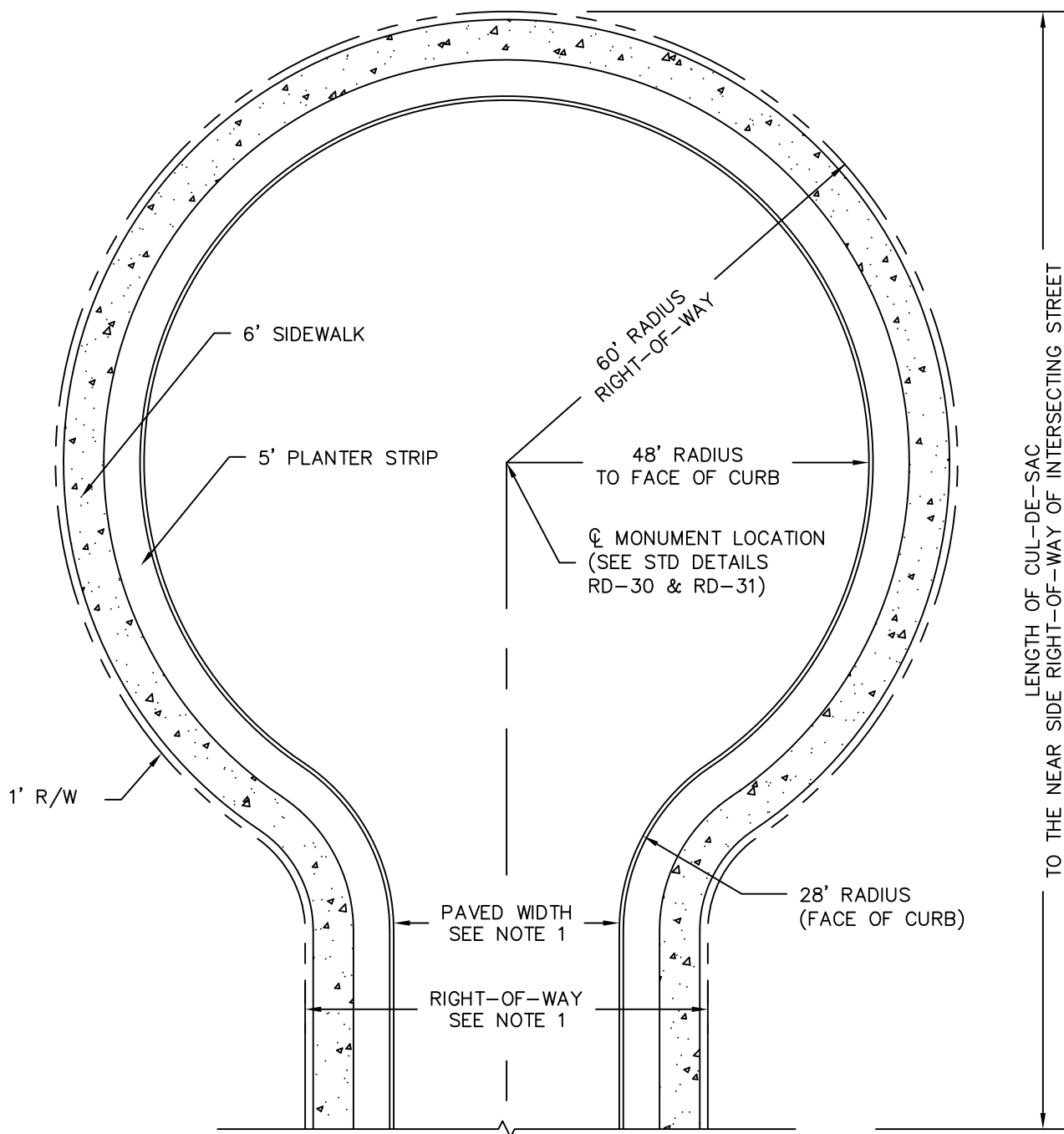
2. THE DOWNTOWN STREET DESIGN STANDARDS ARE CURRENTLY BEING REVISED TO INCORPORATE DETAILS FROM THE DOWNTOWN STREETScape IMPROVEMENTS PROJECT-PHASE A AND THE DOWNTOWN STREETScape MASTER PLAN. PLEASE CONTACT THE CITY OF SHERWOOD ENGINEERING DEPARTMENT FOR CURRENT STANDARDS AND DETAILS.

3. PLEASE REFERENCE THE DOWNTOWN STREETScape IMPROVEMENTS PROJECT-PHASE A CONSTRUCTION PLAN SET AND THE DOWNTOWN STREETScape MASTER PLAN FOR DESIGN DETAILS WITHIN THE DOWNTOWN OVERLAY DISTRICT. ALL DESIGN SHALL BE CONSISTENT WITH THE DESIGN AND MATERIALS AS COMPLETED IN PHASE A.

4. A COPY OF THE DOWNTOWN STREETScape IMPROVEMENTS PROJECT-PHASE A CONSTRUCTION PLAN SET CAN BE FOUND AT THE CITY OF SHERWOOD ENGINEERING DEPARTMENT.


5. A COPY OF THE DOWNTOWN STREETScape MASTER PLAN CAN BE FOUND AT THE CITY OF SHERWOOD ENGINEERING DEPARTMENT.

 <p>City of Sherwood Oregon</p>	STANDARD DRAWING TITLE		DRAWING NUMBER
	DOWNTOWN STREETS STANDARDS DESIGN STANDARD NOTES		RD-8
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
		N.T.S.	MAR '16

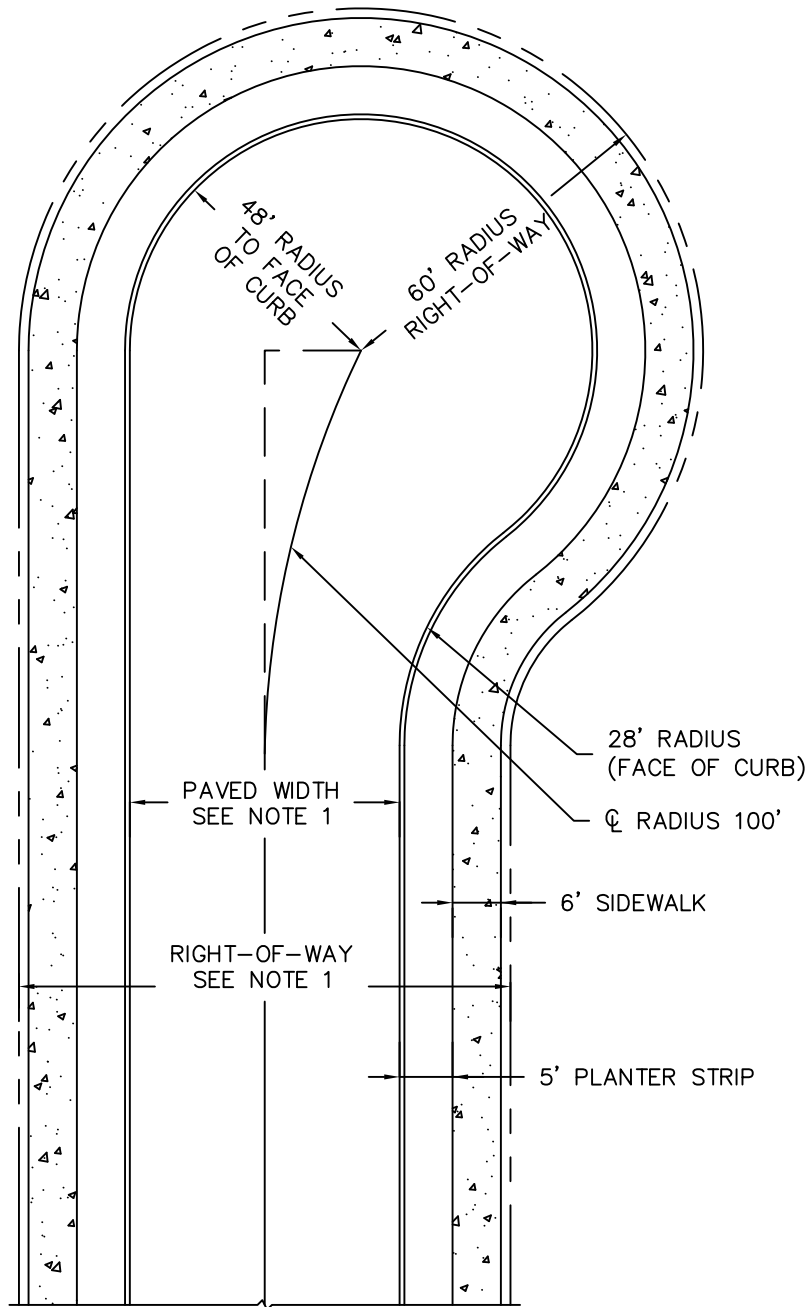


NOTES:

1. SEE CITY STD DET RD-1 FOR RESIDENTIAL STREET RIGHT-OF-WAY AND PAVED WIDTH STANDARDS.
2. A PLANTER STRIP IS REQUIRED ON ALL CUL-DE-SACS.
3. PAVED WIDTH AND PLANTER STRIP ARE MEASURED TO FACE OF CURB.
4. STREET TREES AND STREET LIGHTS ARE REQUIRED AND SHALL BE LOCATED WITHIN THE PLANTER STRIP.
5. LENGTH OF CUL-DE-SAC SHALL NOT EXCEED 200 FEET FROM INTERSECTION.


	STANDARD DRAWING TITLE		DRAWING NUMBER
	CUL-DE-SAC		RD-10
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
N.T.S.		MAR '16	

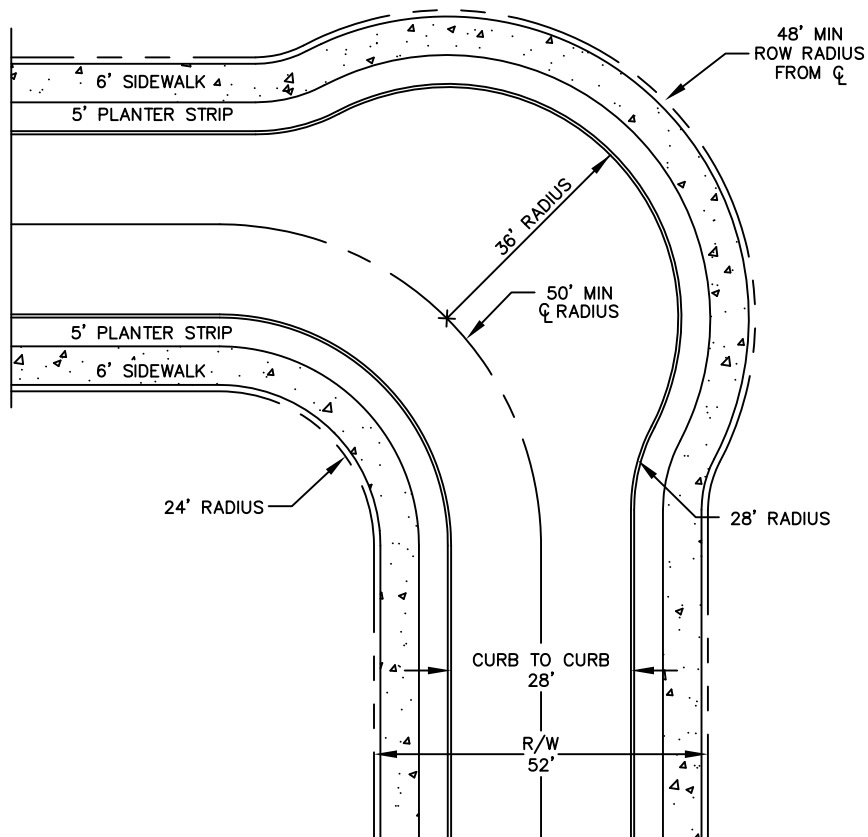




NOTES:

1. SEE CITY STANDARD DWG RD-1 FOR RESIDENTIAL STREET RIGHT-OF-WAY AND PAVED WIDTH STANDARDS.
2. A PLANTER STRIP IS REQUIRED ON ALL CUL-DE-SACS.
3. PAVED WIDTH AND PLANTER STRIP ARE MEASURED TO FACE OF CURB.
4. STREET TREES AND STREET LIGHTS ARE REQUIRED AND SHALL BE LOCATED WITHIN THE PLANTER STRIP.
5. LENGTH OF CUL-DE-SAC SHALL NOT EXCEED 200 FEET.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	TANGENT CUL-DE-SAC		RD-11
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
		N.T.S.	MAR '16

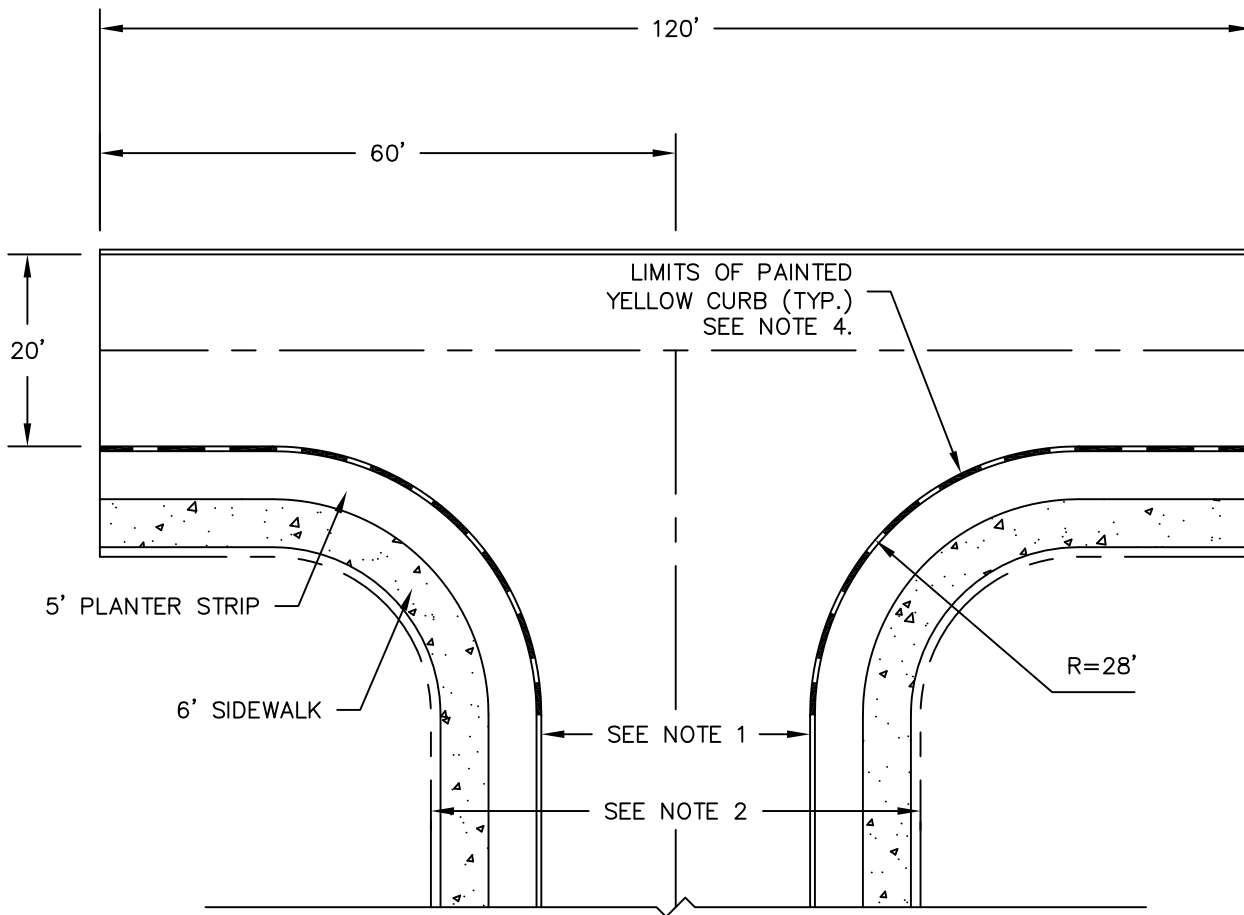


NOTES

1. EYEBROWS ALLOWED ON LOCAL STREETS ONLY.
2. A STANDARD CATCHBASIN SHALL BE LOCATED ALONG THE EYEBROW CURBLINE FOR STREET STORM DRAINAGE.
3. PLANTER STRIPS ARE REQUIRED ON BOTH SIDES OF STREET.
4. PAVED WIDTH AND PLANTER STRIPS ARE MEASURED FROM FACE OF CURB.
5. STREET TREES AND STREET LIGHTS ARE REQUIRED AND SHALL BE LOCATED WITHIN THE PLANTING STRIPS.



STANDARD DRAWING TITLE		DRAWING NUMBER
LOCAL STREET EYEBROW		RD-12
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

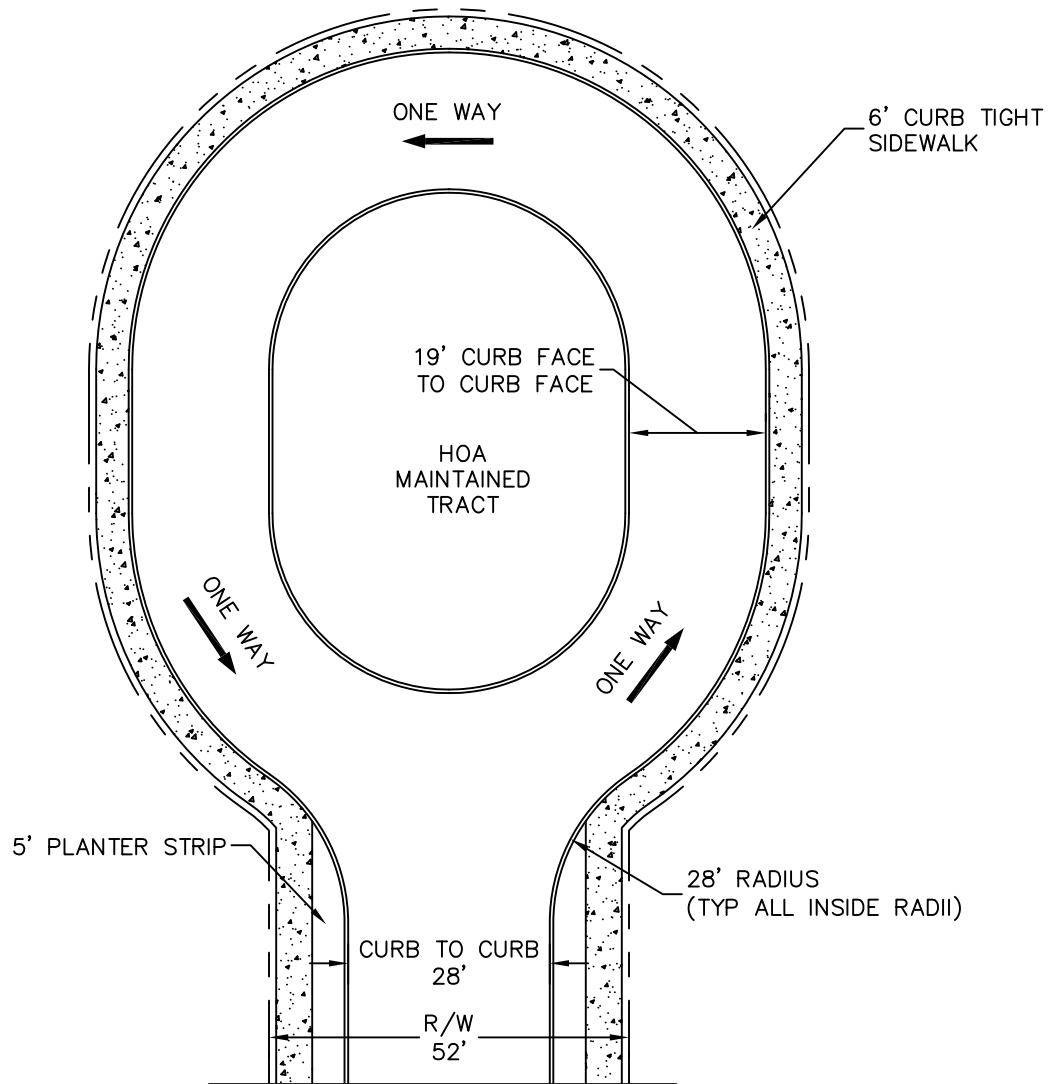


NOTES:

1. ROADWAY SECTION WIDTH TO CONFORM TO CITY TRANSPORTATION SYSTEM PLAN (TSP) AND STD DET DWGS RD-1 THROUGH RD-6.
2. RIGHT-OF-WAY SECTION WIDTH TO CONFORM TO CITY TRANSPORTATION SYSTEM PLAN (TSP) AND STD DET DWGS RD-1 THROUGH RD-6.
3. CURB HEIGHT NOT EXCEED 6-INCHES
4. "NO PARKING" AREA IS TO BE DEFINED BY YELLOW CURB. YELLOW CURB TO EXTEND FROM BEGINNING OF CURB RETURN TO END OF HAMMERHEAD ON BOTH SIDES OF ROADWAY.
5. "NO PARKING/FIRE LANE/TOW ZONE" SIGN LOCATIONS TO BE DETERMINED BY THE FIRE MARSHALL AND CITY ENGINEER.
6. DRAINAGE IS REQUIRED TO PREVENT PONDING.
7. AREA OF TURNAROUND IS TO BE PERMANENTLY MAINTAINED, CLEARED AND UNOBSTRUCTED AT ALL TIMES.
8. ADJACENT LANDSCAPE IS TO BE APPROVED BY THE FIRE MARSHALL AND CITY PRIOR TO INSTALLATION.
9. USE OF THIS TURNAROUND AT ANY SPECIFIC SITE MUST BE APPROVED BY THE FIRE MARSHALL AND CITY ENGINEER.




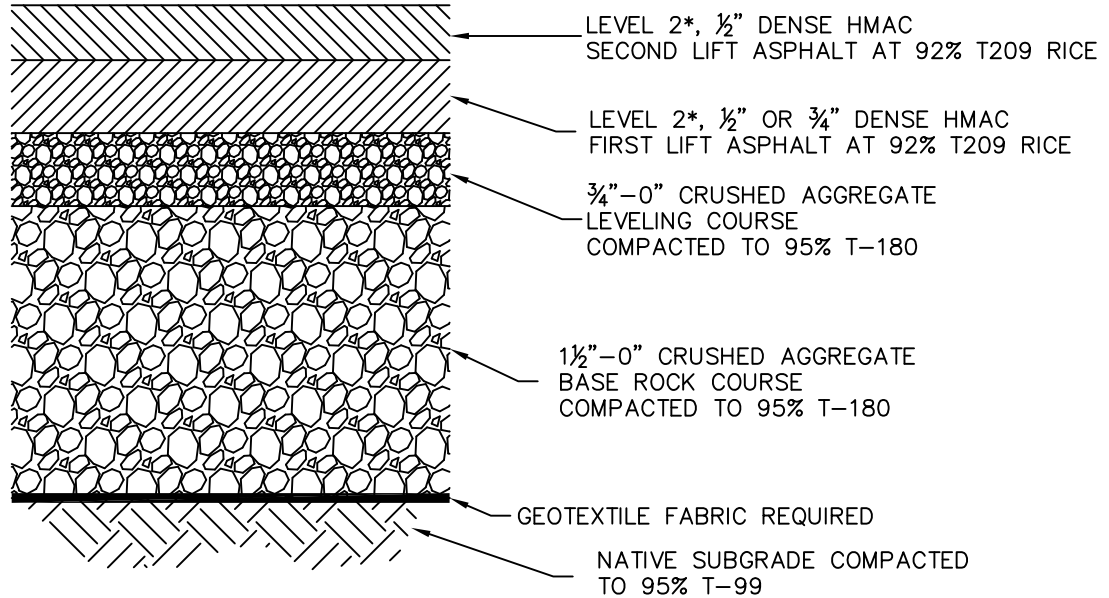
STANDARD DRAWING TITLE		DRAWING NUMBER
HAMMERHEAD TURNAROUND		RD-13
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



**NOTES**

1. LOOP DIMENSIONS SHALL ADEQUATELY ACCOMMODATE EMERGENCY VEHICLES.
2. NO PARKING SIGNS SHALL BE INSTALLED WITHIN LOOP.
3. SEPARATE TRACT TO BE MAINTAINED BY HOMEOWNERS ASSOCIATION (HOA).
4. THIS DETAIL TO BE USED ONLY WITH CITY ENGINEER APPROVAL.
5. LENGTH OF TURNAROUND AND ENTRY ROAD NOT TO EXCEED 200 FEET FROM INTERSECTION.

	STANDARD DRAWING TITLE		DRAWING NUMBER	
	LOOP TURNAROUND		RD-14	
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE		DATE
		N.T.S.		MAR '16




**PAVEMENT SECTION CHART  
COMPONENT THICKNESS (INCHES)**

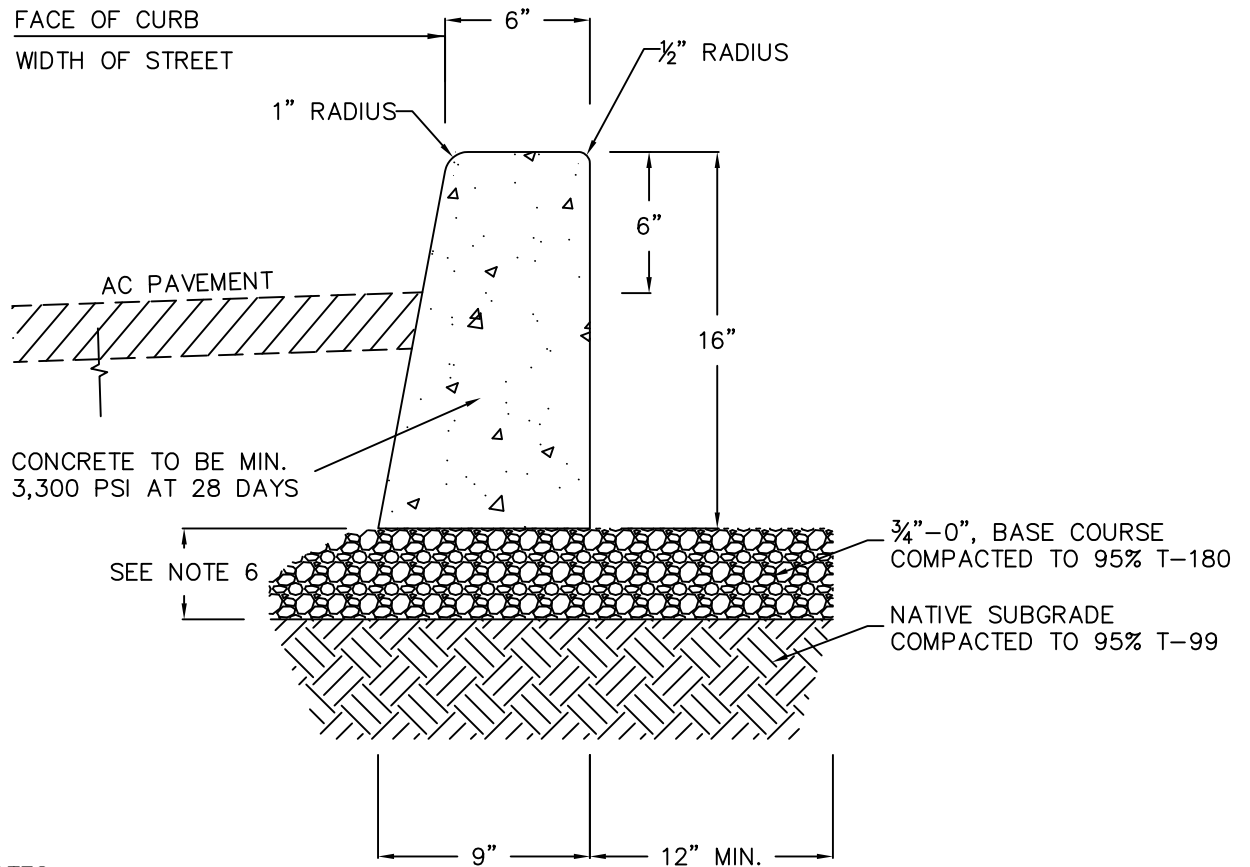
FUNCTIONAL CLASSIFICATION	SECOND LIFT HMAC THICKNESS	FIRST LIFT HMAC THICKNESS	LEVELING COURSE THICKNESS	BASE ROCK COURSE THICKNESS
LOCAL	2"	2"	2"	8"
NEIGHBORHOOD	2"	2"	2"	9"
COMMERCIAL	2"	3"	3"	9"
COLLECTOR	2"	3"	3"	9"
ARTERIAL	2"	3"	4"	10"

**NOTES:**

1. MATERIALS AND PLACEMENT OF THE HOT MIXED ASPHALT CONCRETE PAVEMENT (ACP) SHALL CONFORM TO THE REQUIREMENTS DELINEATED IN SECTION 00744 – ASPHALT CONCRETE PAVEMENT (ACP), OF THE ODOT/APWA, OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION), EXCEPT AS MODIFIED BY CITY AND NOTED IN THE CITY’S ENGINEERING DESIGN AND STANDARD DETAILS MANUAL (LATEST EDITION).
2. THE TOP LIFT OF HMAC SHALL BE PLACED PRIOR TO CITY FINAL ACCEPTANCE OF PUBLIC INFRASTRUCTURE IMPROVEMENTS.
3. CRUSHED AGGREGATE USED FOR BASE ROCK AND LEVELING COURSE SHALL CONFORM TO THE REQUIREMENTS DELINEATED IN SECTION 02630 – BASE AGGREGATE, OF THE ODOT/APWA, OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION).

\* FOR ARTERIAL CLASSIFICATION USE LEVEL 3.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	PAVEMENT SECTION		RD-20
	<small>Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.</small>	SCALE	DATE
		N.T.S.	MAR '16



NOTES:

1. VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
2. CONCRETE SHALL BE COMMERCIAL MIX. MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
3. EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
4. MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2 INCH.
5. CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1 1/2" IN DEPTH..
6. BASE ROCK: 3/4"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.



STANDARD DRAWING TITLE

DRAWING NUMBER

VERTICAL CURB

RD-21

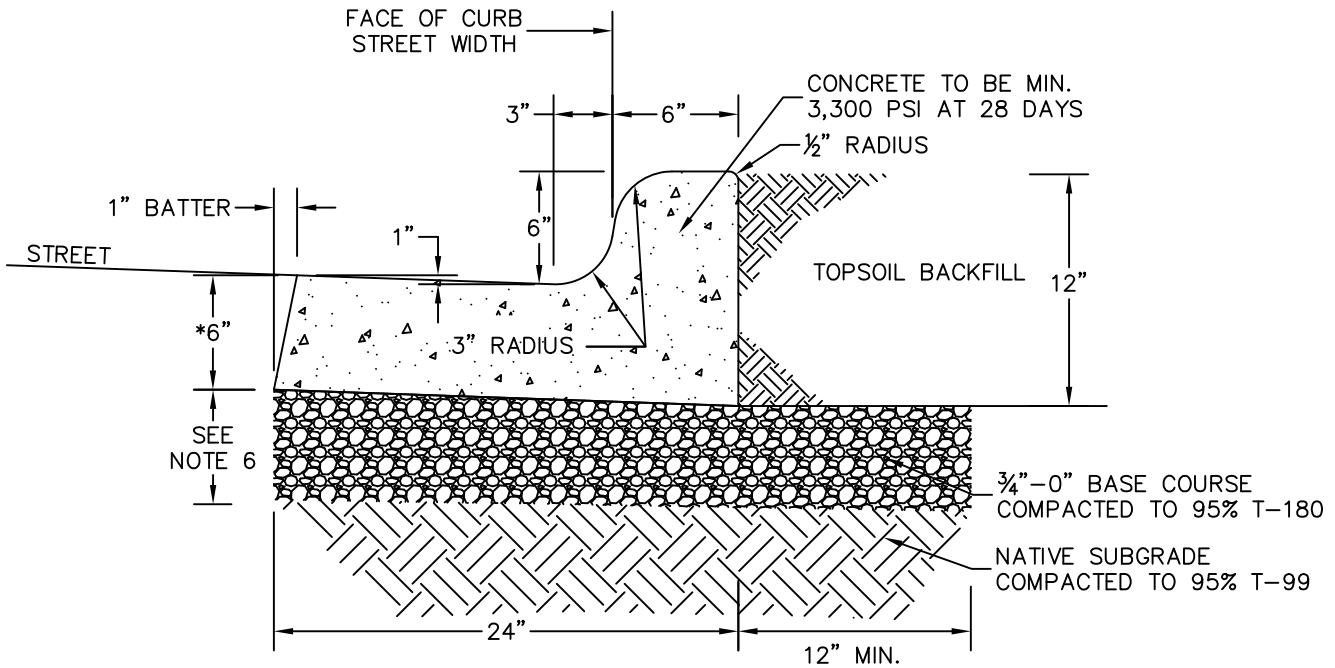
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.

SCALE

DATE

N.T.S.

MAR '16

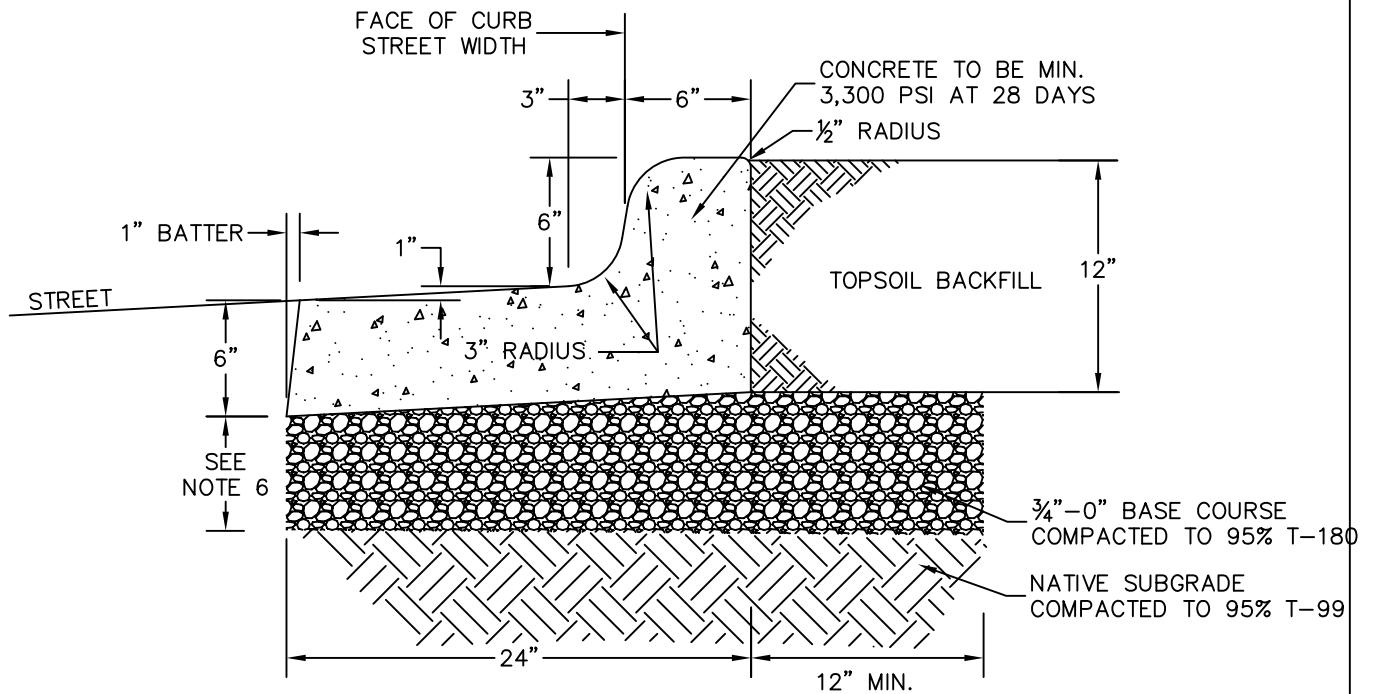


NOTES:

1. MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
2. CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PSI, WITH A 4" MAX SLUMP.
3. EXPANSION JOINTS TO BE PROVIDED AT EACH:
  - A. POINT OF TANGENCY.
  - B. COLD JOINT.
  - C. SIDE OF INLET STRUCTURES.
  - D. SIDE OF DRIVEWAYS.
4. EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2".
5. CONTRACTION JOINTS SHALL HAVE:
  - A. SPACING OF NOT MORE THAN 15 FEET.
  - B. DEPTH OF JOINT OF AT LEAST 1 1/2".
6. BASE ROCK SHALL BE 3/4"-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.
7. FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23
- \* COMMERCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.



STANDARD DRAWING TITLE		DRAWING NUMBER
MONOLITHIC CURB AND GUTTER		RD-22
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



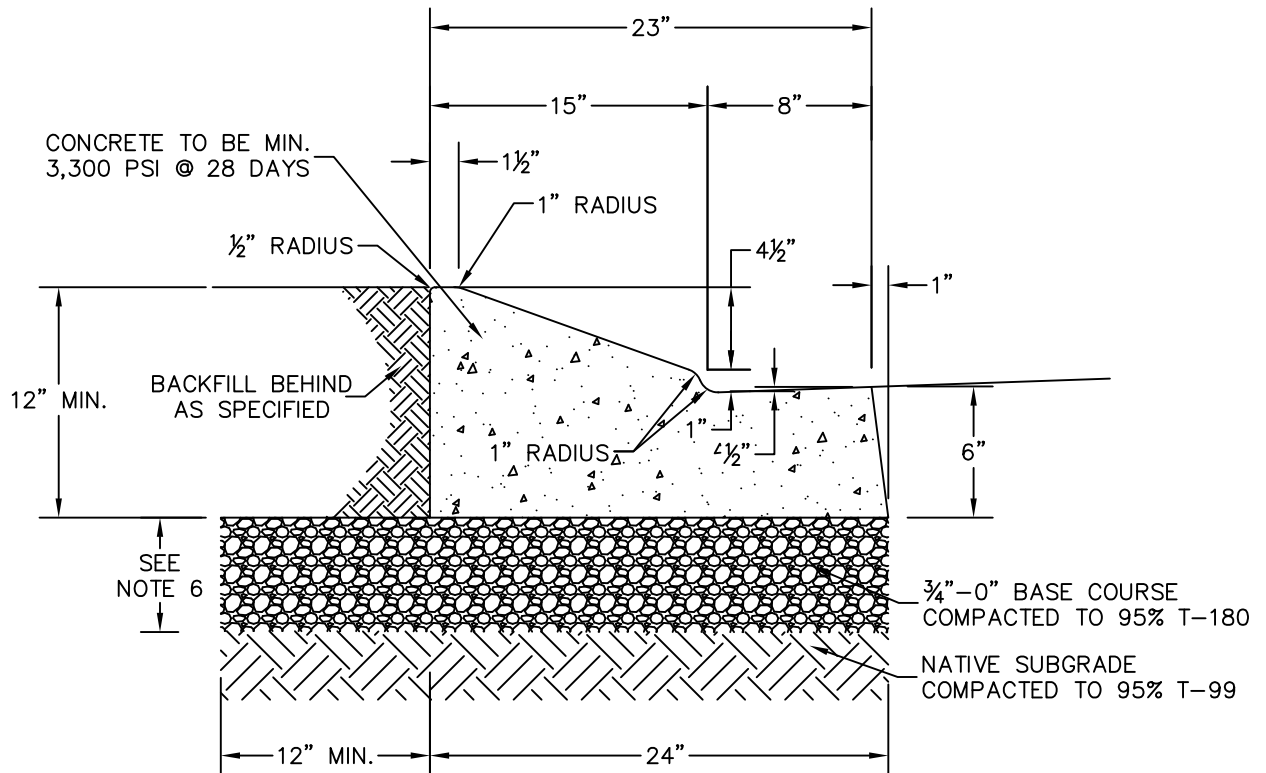
NOTES:

1. REVERSE MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW SHED AND SUPERELEVATED ROADWAY SECTIONS WHERE ROAD CROSS-SLOPE IS AWAY FROM THE GUTTER. AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
2. CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH A 4" MAX SLUMP.
3. EXPANSION JOINTS TO BE PROVIDED AT EACH:
  - A. POINT OF TANGENCY.
  - B. COLD JOINT.
  - C. SIDE OF INLET STRUCTURES.
  - D. SIDE OF DRIVEWAYS.
4. EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2".
5. CONTRACTION JOINTS SHALL HAVE:
  - A. SPACING OF NOT MORE THAN 15 FEET.
  - B. DEPTH OF JOINT OF AT LEAST 1 1/2".
6. BASE ROCK SHALL BE 3/4"-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.



STANDARD DRAWING TITLE		DRAWING NUMBER
REVERSE MONOLITHIC CURB AND GUTTER		RD-23
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



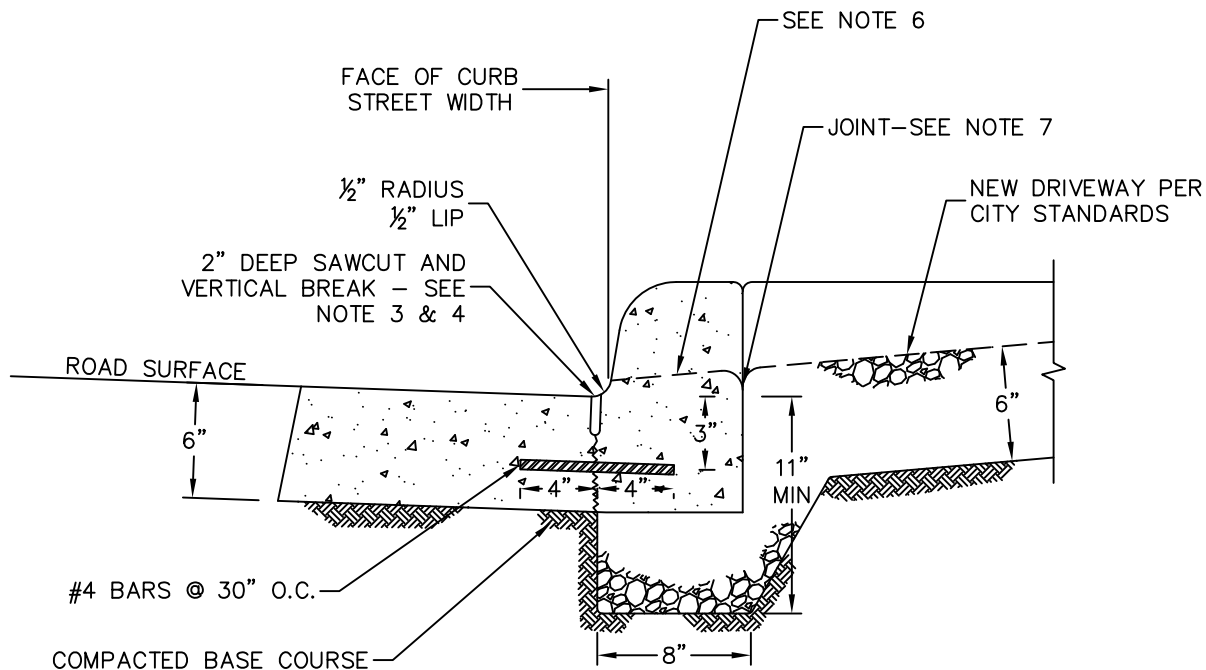


NOTES:

1. FOR USE IN CUL-DE-SACS AND OTHER SPECIAL CIRCUMSTANCES.
2. CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH A 4" MAX SLUMP.
3. EXPANSION JOINTS TO BE PROVIDED AT EACH:
  - A. POINT OF TANGENCY.
  - B. COLD JOINT.
  - C. SIDE OF INLET STRUCTURES.
  - D. SIDE OF DRIVEWAYS.
4. EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2".
5. CONTRACTION JOINTS SHALL HAVE:
  - A. SPACING OF NOT MORE THAN 15 FEET.
  - B. DEPTH OF JOINT OF AT LEAST 1 1/2".
6. BASE ROCK SHALL BE 3/4"-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 4", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.




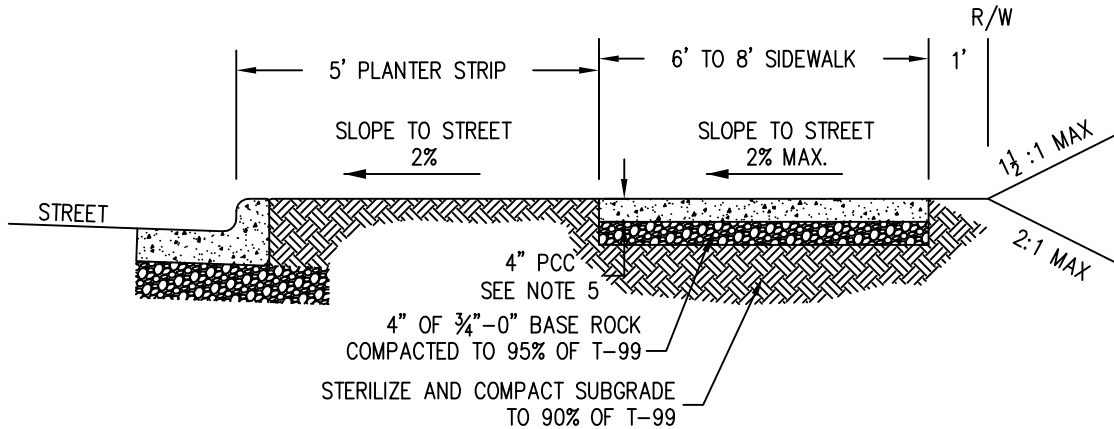
STANDARD DRAWING TITLE		DRAWING NUMBER
MOUNTABLE CURB AND GUTTER		RD-24
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



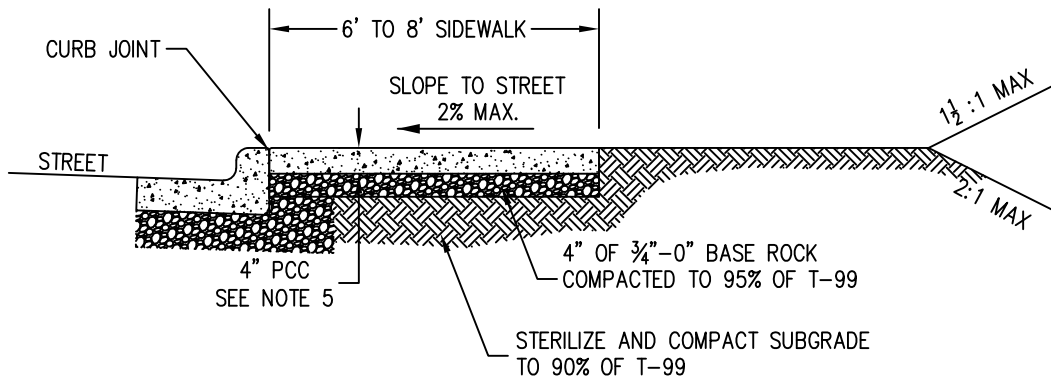
NOTES:

1. REMOVING ONLY THE TOP PORTION OF THE CURB TO MAKE THE CURB CUT IS NOT ALLOWED, UNLESS EXPRESSLY APPROVED BY THE CITY ENGINEER. REMOVE CURB SHOWN CROSS-HATCHED ABOVE, EXCEPT AS REQUIRED OTHERWISE BY NOTE 2.
2. IF CURB IS TO BE REMOVED BEFORE FINAL ASPHALT LIFT IS IN-PLACE OR IF PAVEMENT IS PCC, GUTTER MUST BE REMOVED WITH CURB. REPLACE EXISTING CURB AND GUTTER WITH NEW CURB AND GUTTER WITH CURB CUT PER CITY STANDARD DETAIL RD-21 THROUGH RD-24.
3. IF ALL ASPHALT LIFTS ARE IN PLACE, ONLY CURB SHALL BE REMOVED AT FULL-DEPTH, AND REINSTALLED WITH #4 REBAR. REBAR DOWELS SHALL BE PLACED 3" BELOW TOP OF GUTTER AND SHALL BE 30" MAXIMUM ON CENTER AND 6" FROM EACH END OF CUT.
4. SAWCUT LINE SHALL BE AS CLOSE TO FACE OF CURB AS POSSIBLE.
5. CONCRETE SHALL BE COMMERCIAL MIX WITH A 28-DAY COMPRESSIVE STRENGTH OF 3300 PSI, AND A 4" MAX SLUMP.
6. BEVEL THE CURB BACK FROM THE GUTTER TO BACK OF CURB AT 8.33% (1:12).
7. CURB JOINT SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS ALONG BACK OF CURB.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	CURB RETROFIT		RD-25
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
		N.T.S.	MAR '16



**SIDEWALK WITH PLANTER STRIP**  
(FOR ALL NEW CONSTRUCTION)



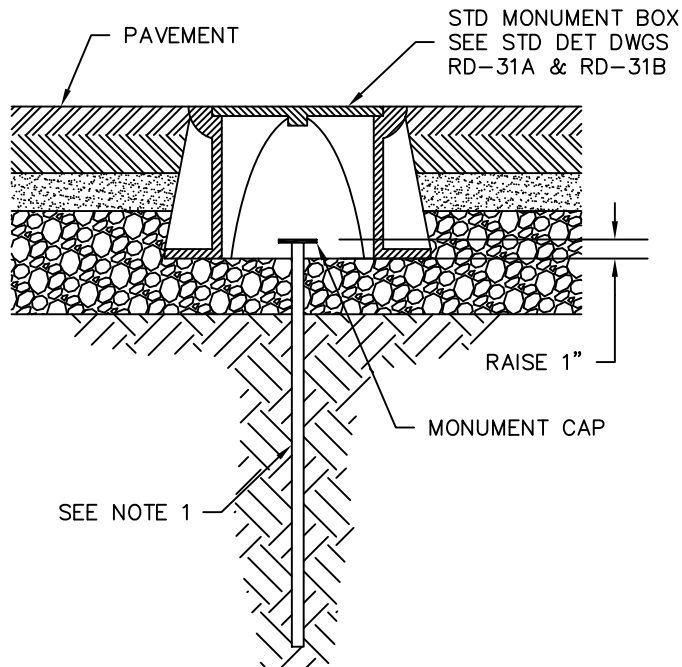
**CURB-TIGHT SIDEWALK**  
(TO BE USED WITH VARIANCE ONLY)

NOTES:

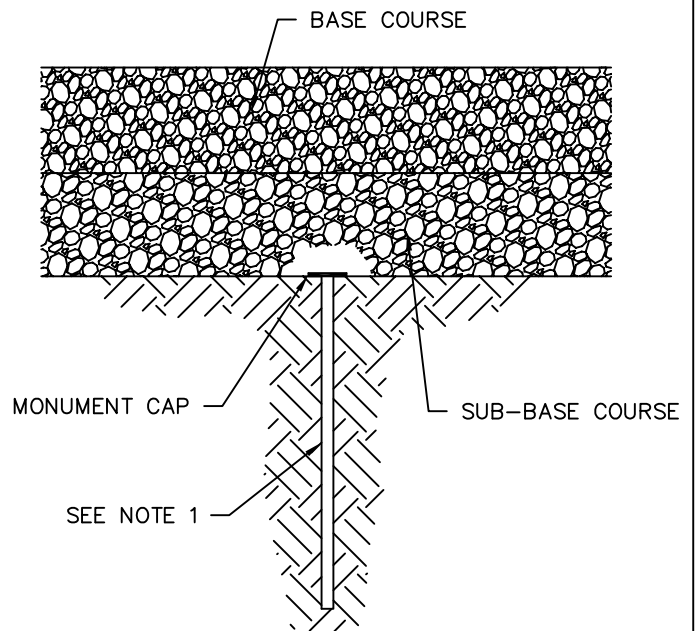
1. CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI @ 28 DAYS, WITH A 4" MAX SLUMP.
2. SIDEWALK PANELS TO BE SQUARE (6' LONG x 6' WIDE TYP.).
3. EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, CURB RAMPS, AND/OR POINTS OF TANGENCY IN CURB AS SHOWN ON THE STANDARD DRAWINGS FOR SIDEWALK RAMPS, AND AT SPACING NOT TO EXCEED 45'.
4. FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS.
5. SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4". IF MOUNTABLE CURB IS USED, OR IF SIDEWALK IS INTENDED AS PORTION OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6" MINIMUM THICKNESS, COMMERCIAL 8".
6. CONCRETE SHALL HAVE A BROOM FINISH, ALL JOINTS SHALL BE EDGED WITH 3" SHINE.
7. WIDTH OF PLANTER STRIP IS MEASURED FROM FACE OF CURB.
8. IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 1' PAST BACK OF SIDEWALK WITH A 3" DIAMETER ADS PIPE. CONTRACTION JOINT SHALL BE PLACE OVER PIPE.



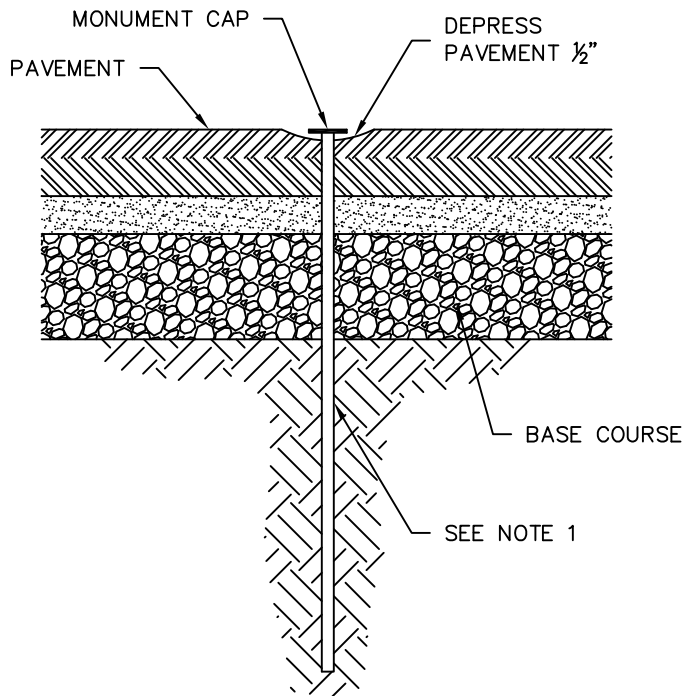
STANDARD DRAWING TITLE		DRAWING NUMBER
SIDEWALK DETAIL		RD-26
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	REVISION DATE
	N.T.S.	2023.03.17



**INTERSECTION  
CENTERLINE  
IN PAVEMENT**



**CENTERLINES  
IN GRAVEL**



**POINT OF TANGENCY  
IN PAVEMENT**

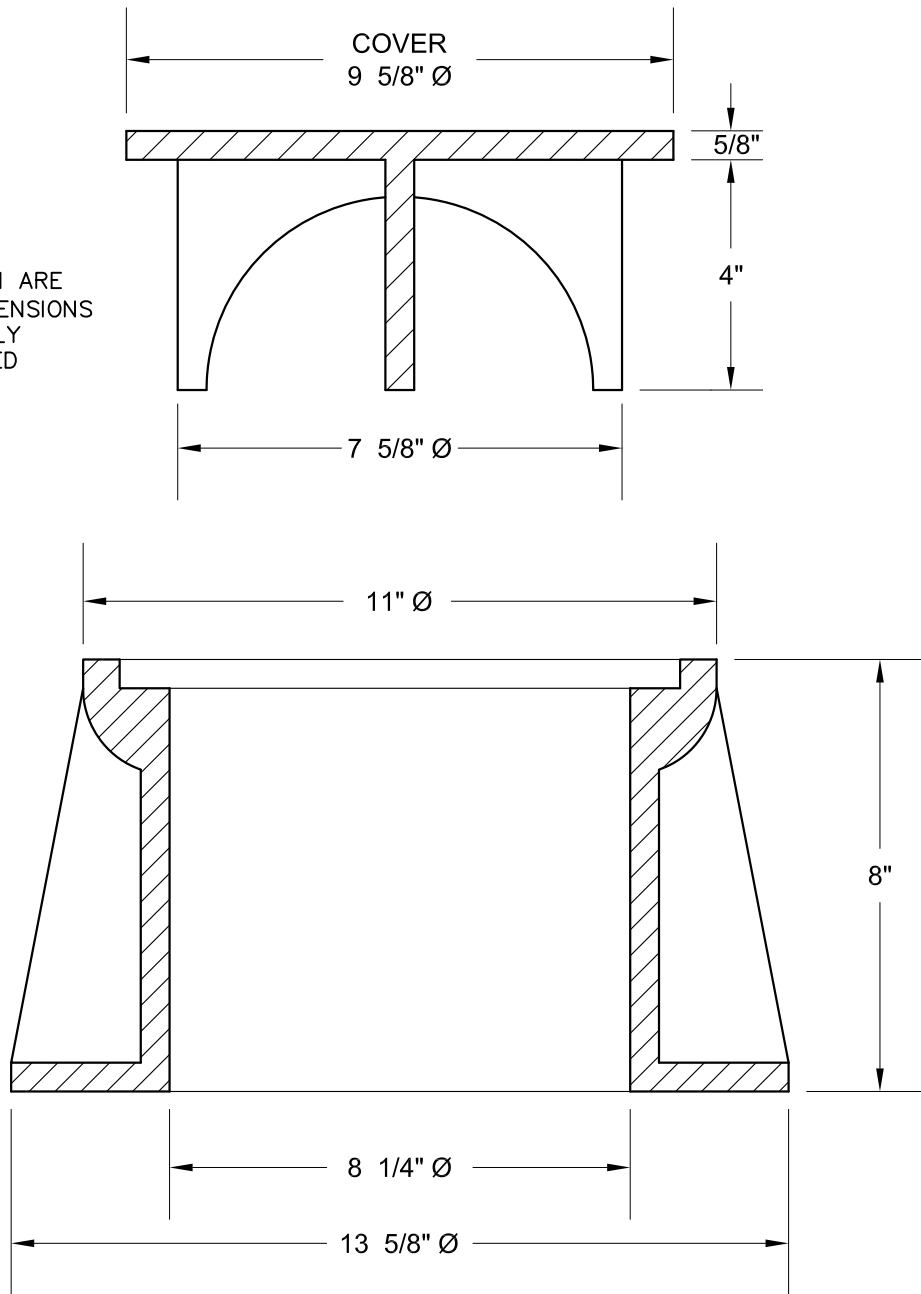
**NOTES:**

1. ALL MONUMENTS SHALL USE EITHER 5/8" DIA x 30" LONG IRON ROD OR 3/4" DIA x 30" LONG IRON PIPE.
2. ALL MONUMENTS SHALL BE IN ACCORDANCE WITH THE OREGON REVISED STATUTES CHAPTERS 92 AND 209.
3. ALL MONUMENTS SHALL MEET WASHINGTON COUNTY SURVEY STANDARDS AND ARE SUBJECT TO APPROVAL BY WASHINGTON COUNTY SURVEYOR.



	STANDARD DRAWING TITLE	DRAWING NUMBER
	CENTERLINE MONUMENTS	RD-30
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

\* DIMENSIONS SHOWN ARE FOR 8" ONLY. DIMENSIONS MAY VARY SLIGHTLY BETWEEN APPROVED VENDORS.

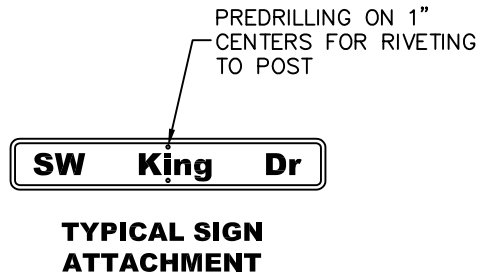
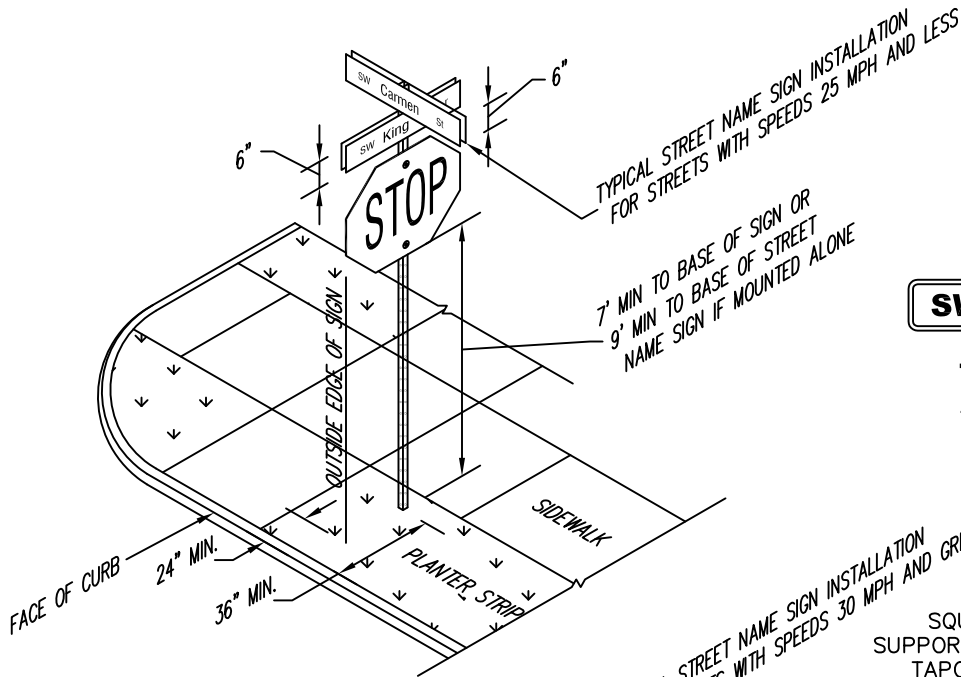


NOTES:

1. STANDARD MONUMENT BOX SHALL BE CAST IRON WITH 8" MINIMUM INSIDE DIAMETER.
2. IF AN 8" MONUMENT BOX IS REQUIRED, USE OLYMPIC FOUNDRY #M1014, EAST JORDAN IRON WORKS #3614 OR APPROVED EQUAL.
3. IF A 12" MONUMENT BOX IS REQUIRED, USE OLYMPIC FOUNDRY #M1035, EAST JORDAN IRON WORKS #3673 OR APPROVED EQUAL.
4. MATCHING MONUMENT BOX COVER SHALL BE CAST IRON WITH "MON" OR "MONUMENT" CAST INTO THE TOP FACE.
5. NEW MANHOLES SHALL BE LOCATED A MINIMUM OF 4 FEET FROM CENTER OF ITS BASE TO MONUMENT BOX.
6. MONUMENT BOXES MUST MEET WASHINGTON COUNTY SURVEY STANDARDS AND ARE SUBJECT TO APPROVAL BY WASHINGTON COUNTY SURVEYOR.

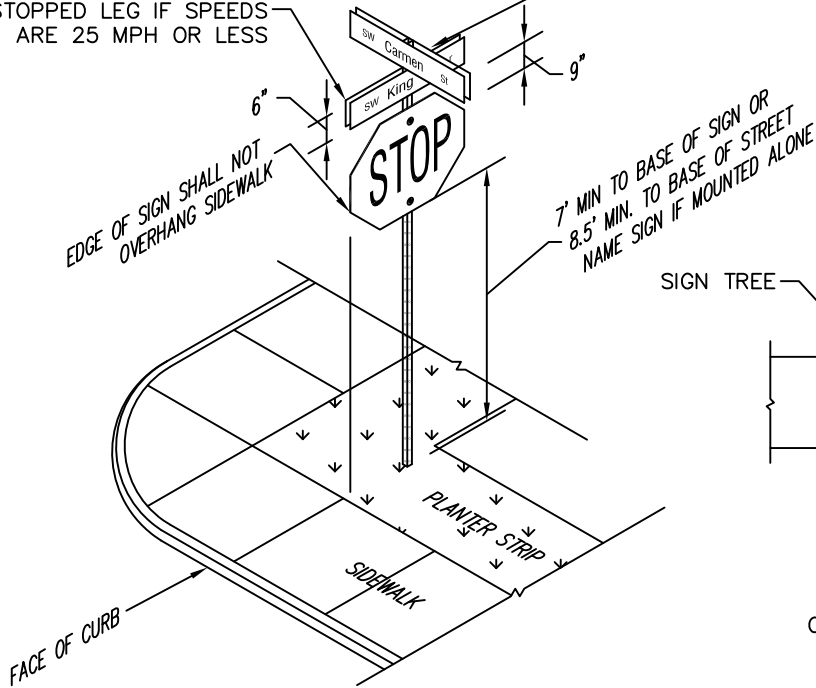


STANDARD DRAWING TITLE		DRAWING NUMBER
MONUMENT BOX		RD-31
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

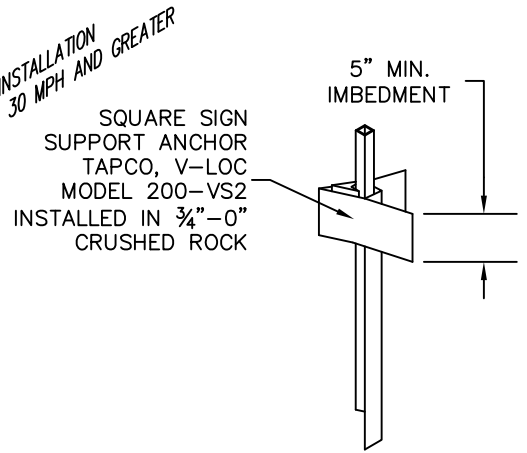


**PLANTER STRIP LOCATION**

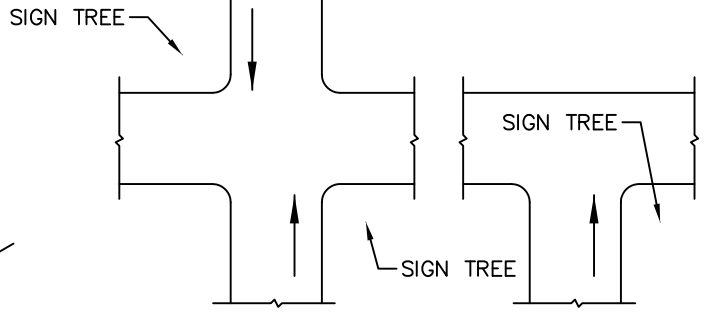
MAY USE 6" SIGN ON STOPPED LEG IF SPEEDS ARE 25 MPH OR LESS



**CURB TIGHT LOCATION**



**SIGN POST ANCHOR**



CROSS INTERSECTION

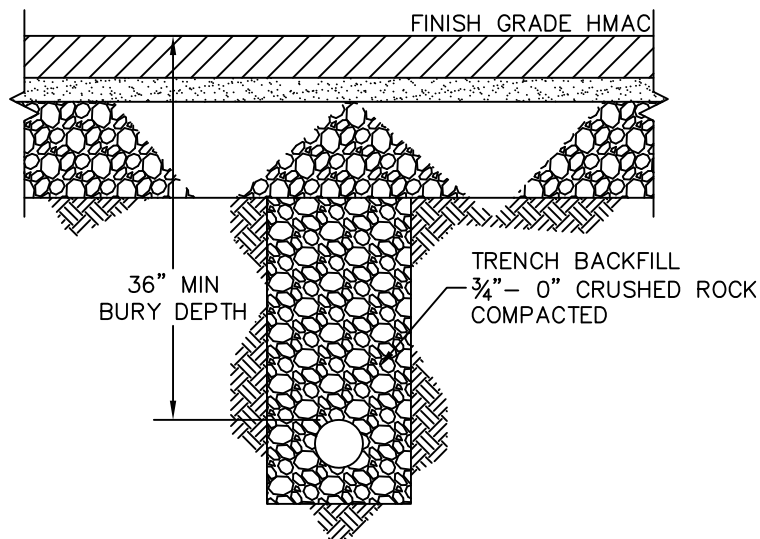
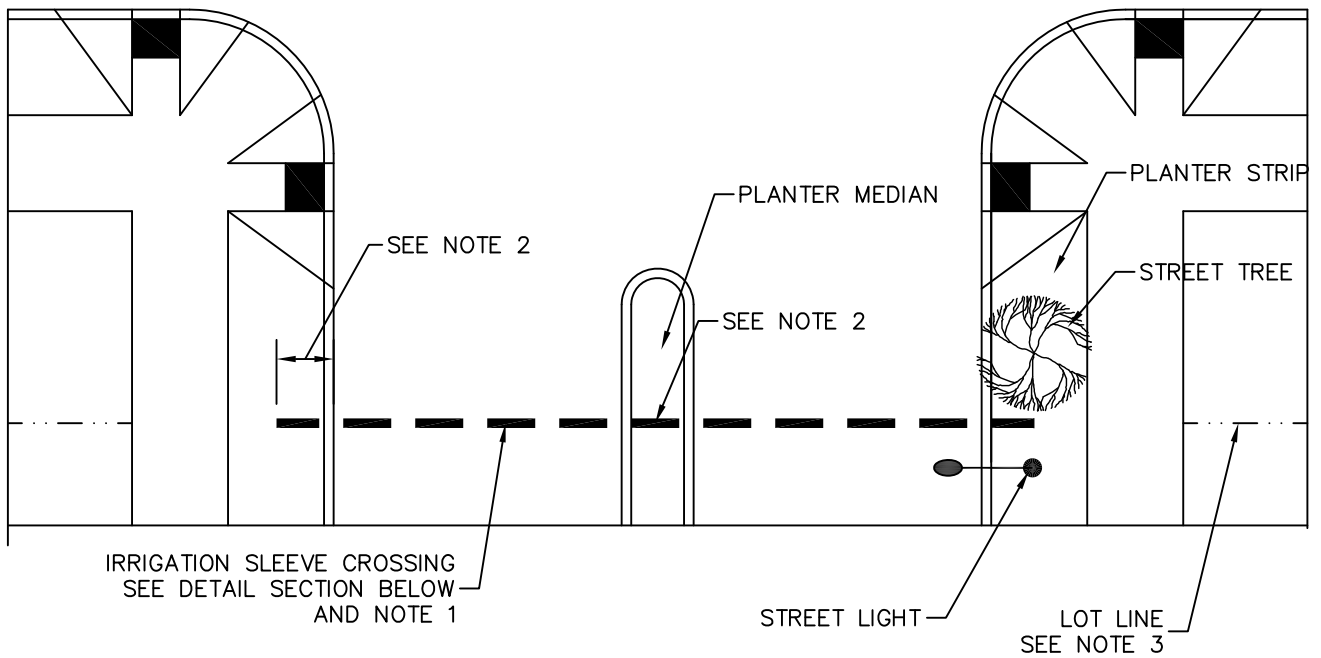
TEE INTERSECTION

**TYPICAL STREET SIGN LOCATIONS**

NOTE: SEE STANDARD DWG S-1 & S-2 FOR STREET NAME SIGN DETAILS



STANDARD DRAWING TITLE		DRAWING NUMBER
STREET SIGN INSTALLATION		RD-35
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

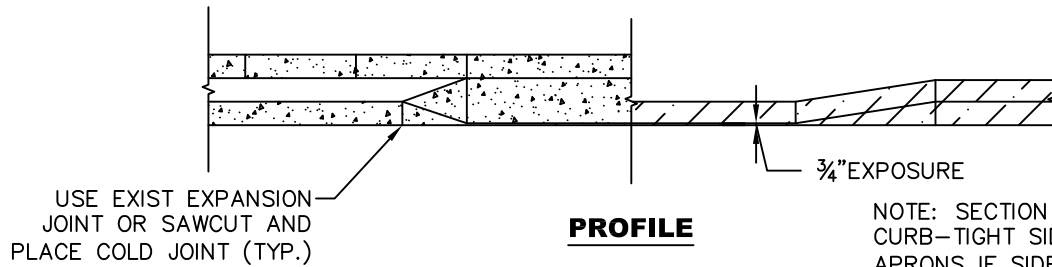
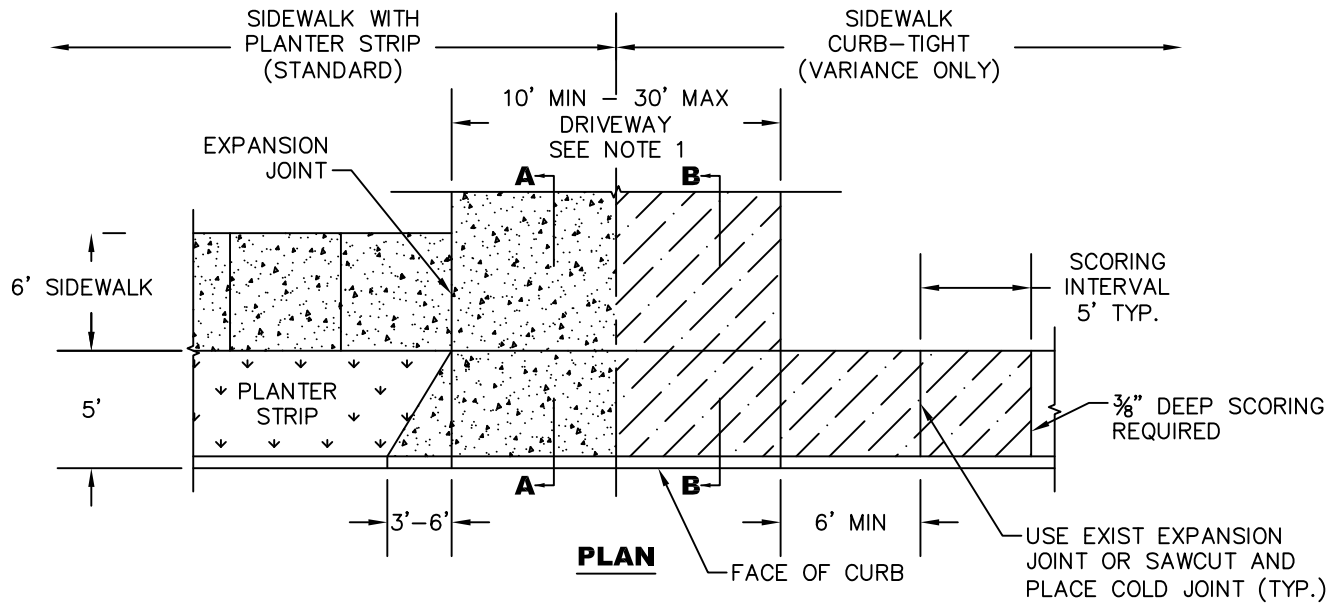


NOTES:

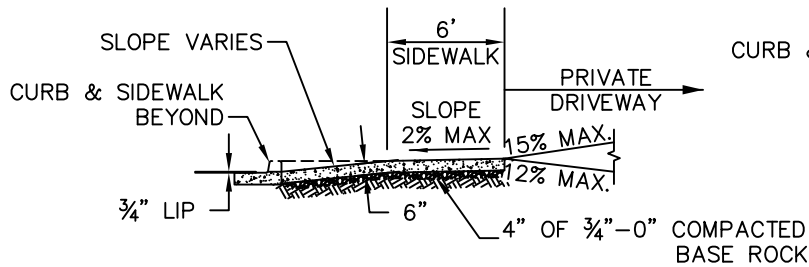
1. IRRIGATION SLEEVE CROSSING MATERIAL SHALL BE 4"Ø DUCTILE IRON PIPE (DIP). LOCATION OF ALL IRRIGATION SLEEVE CROSSINGS SHALL BE SHOWN ON ALL UTILITY PLAN SHEETS.
2. IRRIGATION SLEEVE CROSSING PIPE SHALL EXTEND TO MIDDLE OF PLANTER STRIPS.
3. IRRIGATION SLEEVE CROSSINGS SHALL BE LOCATED AS NEAR TO LOT LINES AS POSSIBLE, AWAY FROM STREET INTERSECTIONS AND ADA RAMPS.
4. IRRIGATION STREET CROSSINGS SHALL BE LOCATED TO AVOID CONFLICT WITH STREET LIGHTS AND STREET TREES.
5. ENDS OF SLEEVE CROSSING SHALL BE EITHER PLUGGED WITH GROUT OR A RUBBER BOOT ATTACHED TO PREVENT INFILTRATION OF SOIL INTO THE ANNULAR SPACE BETWEEN THE SLEEVE AND IRRIGATION LINE. THE SLEEVE CROSSING SHALL NOT BE FILLED WITH SAND OR SLURRY MIX.
6. IRRIGATION SLEEVE CROSSING LOCATIONS SHALL BE NOTED ON CONSTRUCTION PLANS USING STREET CENTERLINE STATIONING. THE SLEEVE CROSSINGS SHALL BE INSTALLED PERPENDICULAR TO STREET CENTERLINE ALIGNMENT.



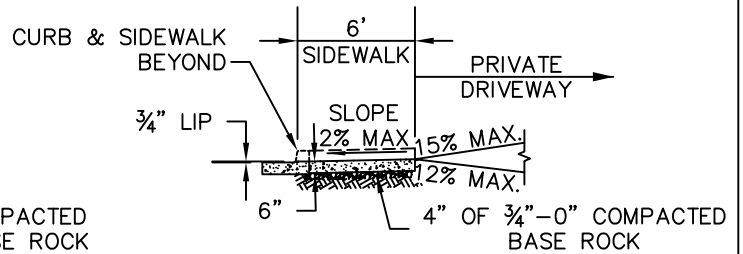
STANDARD DRAWING TITLE		DRAWING NUMBER
IRRIGATION SLEEVE STREET CROSSING DETAIL		RD-40
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



NOTE: SECTION A-A MAY BE USED FOR CURB-TIGHT SIDEWALK DRIVEWAY APRONS IF SIDEWALK WIDTH IS 10' OR GREATER.



**SECTION A-A**



**SECTION B-B**

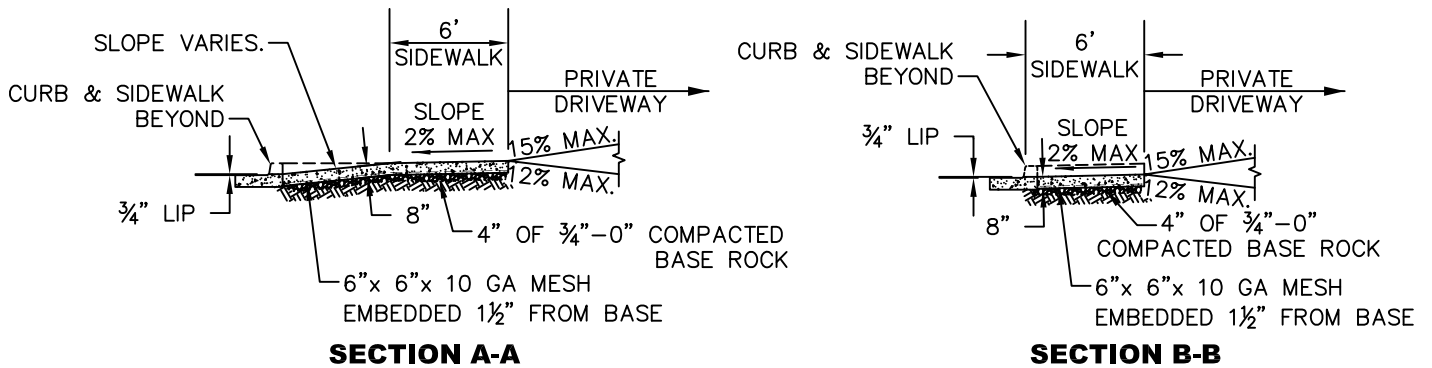
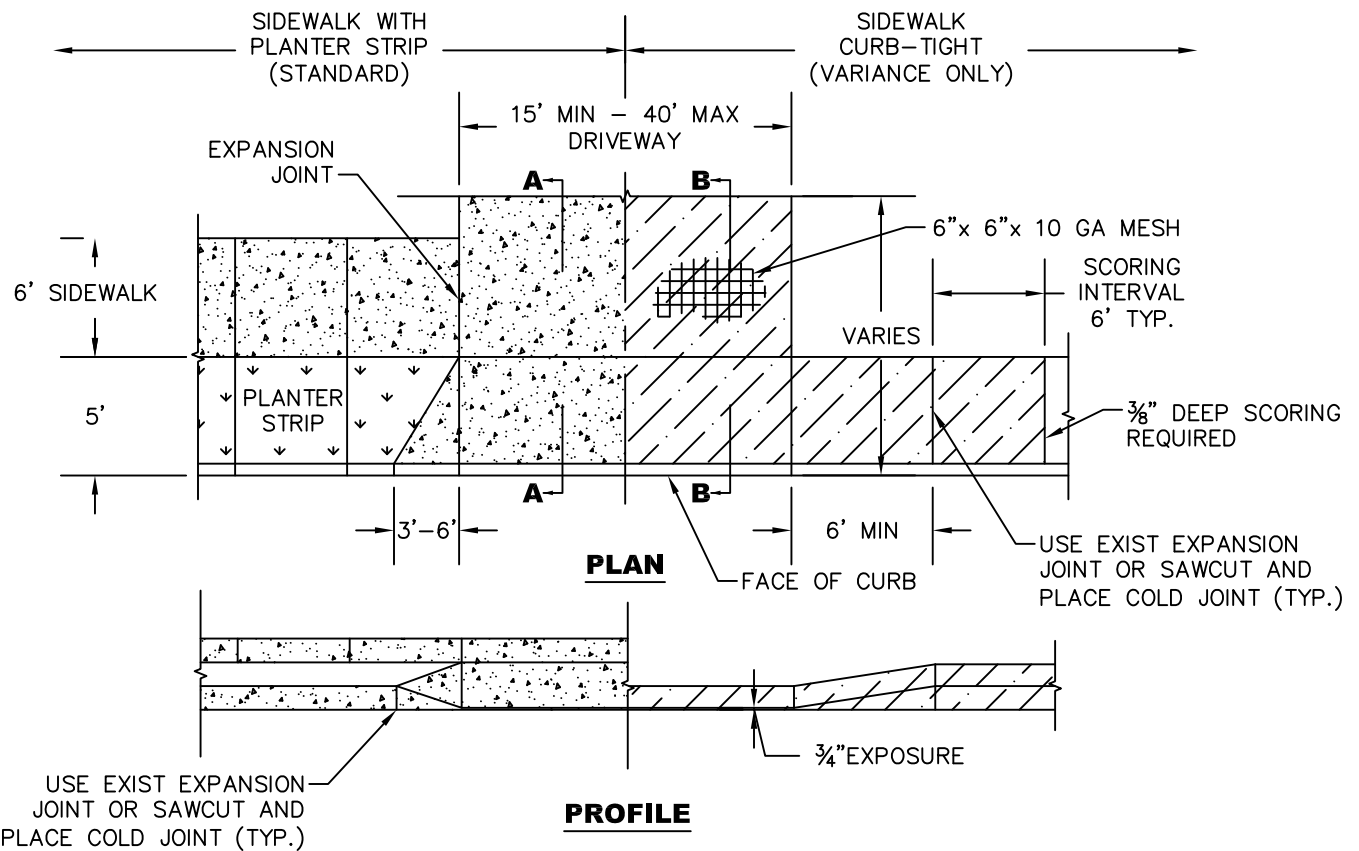
NOTES:

1. DRIVEWAY WIDTHS  
 LOT FRONTAGE UP TO 60': MAX DRIVEWAY WIDTH - 24'  
 LOT FRONTAGE OF GREATER THAN 60': MAX DRIVEWAY WIDTH - 30'
2. DRIVEWAY CONCRETE SHALL HAVE A MINIMUM THICKNESS OF 6".
3. 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 95% OF T-180.
4. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3300 PSI @ 28 DAYS.
5. CURB JOINT SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS ALONG BACK OF CURB.
6. EXPANSION JOINTS SHALL BE 1/2" PRE-MOLDED ASPHALT IMPREGNATED MATERIAL EXTENDING FROM TOP OF BASE ROCK TO FINISHED GRADE.
7. FOR DRIVEWAYS GREATER THAN 24', CONCRETE SHALL BE INCREASED TO A 7" DEPTH.
8. CONCRETE SHALL HAVE A BROOM FINISH AND EDGE ALL JOINTS.
9. WEEPHOLES ARE NOT TO BE PLACED IN WINGS.
10. IF CURBING IS BEING REMOVED TO INSTALL DRIVEWAY AND GUTTER SHOULD BECOME SEPERATED FROM THE DRIVING SURFACE IN EXCESS OF 1/8", THEN THE GUTTER SHALL ALSO BE REMOVED AND REPLACED.
11. SLOPE OF THE DRIVEWAY MAY BE AWAY FROM THE CURB WHEN PRE-APPROVED BY THE CITY ENGINEER.



STANDARD DRAWING TITLE		DRAWING NUMBER
RESIDENTIAL DRIVEWAY DETAIL		RD-41
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	REVISION DATE
	N.T.S.	2023.03.17

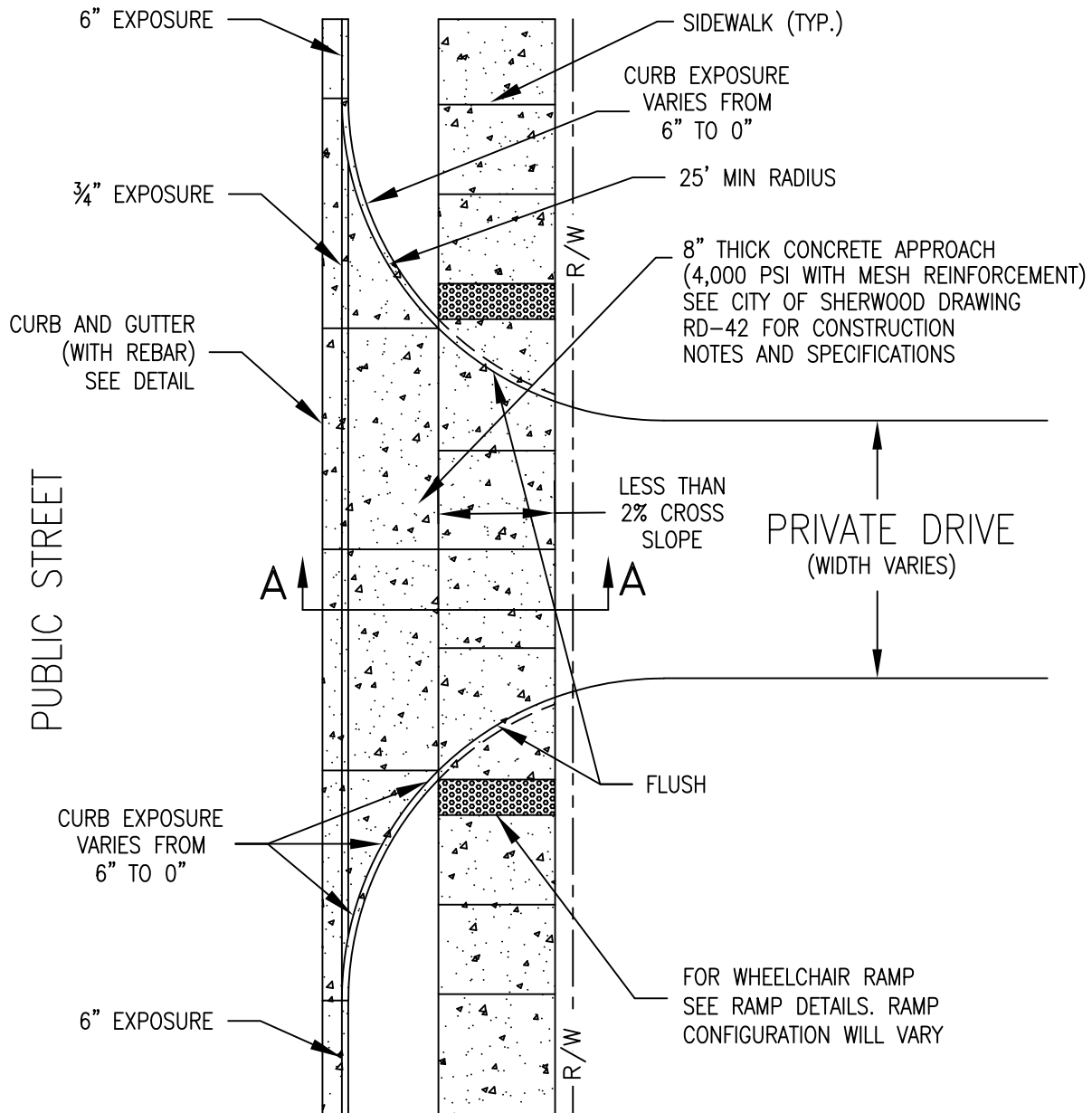




**NOTES:**

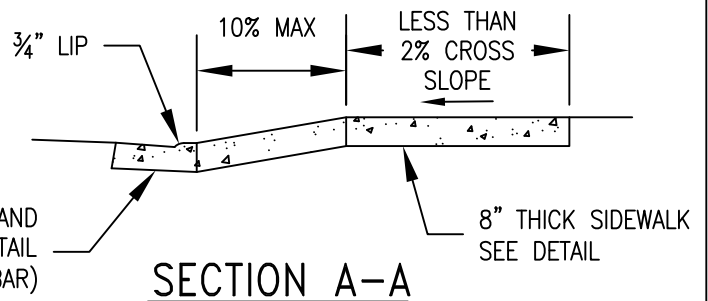
1. CONCRETE SHALL HAVE 8" MINIMUM DEPTH AND A COMPRESSIVE STRENGTH OF 4000 PSI @ 28 DAYS.
2. 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 95% OF T-180.
3. CURB JOINT SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS ALONG BACK OF CURB.
4. EXPANSION JOINTS SHALL BE 1/2" PRE-MOLDED ASPHALT IMPREGNATED MATERIAL EXTENDING FROM TOP OF BASE ROCK TO FINISHED GRADE.
5. CONCRETE SHALL HAVE A BROOM FINISH AND EDGE ALL JOINTS.
6. WEEPHOLES ARE NOT TO BE PLACED IN WINGS.
7. IF CURBING IS BEING REMOVED TO INSTALL DRIVEWAY AND GUTTER SHOULD BECOME SEPARATED FROM THE DRIVING SURFACE IN EXCESS OF 1/16", THEN THE GUTTER SHALL ALSO BE REMOVED AND REPLACED.
8. WINGS OF THE COMMERCIAL DRIVEWAY WHICH ARE A PORTION OF THE SIDEWALK SHALL NOT EXCEED A SLOPE OF 8.33% (1:12).
9. ODOT STANDARD DRAWINGS FOR COMMERCIAL DRIVEWAYS MAY BE USED WHEN PRE-APPROVED BY CITY ENGINEER.
10. SLOPE OF THE DRIVEWAY MAY BE AWAY FROM THE CURB WHEN PRE-APPROVED BY THE CITY ENGINEER.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	COMMERCIAL DRIVEWAY DETAIL		RD-42
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	REVISION DATE
	N.T.S.	2023.03.17	



**PLAN VIEW**

N.T.S.

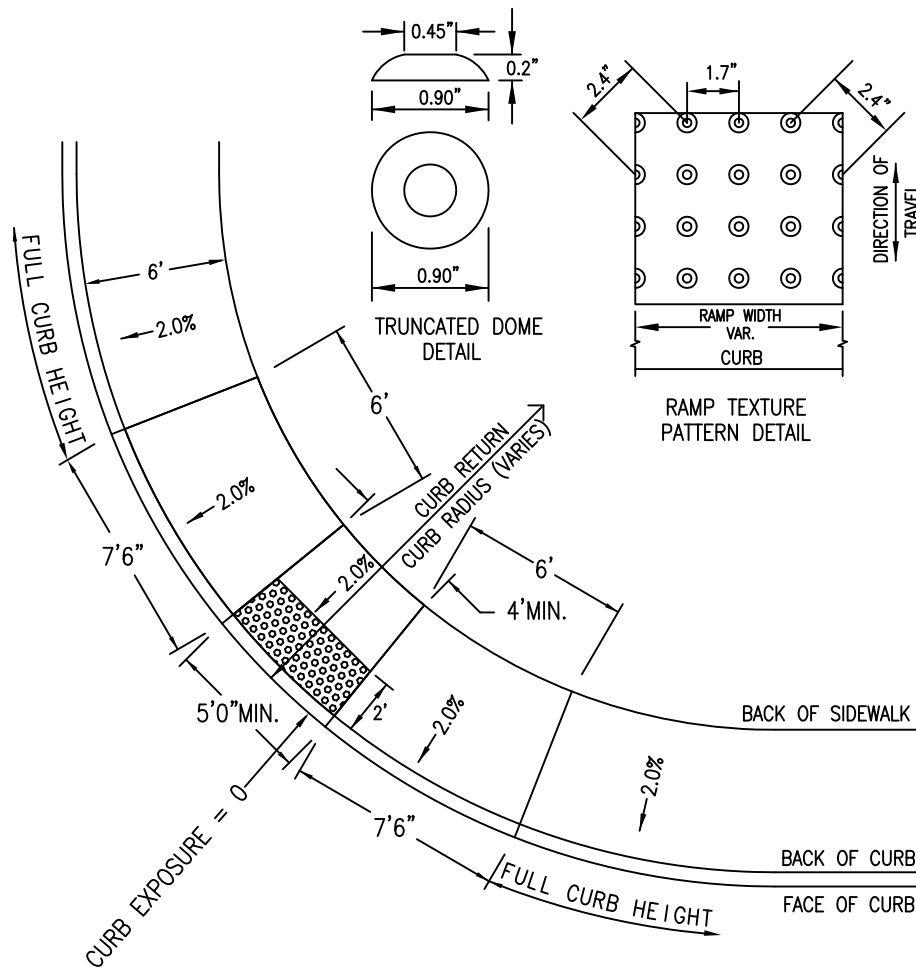


SEE CURB AND GUTTER DETAIL (8" THICK WITH REBAR)




STANDARD DRAWING TITLE		DRAWING NUMBER
HIGH VOLUME COMMERCIAL DRIVEWAY		RD-42 HV
SCALE		DATE
N.T.S.		2023.03.17

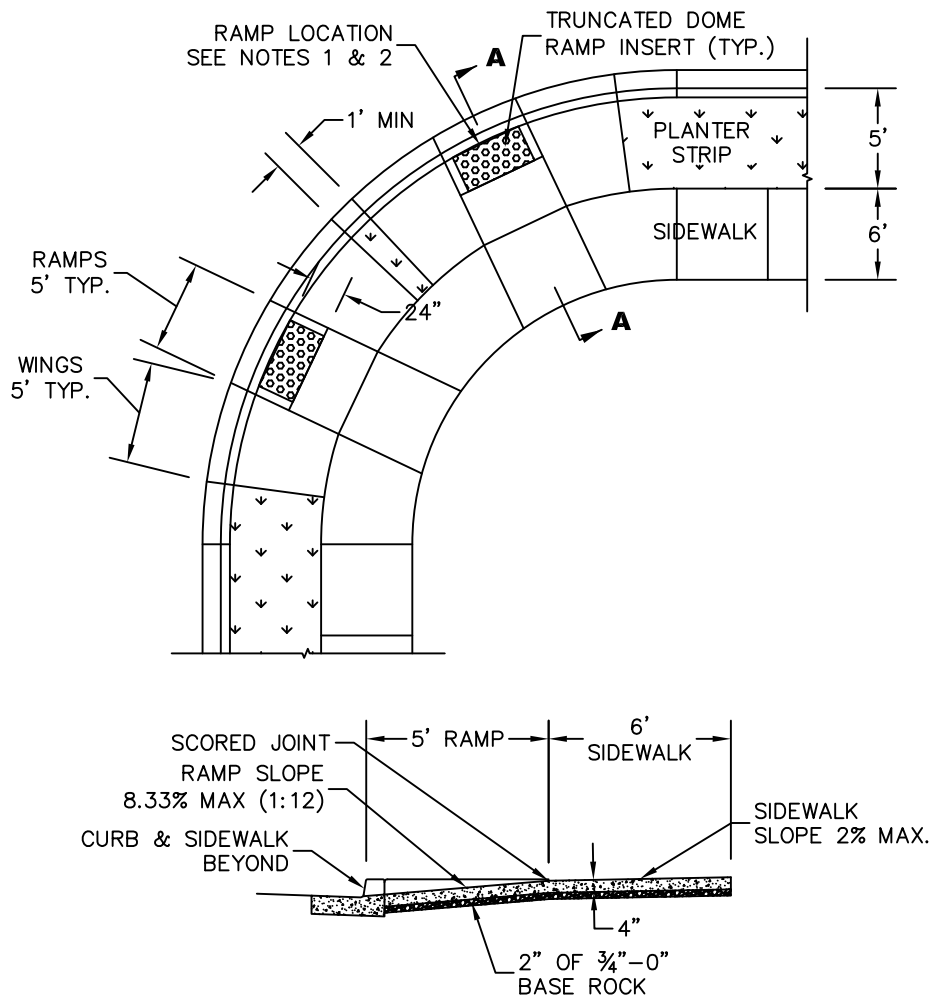
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.



NOTES:

1. PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
2. PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
3. SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
4. PLANTER STRIP DETAIL – REFER TO ODOT STANDARD DRAWING RD 755 (SIDEWALK RAMP DETAIL).
5. DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
6. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
7. CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI WITH A 4" MAX SLUMP.
8. ¾"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
9. BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
10. SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
11. CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
12. PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.


	STANDARD DRAWING TITLE		DRAWING NUMBER	
	CURB-TIGHT CURB RETURN & RAMP DETAIL		RD-43	
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE		DATE
		N.T.S.		MAR '16

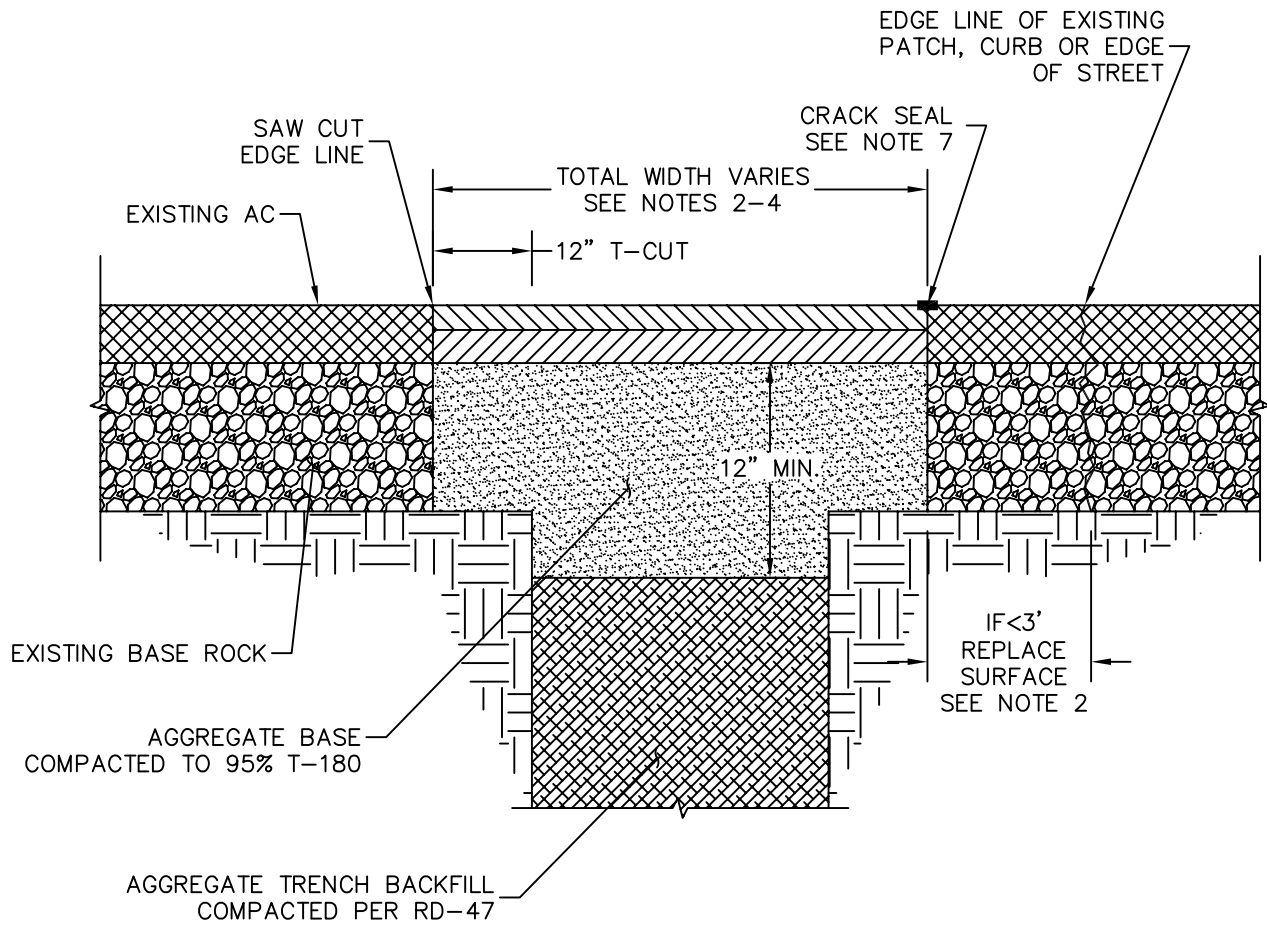


**SECTION A-A**

NOTES:

1. PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
2. PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
3. SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
4. DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
5. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
6. CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH A 4" MAX SLUMP.
7. ¾"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
8. BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
9. SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
10. CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
11. PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	DOUBLE CROSSING CURB RETURN & RAMP DETAIL		RD-44
	<small>Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.</small>	SCALE	DATE
		N.T.S.	MAR '16



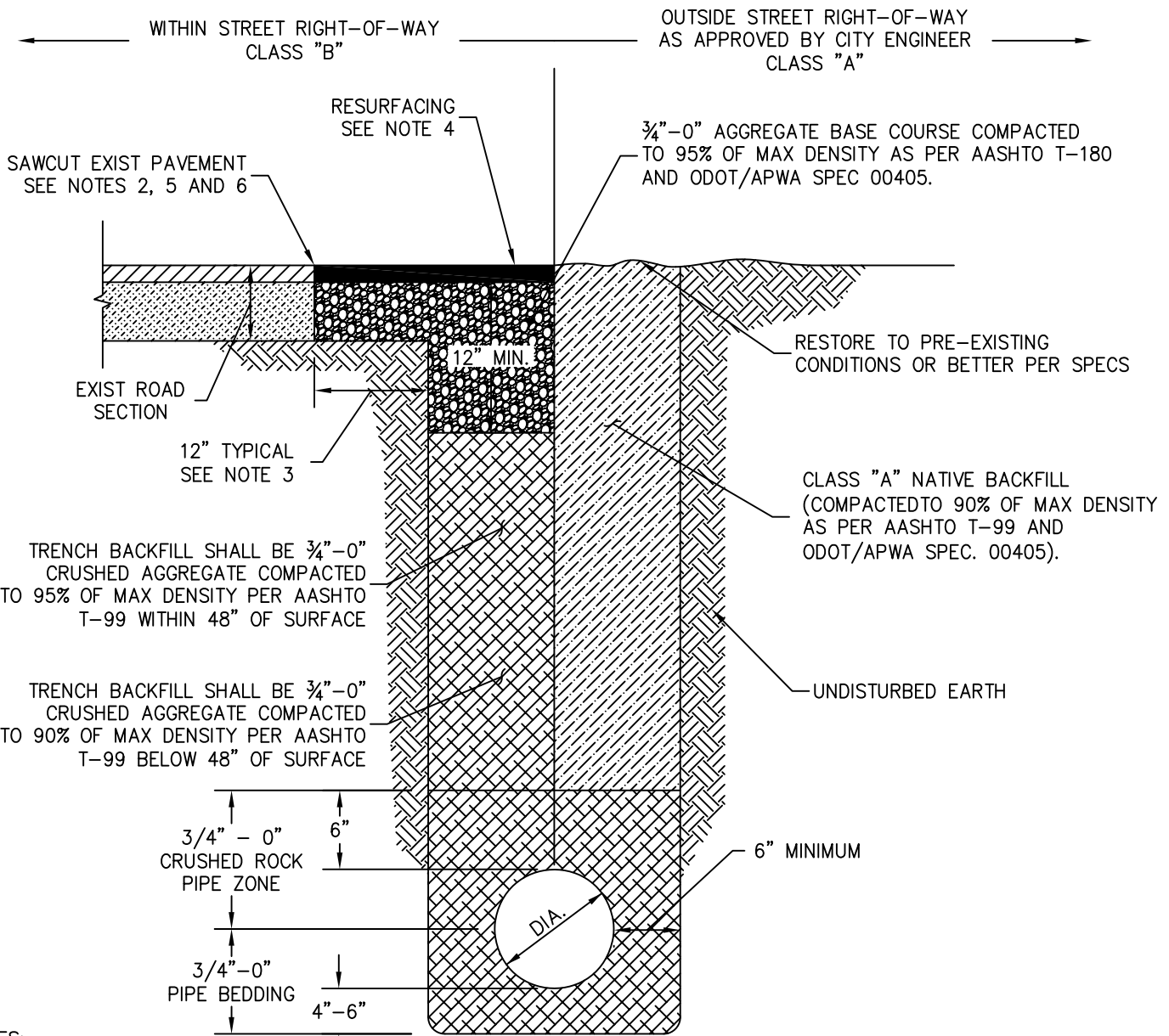
**CROSS-SECTION**

NOTES:

1. T-CUT IS 12" MINIMUM FOR TRENCHES WIDER THAN 12".
2. IF NEW EDGE OF PAVEMENT IS LESS THAN 3' FROM ANOTHER PATCH, CURB, EDGE OF STREET OR LONGITUDINAL CRACK, REPLACE THE PAVEMENT IN BETWEEN.
3. IF MORE THAN ONE EXISTING PATCH EDGE IS WITHIN THE 3' ZONE, REMOVE PAVEMENT TO THE FAR EDGE OF THE PRE-EXISTING PATCH.
4. NEW EDGE OF PAVEMENT (EDGE LINE) SHALL NOT LIE IN A WHEEL PATH. WIDTH OF T-CUT SHALL BE WIDENED WHERE NECESSARY TO MOVE THE EDGE LINE OUT OF THE WHEEL PATH.
5. SEE STD DET RD-20 FOR TYPICAL STREET PAVEMENT SECTION.
6. SEE STD DET RD-47 FOR TYPICAL TRENCH BACKFILL REQUIREMENTS.
7. ALL CUT EDGES AND COLD JOINTS SHALL BE SEALED WITH HOT RUBBERIZED ASPHALT CRACK SEALER CONFORMING TO ASTM D3405.
8. OVERCUTTING OF ASPHALT NOT ALLOWED.




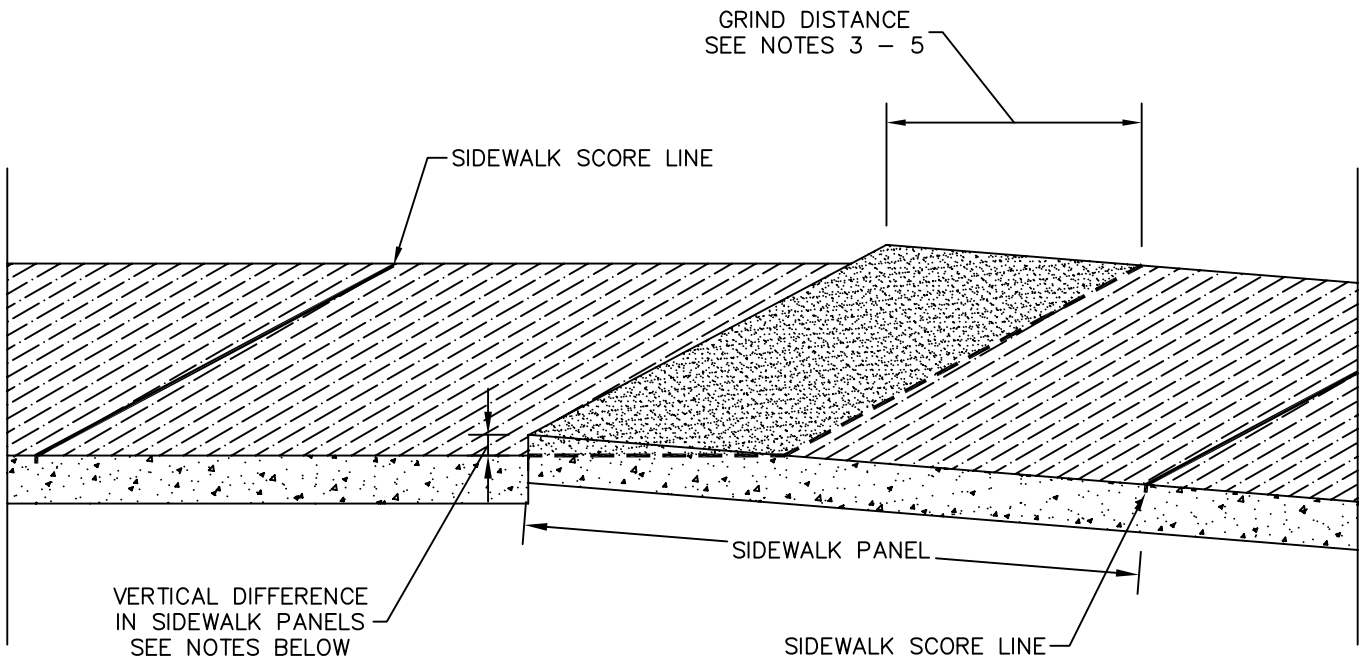
STANDARD DRAWING TITLE		DRAWING NUMBER
PIPE TRENCH RESTORATION		RD-45
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	2023.03.17



NOTES:

1. THESE TRENCH BACKFILL REQUIREMENTS APPLY TO ALL UTILITY PIPES. FOR ADDITIONAL REQUIREMENTS, SEE CITY STANDARD DESIGN MANUAL SECTION 210.19.
2. SAWCUT EXISTING HMAC PAVEMENT FULL DEPTH. SAWCUT EXISTING PCC PAVEMENT ACCORDING TO CITY STANDARD DETAILS.
3. 12" FOR TRENCHES WIDER THAN 12". 6" FOR TRENCHES LESS THAN 12".
4. MATCH EXISTING PAVEMENT MATERIAL(S). THICKNESS SHALL BE AS FOLLOWS:
  - 4.1. FOR EXISTING HMAC: RESURFACE TO A MINIMUM OF 3" OF LEVEL 2, 1/2" DENSE HMAC OR EXISTING AC THICKNESS PLUS 2", WHICHEVER IS GREATER, NOT TO EXCEED 6". COMPACT AC IN 2" MAX LIFTS TO 92% OF MAXIMUM DENSITY (RICE).
  - 4.2. FOR EXISTING PCC: EXISTING PAVEMENT THICKNESS PLUS 2", BUT NOT LESS THAN 8". ON ARTERIAL AND COLLECTOR STREETS, CONCRETE PATCHING MATERIAL SHALL BE HIGH EARLY STRENGTH CLASS 5000 PSI PCC APPROVED BY CITY ENGINEER.
5. ALL CUT EDGES AND COLD JOINTS OF AC SHALL BE SEALED WITH HOT RUBBERIZED ASPHALT CRACK SEALER CONFORMING TO ASTM D3405.
6. OVERCUTTING OF ASPHALT NOT ALLOWED.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	PIPE TRENCH BACKFILL		RD-47
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
N.T.S.		2023.03.14	

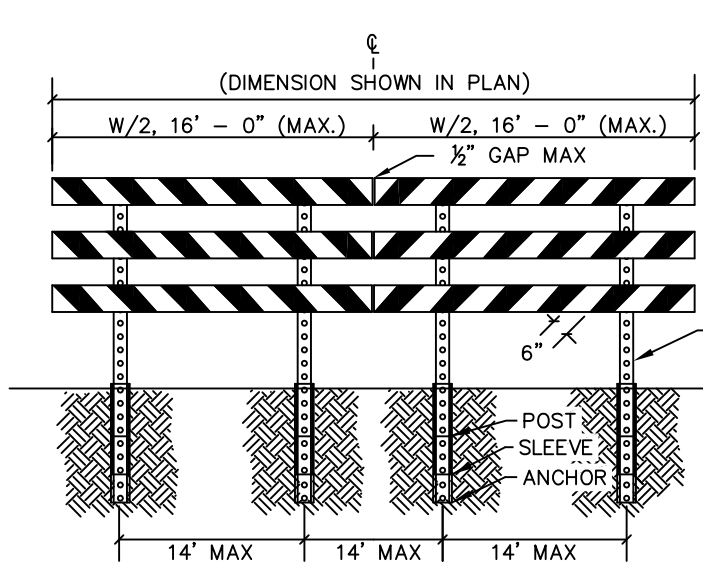


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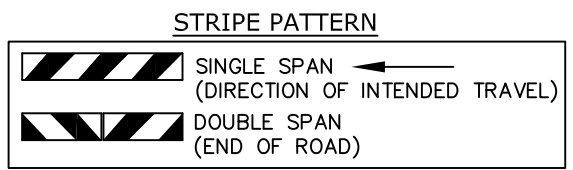
1. A SIDEWALK TRIP HAZARD EXISTS IF THERE IS A VERTICAL HEIGHT DIFFERENCE BETWEEN ADJACENT SIDEWALK PANEL SECTIONS.
2. IF THE SIDEWALK IS RAISED NOT MORE THAN ONE (1) INCH AND THE CONCRETE EDGES ARE SOLID, THE CONCRETE MAY BE GROUND TO REMOVE THE TRIP HAZARD.
3. FOR A TRIP HAZARD OF LESS THAN 1/2", GRIND BACK A MINIMUM OF SIX (6) INCHES.
4. FOR A TRIP HAZARD OF BETWEEN 1/2" AND 1", GRIND BACK A MINIMUM OF TWELVE (12) INCHES.
5. FOR A TRIP HAZARD OF MORE THAN 1", REMOVE AND REPLACE ENTIRE PANEL.



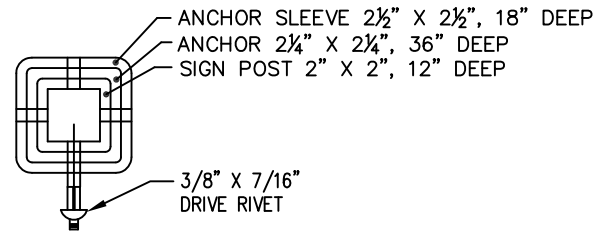
STANDARD DRAWING TITLE		DRAWING NUMBER
SIDEWALK GRIND DETAIL		RD-50
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



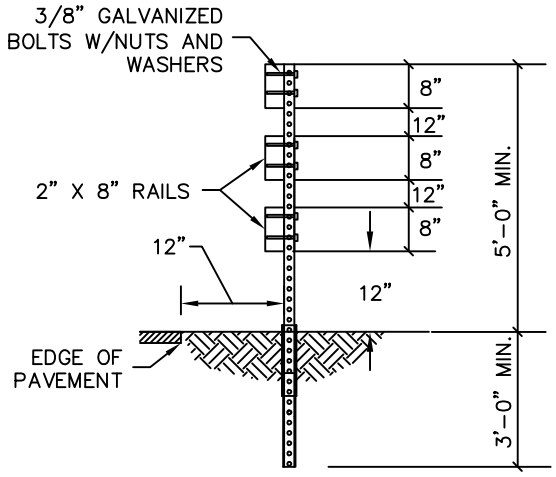
**ELEVATION**



A MINIMUM OF 2" X 2" X 6" GALVANIZED "UNISTRUT TELESAR" OR 12 GAUGE PERFORATED POSTS OR APPROVED EQUIVALENT SHALL BE USED.



**TYPICAL POST INSTALLATION**



**END VIEW**

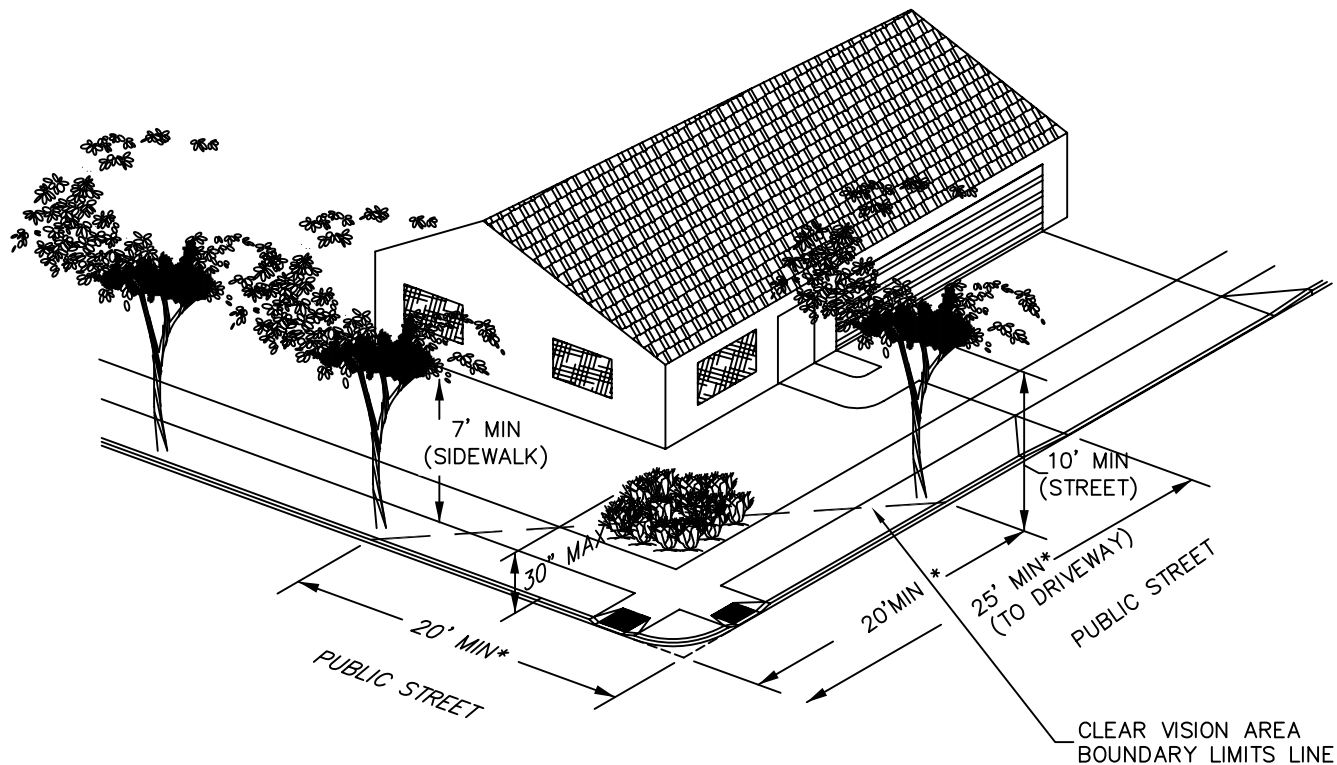
**NOTES:**

1. RAILS TO HAVE ALTERNATING RED AND WHITE RETROREFLECTIVE STRIPING.
2. SEE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE OREGON SUPPLEMENT.
3. FOR INSTALLATION IN A HARD SURFACE, A 4" I.D. PVC PIPE SHALL BE USED. THE ANNULAR SPACE BETWEEN THE SIGN POST PIPE AND THE PVC PIPE SHALL BE FILLED WITH COMPACTED SAND.
4. 1" PAN HEAD SHEET METAL SCREWS SHALL BE USED IF SHEET PANELS ARE TO BE FASTENED TO THE FACE OF THE CROSS-BOARDS.
5. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE CURRENT STATE OF OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION.
6. FOR APPLICATIONS SUCH AS AN END OF SIDEWALK BARRICADE, A HALF SECTION SHALL BE USED AND STRIPING ADJUSTED TO SLOPE DOWNWARD IN THE DIRECTION THE USER IS INTENDED TO TRAVEL.




STANDARD DRAWING TITLE		DRAWING NUMBER
TYPE III STREET BARRICADE DETAIL		RD-51
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

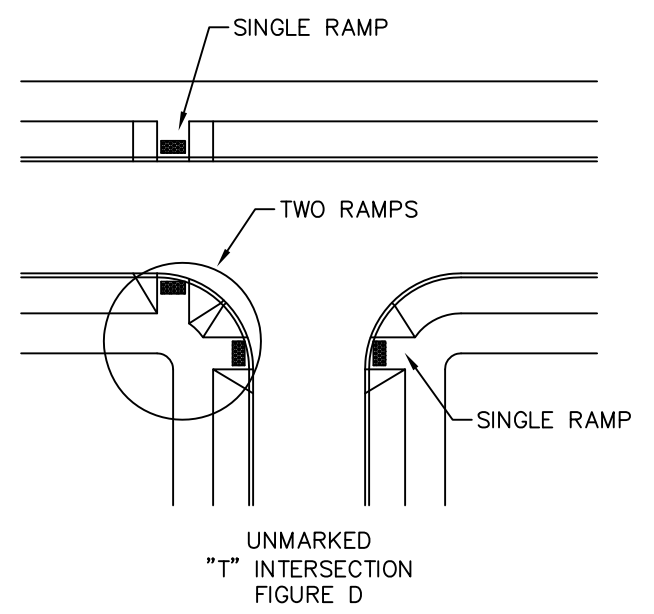
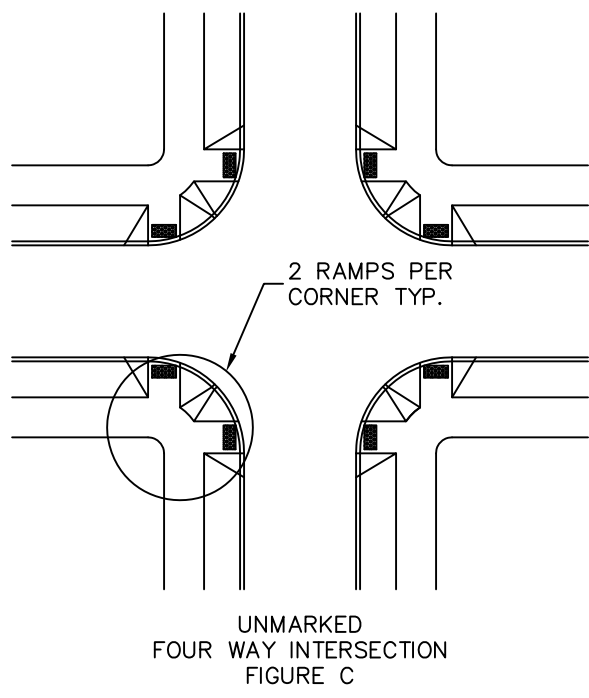
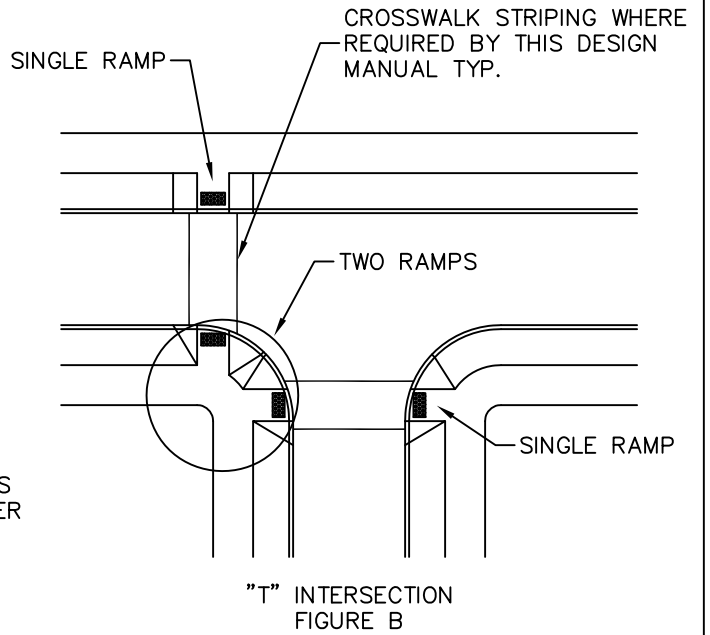
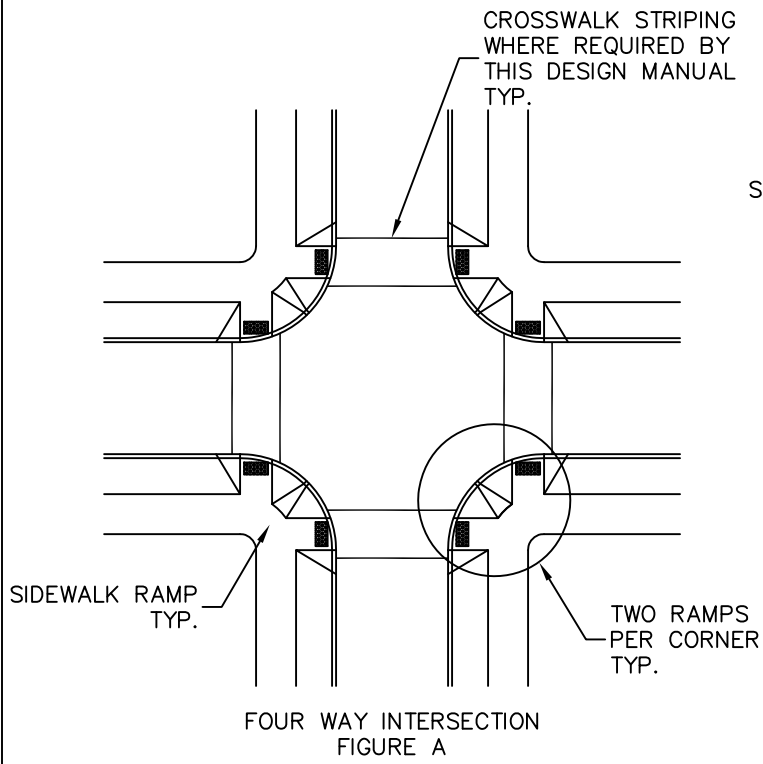




NOTES

1. CLEAR VISION AREA SHALL CONFORM TO THE REQUIREMENTS OF SECTION 210.5.1, "VISIBILITY AT INTERSECTIONS" OF THE ENGINEERING DESIGN AND STANDARD DETAILS MANUAL.
  2. NO MODIFICATIONS OR EXCEPTIONS TO THESE STANDARDS SHALL OCCUR UNLESS APPROVED IN WRITING BY THE CITY ENGINEER.
- \* THESE MEASUREMENTS VARY AS A FUNCTION OF STREET CLASSIFICATION AND ZONING. SEE REQUIREMENTS IN ENGINEERING DESIGN AND STANDARD DETAILS MANUAL.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	CLEAR VISION AREA DETAIL		RD-53
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16	

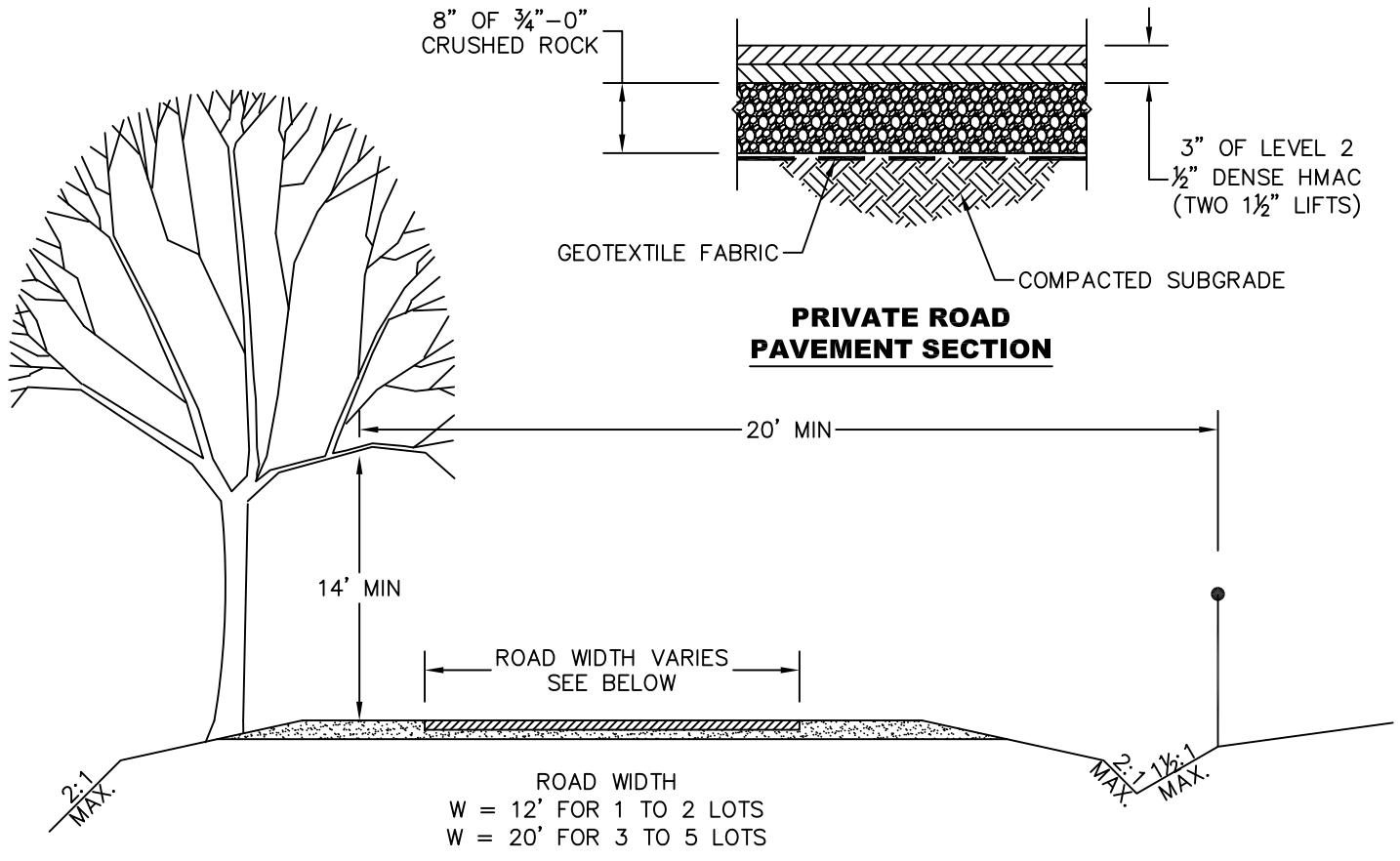


NOTES:

1. SIDEWALK RAMPS SHOWN ARE THE MINIMUM NUMBER OF RAMPS REQUIRED.
2. SEE STANDARD DRAWINGS FOR ADDITIONAL DETAILS OF RAMP DESIGN.




STANDARD DRAWING TITLE		DRAWING NUMBER
PEDESTRIAN RAMP CONFIGURATION		RD-55
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



**PRIVATE ROAD SECTION**


**NOTES**

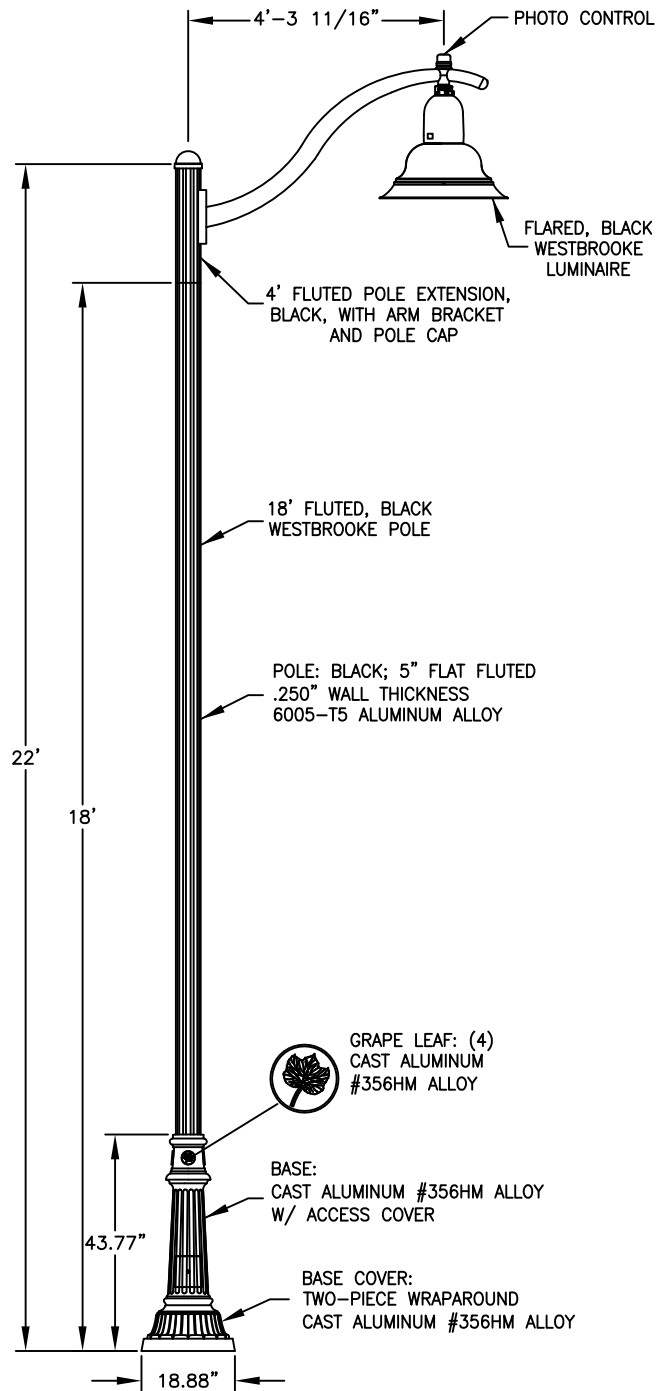
1. ALL WORK AND MATERIALS SHALL CONFORM TO CURRENT CITY STANDARDS.
2. PRIVATE ACCESS ROADS SERVING MORE THAN 5 LOTS SHALL CONFORM TO CITY STANDARDS FOR PUBLIC ROADS.
3. PRIVATE ACCESS ROADS WITH LENGTHS GREATER THAN 150 FEET SHALL INCLUDE AN APPROVED FIRE TRUCK TURN AROUND.
4. PRIVATE ACCESS ROADS WITH LENGTHS GREATER THAN 500 FEET SHALL HAVE A PAVED ROAD SURFACE WIDTH OF NOT LESS THAN 26 FEET.
5. PRIVATE ACCESS ROAD GRADES SHALL NOT EXCEED 10 PERCENT WITHOUT CITY ENGINEER AND TVF&R APPROVAL.
6. INTERSECTION GRADES OF PRIVATE ACCESS ROAD WITH PUBLIC ROADS SHALL NOT EXCEED 5 PERCENT, WITH THE EXCEPTION OF ROAD CROWNING FOR STORM WATER RUN-OFF. WHEN RESIDENCES ARE EQUIPPED WITH FIRE SPRINKLERS, PRIVATE ACCESS ROADS GRADE SHALL NOT EXCEED 15 PERCENT.
7. PRIVATE ACCESS ROADS SHALL HAVE AN UNOBSTRUCTED DRIVING SURFACE WIDTH OF 20 FEET (26 FEET ADJACENT TO FIRE HYDRANTS), AND AN UNOBSTRUCTED VERTICAL CLEARANCE OF NOT LESS THAN 13 FEET 6 INCHES.
8. THE INSIDE TURNING RADIUS AND OUTSIDE TURNING RADIUS FOR PRIVATE ACCESS ROADS SHALL NOT BE LESS THAN 28 FEET AND 48 FEET RESPECTIVELY.
9. PRIVATE ACCESS ROADS PAVED SURFACE SERVING TWO OR LESS DWELLING UNITS (INCLUDING OUT BUILDINGS), MAY BE REDUCED TO 12 FEET, ALTHOUGH THE UNOBSTRUCTED WIDTH SHALL BE NOT LESS THAN 20 FEET.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	PRIVATE ACCESS ROAD		RD-56A
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
		N.T.S.	MAR '16

NOTES (CONTINUED)

10. PRIVATE ACCESS ROADS SERVING BUILDINGS TALLER THAN 30 FEET SHALL BE CAPABLE OF USE BY AERIAL APPARATUS. THE UNOBSTRUCTED PAVED SURFACE WIDTH SHALL BE NOT LESS THAN 26 FEET. AERIAL ACCESS MUST BE PLACED PARALLEL ALONG AT LEAST ONE ENTIRE SIDE OF EACH BUILDING, BETWEEN 15 AND 30 FEET FROM THE BUILDING.
11. WHEN PRIVATE ACCESS ROADWAYS ARE NOT OF SUFFICIENT WIDTH TO ACCOMMODATE PARKED VEHICLES AND 20 FEET OF UNOBSTRUCTED PAVED DRIVING SURFACE, "NO PARKING – FIRE LANE" SIGNS SHALL BE INSTALLED ONE OR BOTH SIDES OF THE ROADWAY AND IN TURNAROUNDS AS NEEDED. PRIVATE ACCESS ROADS 26 FEET WIDE OR LESS SHALL BE POSTED ON BOTH SIDES AS A "FIRE LANE". PRIVATE ACCESS ROADS OF MORE THAN 26 FEET TO 32 FEET IN WIDTH SHALL BE POSTED ON ONE SIDE AS A "FIRE LANE".
12. DEVELOPERS OF PRIVATE ACCESS ROADS LESS THAN 32 FEET IN WIDTH SHALL RECORD A STREET MAINTENANCE AGREEMENT THAT PROVIDES FOR MAINTENANCE AND ENFORCEMENT OF PARKING RESTRICTIONS BY STREET OWNERS.

 <p>City of Sherwood Oregon</p>	STANDARD DRAWING TITLE		DRAWING NUMBER
	PRIVATE ACCESS ROAD NOTES		RD-56B
	<small>Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.</small>	SCALE	DATE
		N.T.S.	MAR '16



PREFERRED PRECAST FOOTING:  
UTILITY VAULT 20R-LB-4-PGE  
(CONFIRM BEFORE ORDERING)

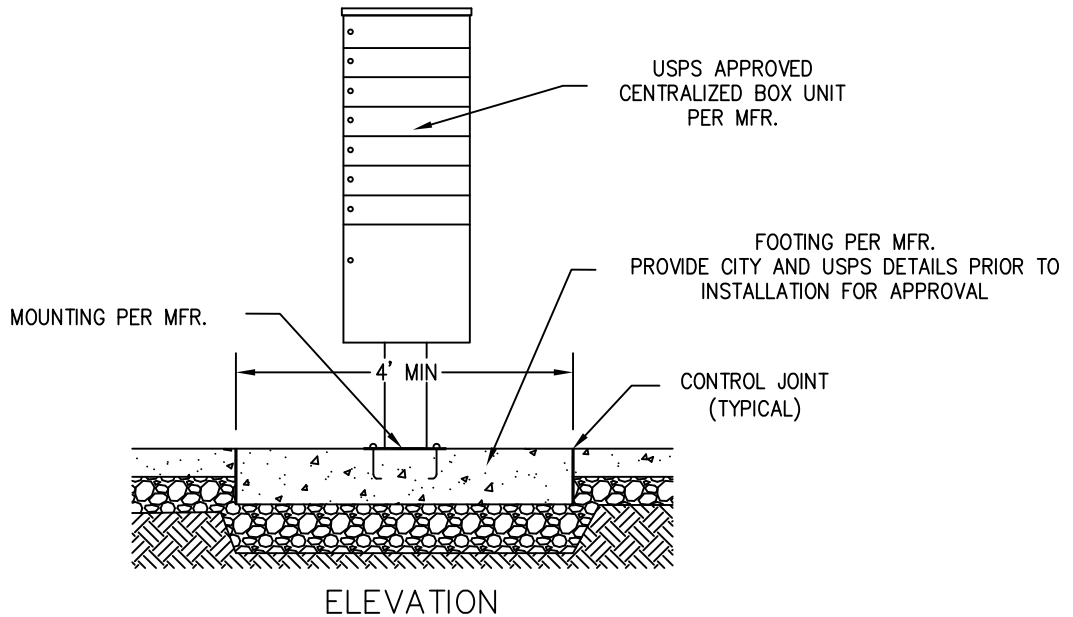
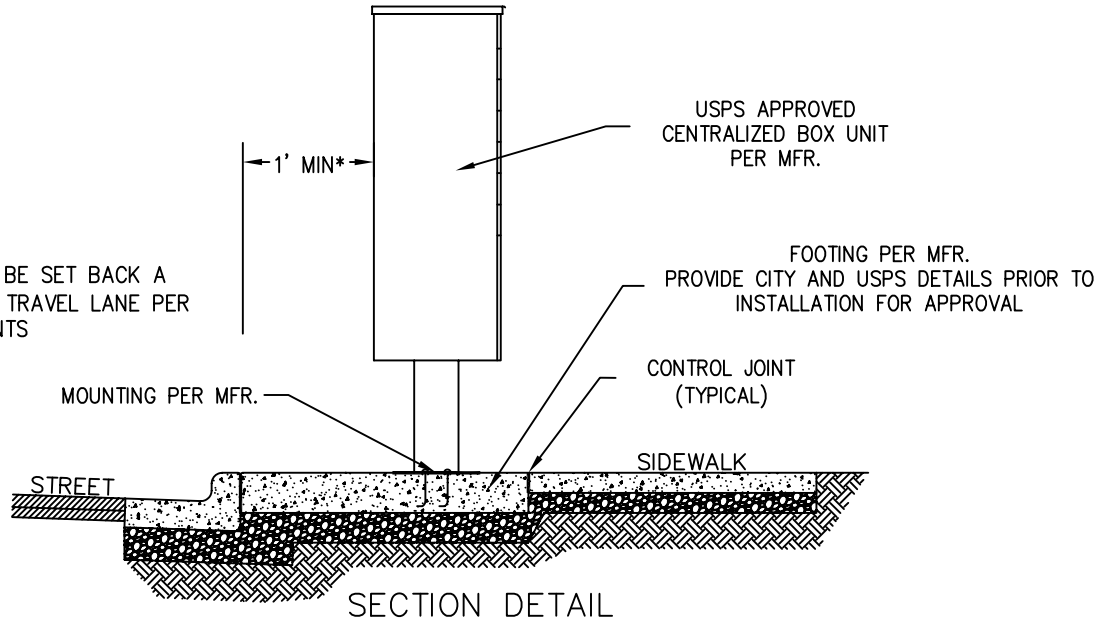
NOTES:

1. STREETLIGHTS SHALL BE OPTION "C".
2. PGE APPROVED STREET LIGHTING EQUIPMENT, LATEST EDITION, SHALL BE USED WHEN ORDERING LIGHTING COMPONENTS AND APPURTENANCES.
3. STREETLIGHT COMPONENTS AND INSTALLATION SHALL MEET PGE SPECIFICATIONS.
4. WESTBROOKE TO HAVE 18' BLACK-FLUTED POLE, 4' EXTENSION POLE, AND BLACK-FLARED LUMINAIRE.
5. STREETLIGHT SHALL BE CENTERED IN PLANTER STRIP.
6. LAMP WATTAGE AND TYPE SHALL BE DETERMINED BY PHOTOMETRIC DESIGN.
7. FOOTING TO BE PER PGE SPECIFICATIONS FOR POLE TYPE.
8. THIS DETAIL IS SCHEMATIC AND FOR REFERENCE ONLY. SPECIFICATIONS SHALL BE SUBMITTED TO THE CITY FOR APPROVAL PRIOR TO ORDERING OR INSTALLING STREETLIGHTING.



STANDARD DRAWING TITLE		DRAWING NUMBER
WESTBROOKE STREETLIGHT		RD-57
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	AUGUST '19

\* CBU MUST ALSO BE SET BACK A MIN. OF 2.5' FROM TRAVEL LANE PER MUTCD REQUIREMENTS

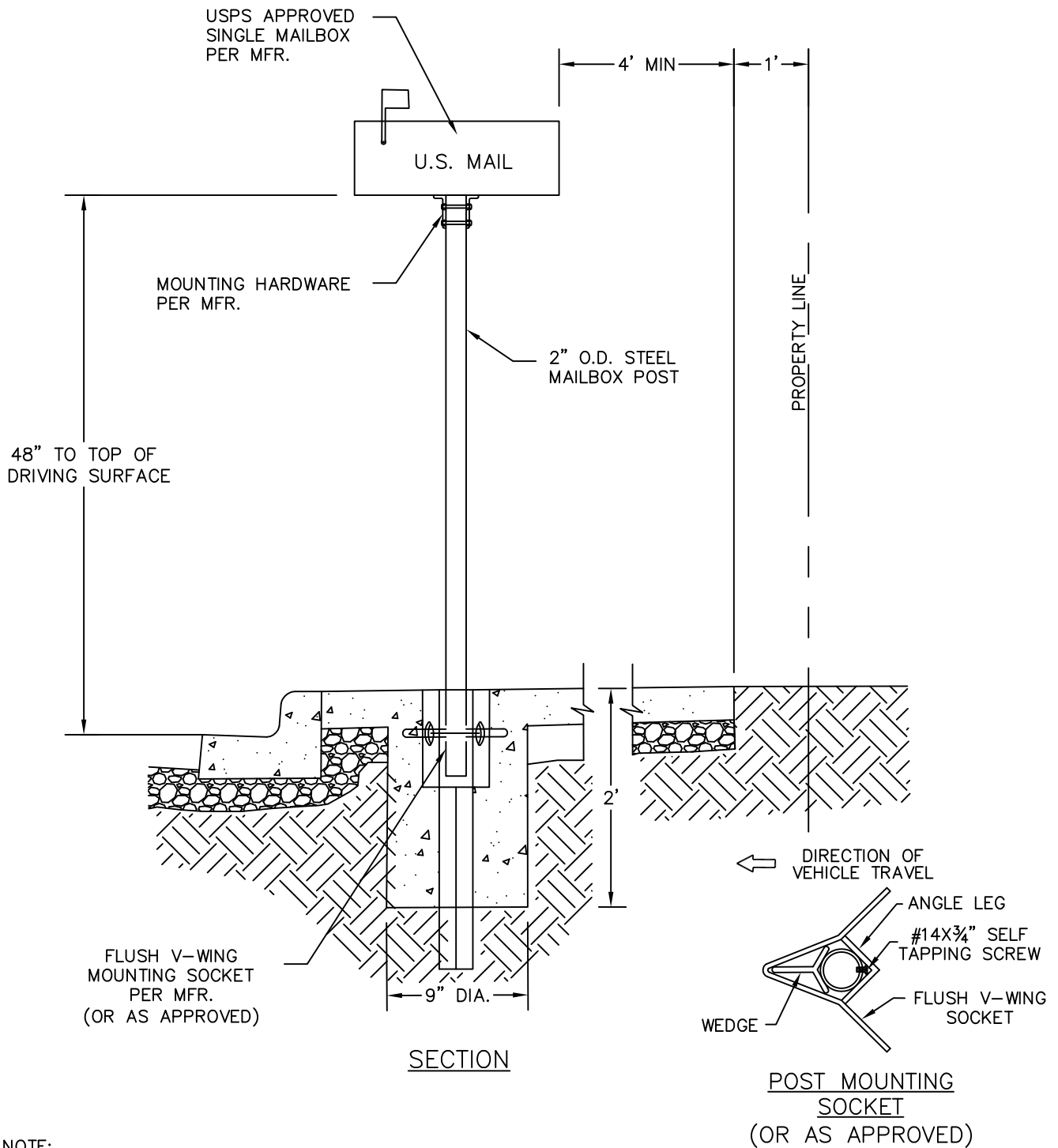


NOTE:

1. SHOULD A CENTRALIZED BOX UNIT (CBU) BE INSTALLED BEHIND A CURBTIGHT SIDEWALK, THE FRONT OF THE CBU SHALL NOT PROJECT BEHIND THE BACK OF SIDEWALK. A SIDEWALK CLEARANCE APRON SHALL BE INSTALLED PER PER RD-62.
2. SHOULD A CBU BE INSTALLED WITH THE FACE OF THE UNIT PERPENDICULAR TO SIDEWALK, SUCH AS IN A PLANTER STRIP, THE CONCRETE FOOTING SHALL BE INSTALLED THE FULL WIDTH OF THE PLANTER STRIP AND LONG ENOUGH TO PROVIDE USERS A MIN. 3' OF PAVED CLEARANCE IN FRONT OF AND 1' PAVED CLEARANCE BEHIND THE CBU TO PROVIDE USER AND USPS ACCESS.
3. MAILBOX PLACEMENT SPECIFICATIONS:
  - PLACEMENT AND INSTALLATION SHALL MEET ALL CURRENT CITY, USPS, MUTCD AND ADA GUIDELINES.
  - PLACEMENT SHALL BE APPROVED BY BOTH THE USPS AND CITY BEFORE INSTALLATION.
  - CBU SHOULD FACE SIDEWALK. CBU SHALL NOT PROTRUDE INTO THE VERTICAL PLANE OF THE SIDEWALK.
  - CBU SHALL NOT BE INSTALLED WITHIN THE LIMITS OF AN INTERSECTION SIGHT DISTANCE TRIANGLE.
4. WIDENED SIDEWALK CLEARANCE APRON OUTSIDE OF PUBLIC RIGHT OF WAY SHALL HAVE A SIDEWALK EASEMENT DEDICATED TO THE CITY OF SHERWOOD.
5. DEVELOPER SHALL REFERENCE THE LATEST EDITION OF THE USPS "DEVELOPERS GUIDE TO CENTRALIZED BOX UNITS" FOR CBU PLACEMENT GUIDELINES. IF MANUFACTURER'S SPECIFICATIONS FOR THE FOOTING AND MOUNTING GUIDELINES ARE NOT AVAILABLE, THE DEVELOPER SHALL REFER TO THE ABOVE MENTIONED USPS CBU GUIDE FOR GENERAL MOUNTING AND FOOTING SPECIFICATIONS.



STANDARD DRAWING TITLE		DRAWING NUMBER
MAILBOX - CENTRALIZED BOX UNITS		RD-60
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



**NOTE:**

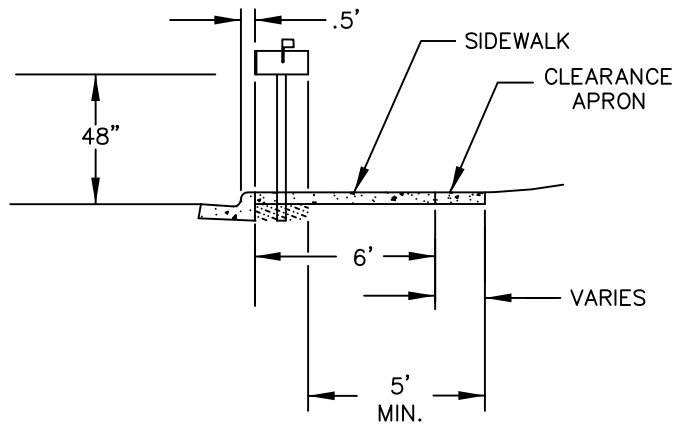
**1. MAILBOX PLACEMENT SPECIFICATIONS:**

- FRONT FACE OF MAILBOX TO BE SET BACK 6" FROM FACE OF CURB (FLUSH WITH BACK OF CURB).
- BASE OF MAILBOX TO BE 48" ABOVE HMAc PAVEMENT GRADE AT GUTTER (42" ABOVE TOP OF CURB).
- MAINTAIN 4' MIN CLEARANCE BETWEEN BACK OF SINGLE MAILBOX AND BACK OF WALK.
- PLACEMENT AND INSTALLATION SHALL MEET ALL CURRENT USPS, MUTCD AND ADA GUIDELINES.

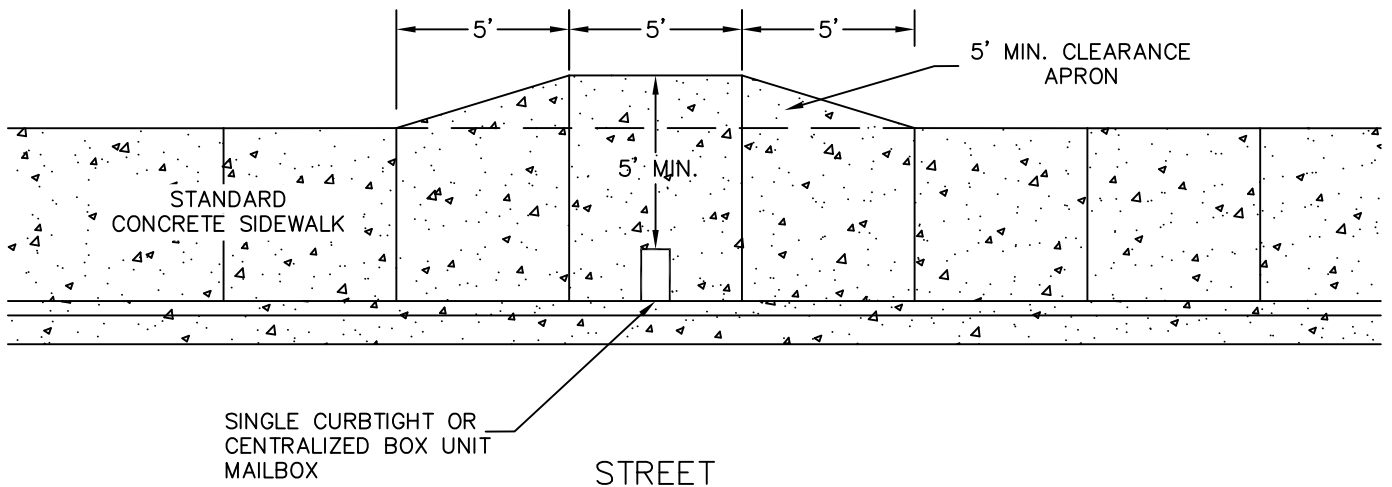
**2. WIDENED SIDEWALK OUTSIDE OF PUBLIC RIGHT OF WAY SHALL HAVE A SIDEWALK EASEMENT DEDICATED TO THE CITY OF SHERWOOD.**



STANDARD DRAWING TITLE		DRAWING NUMBER
MAILBOX - SINGLE UNIT		RD-61
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



PROFILE VIEW



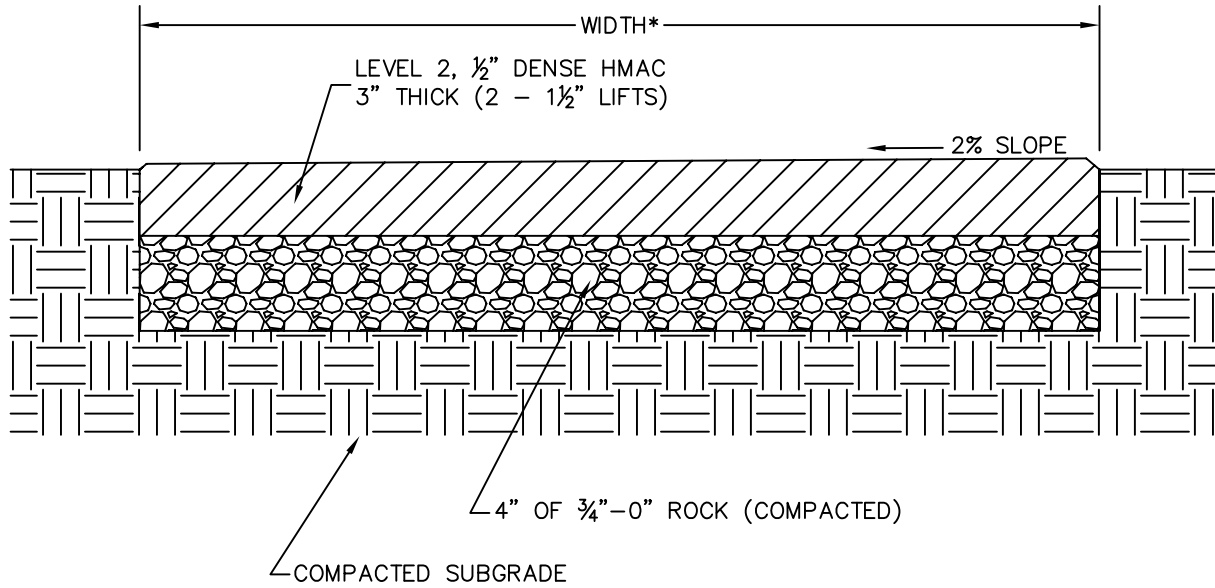
NOTE:

1. CONSTRUCT WIDENED SIDEWALK AT ALL MAILBOX LOCATIONS.
2. MAILBOX PLACEMENT SPECIFICATIONS:
  - FRONT FACE OF MAILBOX TO BE SET BACK 6" FROM FACE OF CURB (FLUSH WITH BACK OF CURB).
  - BASE OF MAILBOX TO BE 48" ABOVE HMAV PAVEMENT GRADE AT GUTTER (42" ABOVE TOP OF CURB).
  - MAINTAIN 5' CLEARANCE BETWEEN BACK OF MAILBOX AND BACK OF WALK
  - PLACEMENT AND INSTALLATION SHALL MEET ALL CURRENT USPS, MUTCD AND ADA GUIDELINES.
3. WIDENED SIDEWALK OUTSIDE OF PUBLIC RIGHT OF WAY SHALL HAVE A SIDEWALK EASEMENT DEDICATED TO THE CITY OF SHERWOOD.

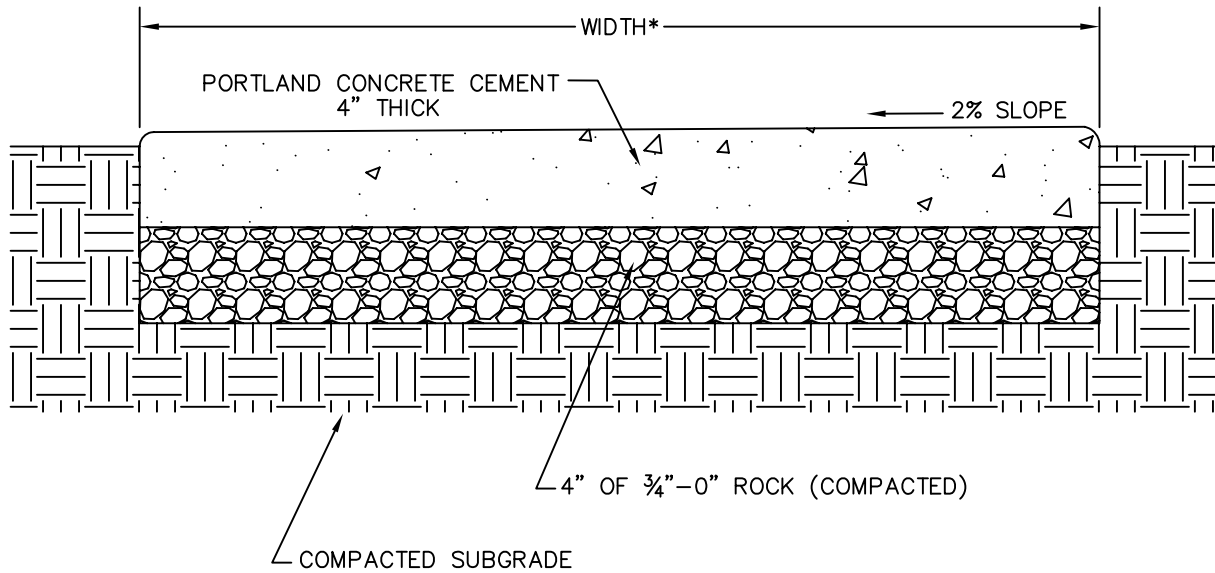


STANDARD DRAWING TITLE		DRAWING NUMBER
WIDENED SIDEWALK AT MAILBOX LOCATIONS		RD-62
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16





— OR —



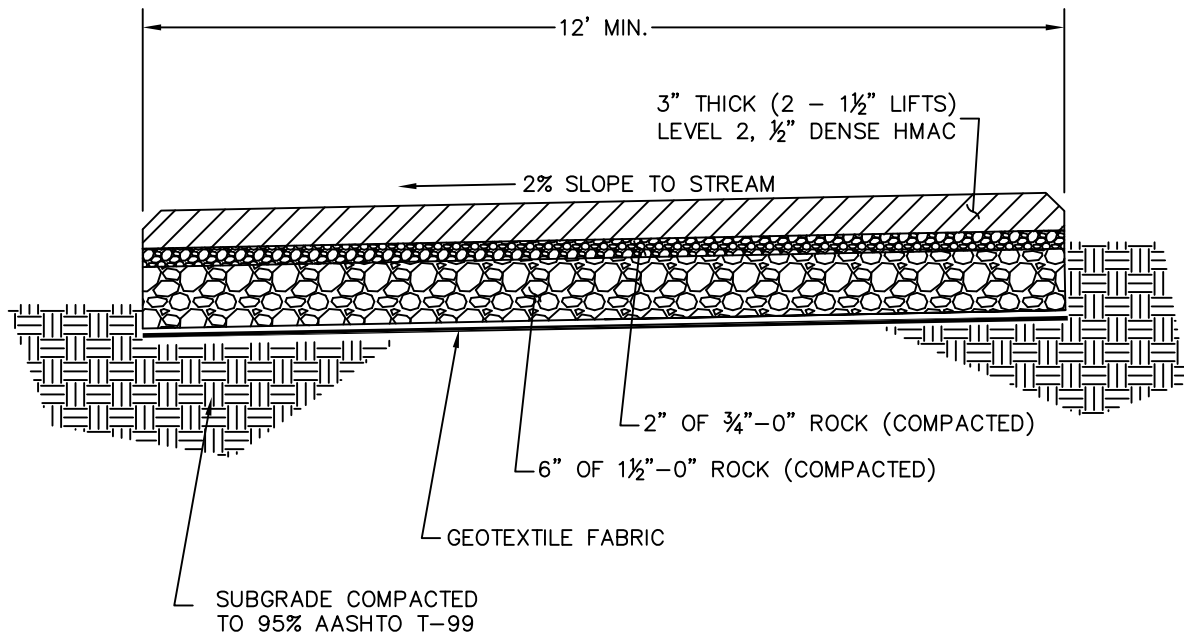
NOTES:

1. CONCRETE SHALL BE 3300 PSI AT 28 DAYS. HMAC LVL 2, 1/2" DENSE MIX SHALL MEET ODOT SPEC.
2. CONCRETE PATH PANELS SHALL BE SQUARE, HAVE 3/4" DEEP SCRIBES AT JOINTS, 3" EDGE SHINE ON 4 SIDES AND HAVE A LIGHT BROOM BRUSH FINISH.
3. PEDESTRIAN PATH OR BIKEWAY SHALL HAVE A MINIMUM WIDTH OF 7 FEET. \*

\*SEE FIGURE 8-6 OF THE TSP FOR TRAIL WIDTH STANDARDS



STANDARD DRAWING TITLE		DRAWING NUMBER
PEDESTRIAN PATH & BIKEWAY SECTIONS (NOT ADJACENT TO ROADWAY)		RD-63
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16



MAINTENANCE ACCESS ROAD

NOTES:

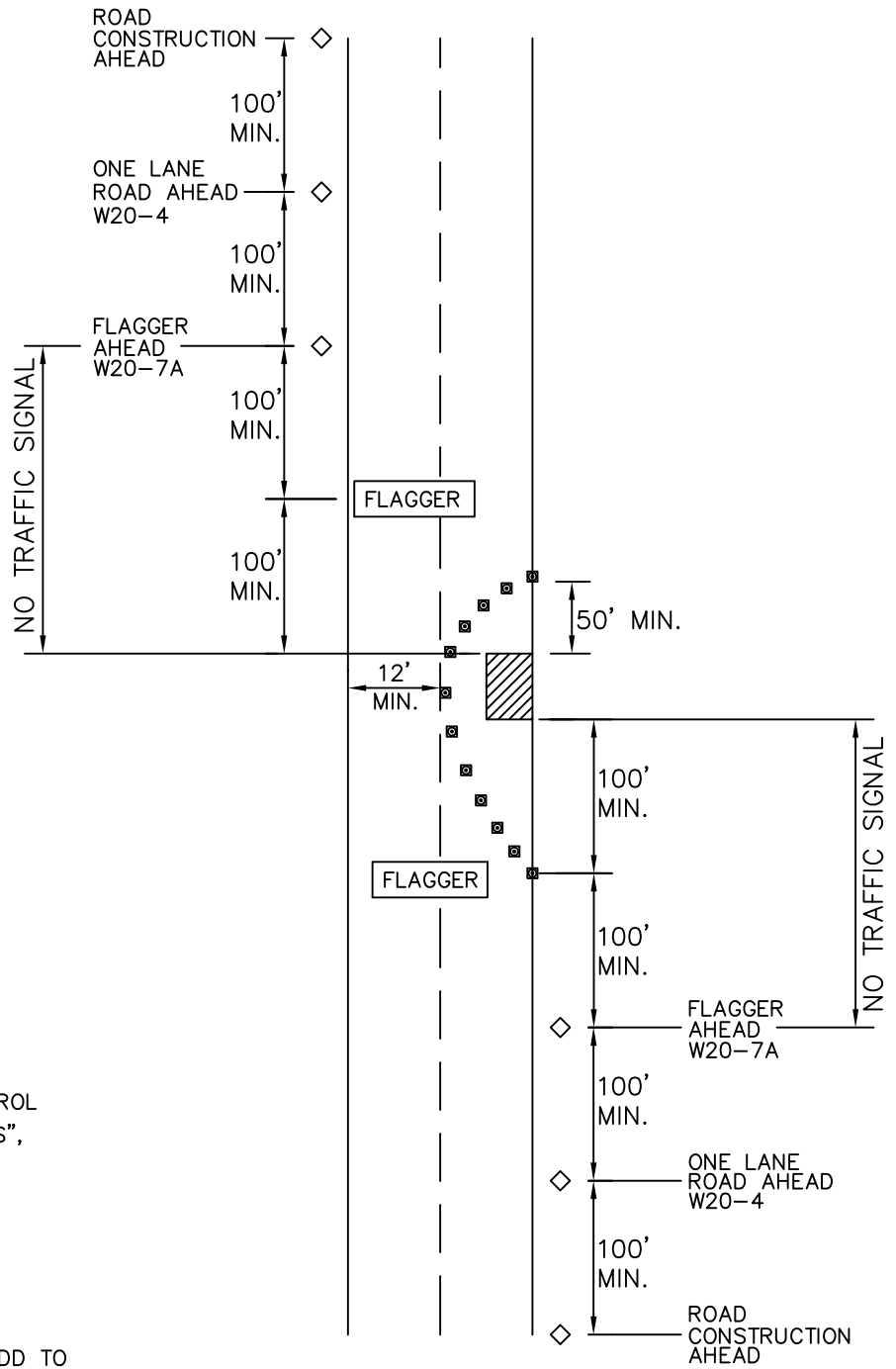
1. ALL WORK AND MATERIALS SHALL CONFORM TO CURRENT CITY STANDARDS.
2. FABRIC TO BE WOVEN GEOTEXTILE (AMOCO 2006) OR APPROVED EQUAL.
3. COMPACT AND STERILIZE SUBGRADE.
4. ACCESS SHALL EXTEND TO WITHIN 10 FT OF ALL STRUCTURES.



STANDARD DRAWING TITLE		DRAWING NUMBER
MAINTENANCE ACCESS ROAD SECTION		RD-64
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

LEGEND

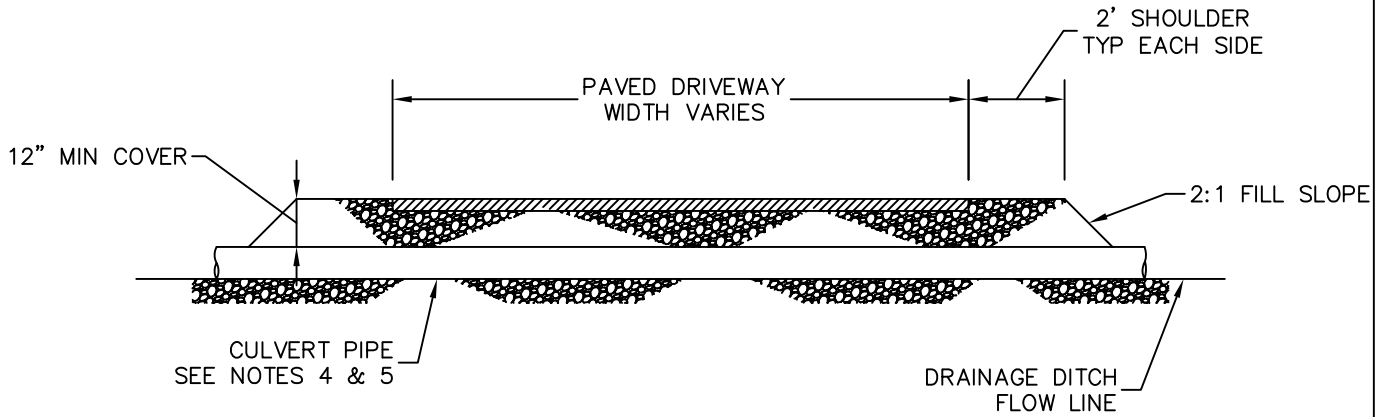
- ◇ TRAFFIC SIGN
- ▨ CONSTRUCTION ZONE
- ⊠ TRAFFIC CONES OR APPROPRIATE DELINEATION DEVICE.



1. TRAFFIC CONTROL PLANS REQUIRED.
2. TRAFFIC SIGNS TO BE 48" X 48" BLACK ON ORANGE.
3. NO COLLECTOR STREET LANE OR LOCAL STREET CLOSURES DURING THE FOLLOWING TIME PERIODS:  
  
7:00 – 9:00 A.M.  
3:30 – 6:00 P.M.
4. INSTALL IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", U.S. DEPT. OF TRANSPORTATION, LATEST EDITION.
5. ACTUAL SIGN PLACEMENT TO BE ADJUSTED IN FIELD.
6. THE CITY RESERVES THE RIGHT TO ADD TO OR MODIFY TRAFFIC CONTROL REQUIREMENTS AS MAY BE NECESSARY TO EFFECTIVELY CONTROL TRAFFIC AND TO ENSURE PUBLIC SAFETY.
7. NO LANE CLOSURES WITHIN 200 FEET OF A TRAFFIC SIGNAL WITHOUT PRIOR CITY APPROVAL.




STANDARD DRAWING TITLE		DRAWING NUMBER
TYPICAL TRAFFIC CONTROL PLAN		RD-70
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16

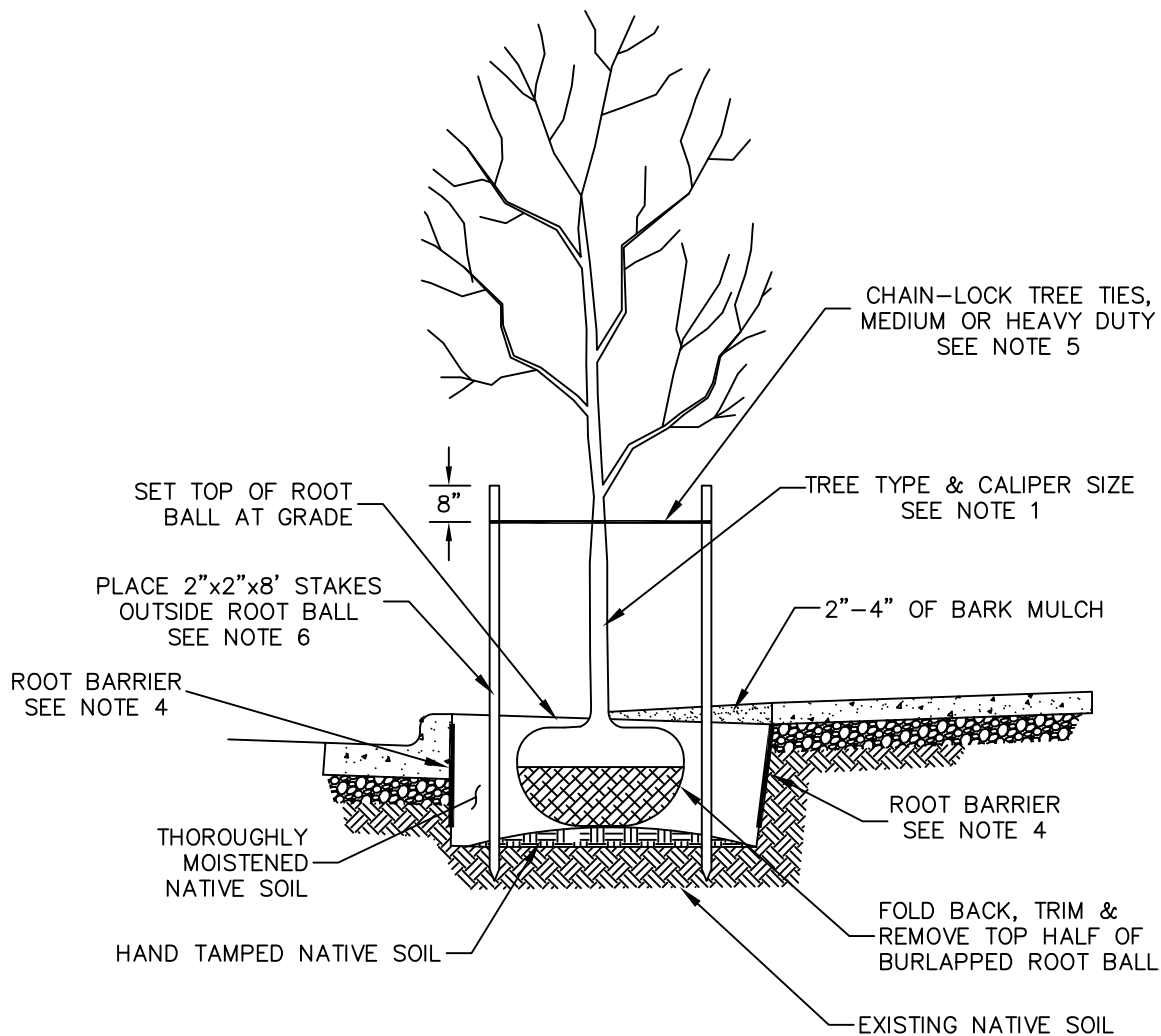


**DRIVEWAY CULVERT CROSSING SECTION**

NOTES

1. MINIMUM PIPE SIZE SHALL BE 8" DIAMETER. PIPE LENGTH VARIES, EXTEND PIPE 12" MINIMUM BEYOND TOE OF FILL SLOPE.
2. PROVIDE AN ADDITIONAL 3 FEET OF PIPE LENGTH, OR INSTALL A CONCRETE HEADWALL (6" THICK), WHEN USING CORRUGATED METAL PIPE.
3. MATERIALS USED IN CONSTRUCTION OF THE DRIVEWAY CULVERT CROSSING SHALL CONFORM TO CURRENT APWA/ODOT SPECIFICATIONS.
4. PIPE SHALL BE INSTALLED PER MANUFACTURERS SPECIFICATIONS, AND BE ABLE TO SUPPORT H-20 LOADING REQUIREMENTS.
5. ACCEPTABLE PIPE MATERIALS INCLUDE; CONCRETE, HDPE, DUCTILE IRON, CORRUGATED METAL. CORRUGATED STEEL METAL PIPE SHALL BE COATED.
6. PROVIDE A 3'x 3'x 12" DEEP CLASS 10 RIP-RAP AT PIPE OUTFALL WHEN CULVERT SLOPE IS GREATER THAN 5 PERCENT.

	STANDARD DRAWING TITLE		DRAWING NUMBER
	DRIVEWAY CULVERT DETAIL		RD-71
	Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
		N.T.S.	MAR '16



**CROSS SECTION**

NOTES:

1. TREE SPECIES AND CALIPER SIZE ARE TO BE APPROVED BY THE CITY PLANNING DEPARTMENT.
2. ADJUST PLANTING LOCATIONS SO THAT TREE CROWN OR ROOT BALL DOES NOT CONFLICT WITH ABOVE OR BELOW – GROUND UTILITIES.
3. DO NOT UNDERMINE CURB OR SIDEWALK WHEN EXCAVATING.
4. ALL TREES WITH TRUNKS CLOSER THAN 4' TO ASPHALT OR CONCRETE SURFACE SHALL HAVE A MINIMUM OF AN 18" ROOT BARRIER INSTALLED AT 10' IN LENGTH AND CENTERED ON TREE.
5. PROVIDE A LOOP IN CHAIN-LOCK LARGE ENOUGH TO ALLOW FOR TRUNK GROWTH.
6. TREE STAKES ARE TO BE REMOVED FOLLOWING THE REQUIRED ESTABLISHMENT PERIOD.



	STANDARD DRAWING TITLE	DRAWING NUMBER
	STREET TREE DETAIL	RD-80
Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.	SCALE	DATE
	N.T.S.	MAR '16