## CITY OF SHERWOOD

# PAVEMENT REHABILITATION (SUNSET, MEINECKE ROUNDABOUT, AND TIMBREL)

**APRIL 2024** 

(BID DOCUMENT VOLUME 2 OF 2 - CONTRACT DRAWINGS)

PROJECT LOCATION:
SW SUNSET BOULEVARD BETWEEN SW EUCALYPTUS TERRACE AND SW MAIN ST/SW LADD HILL RD

SW TIMBREL LN BETWEEN SW MIDDLETON RD AND SW SUNSET

MEINECKE RD. AND SW DEWEY DR

DEVELOPER/OWNER: CITY OF SHERWOOD 22550 SW PINE ST SHERWOOD, OREGON 97140 CONTACT: CRAIC CHRISTENSEN, P.E. PH. 503-925-2301 CHRISTENSENCOSHERWOODOREGON.GOV

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SW TUALATIN SHERWOOD RD **PROJECT** PROJECT VICINITY

THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIMSION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL (503) 246-6699.

#### VICINITY MAP

#### INSPECTOR INFORMATION:

CITY OF SHERWOOD: ANDY STIRLING (503) 925-2307 CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

#### SURVEY INFORMATION:

AERIAL TOPOGRAPHIC MAP (2004) AND VISUAL

AERIAL TOPOGRAPHIC MAP (2004)

#### GENERAL NOTES

- ALL CONSTRUCTION WORK, MATERIALS AND TESTING SHALL CONFORM TO THE APPLICABLE SECTION OF THE CITY OF SHERWOOD ENGINEERING DESIGN AND STANDARD DETAILS MANUAL CONTRACTOR AND SUBCONTRACTOR(S) SHALL HAVE A MINIMUM OF ONE SET OF APPROVED PLANS AND CITY OF SHERWOOD CONSTRUCTION SPECIFICATIONS ON THE JOB SITE AT
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROCURE ALL APPLICABLE PERMITS, LICENSES, AND CERTIFICATES RELATIVE TO THE TRADES TO COMPLETE THE PROJECT AND FOR THE USE OF SUCH WORK WHEN COMPLETED. COMPLIANCE SHALL BE AT ALL LEVELS, FEDERAL, STATE, COUNTY, AND LOCAL, RELATING TO THE PERFORMANCE OF THIS WORK. A CITY OF SHERWOOD OR A METRO BUSINESS LICENSE IS REQUIRED FOR THE CONTRACTOR AND ALL
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION AND ARRANGE FOR PROTECTION OF UTILITIES IN CONFLICT WITH PROPOSED WORK. THE LOCATIONS, DEPTH AND DESCRIPTION OF EXISTING UTILITIES SHOWN WERE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPILETENESS OF SUCH RECORDS. ADDITIONAL UTILITIES MAY EXIST WITHIN THE WORK AREA. POTHOLING PRIOR TO CONSTRUCTION MAY BE NECESSARY. ALL WORK TO VERIFY UTILITY LOCATIONS AND DEPTHS IS INCIDENTAL.
- OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT WWW.STATE OR US/GOVERNMENT HTM. THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS. BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL (503) 246-6699.
- THE CONTRACTOR SHALL MAKE PROVISIONS TO KEEP ALL EXISTING UTILITIES IN SERVICE AND PROTECT THEM DURING CONSTRUCTION. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGED UTILITIES USING MATERIAL AND METHODS APPROVED BY THE UTILITY OWNER. NO SERVICE INTERRUPTIONS SHALL BE PERMITTED WITHOUT PRIOR WRITTEN AGREFMENT WITH THE UTILITY PROVIDER AND NOTIFICATION TO SERVICE CUSTOMER. PLEASE NOTE THE STORM SEWER AND GAS LINE IN THIS AREA IS SHALLOW - DO NOT DAMAGE. REPAIR TO UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE
- THE CONTRACTOR SHALL PRUNE ALL VEGETATION, AS NECESSARY FOR EQUIPMENT CLEARANCES AWAY AND UP FROM THE STREET AND SIDEWALK. AS WELL AS ANY ROOT PRUNING AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING THAT IS TO REMAIN. DAMAGE TO TREES AND SHRUBS IS THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR. COST OF TREE PRUNING IS INCIDENTAL.
- CONTRACTOR SHALL NOTIFY THE CITY OF SHERWOOD 48 HOURS IN ADVANCE OF STARTING CONSTRUCTION AND 24 HOURS BEFORE RESUMING WORK AFTER SHUTDOWNS, EXCEPT FOR NORMAL RESUMPTION OF WORK FOLLOWING SATURDAYS,
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, EXCESS TOPSOIL, AND OTHER EXCESS MATERIAL OF SITE MATERIALS SHALL BE DISPOSED OF IN SUCH A MANNER AS TO MEET ALL APPLICABLE REGULATIONS, DISPOSAL LOCATION INFORMATION TO BE PROVIDED TO CITY PRIOR TO BEGINNING ANY WORK.
- CONSTRUCTION VEHICLES SHALL PARK AT A LOCATION APPROVED BY THE INSPECTOR OR AT A LOCATION(S) INDICATED ON AN APPROVED PLAN. HOURS OF CONSTRUCTION SHALL BE AS SPECIFIED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE INSPECTOR. CONSTRUCTION IS PROHIBITED ON SATURDAY AND SUNDAY UNLESS WRITTEN APPROVAL IS OBTAINED FROM THE PUBLIC WORKS DIRECTOR AND CITY MANAGER. CONSTRUCTION ACTIVITIES INCLUDE ALL FIELD MAINTENANCE OF EQUIPMENT, REFUELING, AND PICK UP AND DELIVERY OF EQUIPMENT AS WELL AS THE ACTUAL CONSTRUCTION ACTIVITY.
- 10. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN UP THE PROJECT AREA AND LEAVE IT IN A NEAT AND SECURED MANNER. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED
- THE CONTRACTOR SHALL KEEP AN APPROVED SET OF PLANS ON THE PROJECT SITE AT ALL TIMES WITH ALL KNOWN CHANGES REDLINED ON THE WORKING PLAN SET FOR THE SOLE PURPOSE OF RECORDING AS BUILT INFORMATION OF IMPROVEMENTS AND LOCATION AND DEPTH OF ANY EXISTING UTILITIES ENCOUNTERED.
- ANY ALTERATIONS OR VARIATIONS FROM THESE PLANS EXCEPT MINOR FIELD ADJUSTMENTS NEEDED TO MEET EXISTING FIELD CONDITIONS, SHALL BE APPROVED BY THE ENGINEER AND APPLICABLE REGULATORY AGENCY REPRESENTATIVE. CONTRACTOR SHALL KEEP RECORD OF ALL CHANGES AND NOTE ON AS BUILT PLANS. CONTRACTOR SHALL SUBMIT ACCURATE AND COMPLETE "AS-BUILT" INFORMATION TO THE ENGINEER UPON COMPLETION OF CONSTRUCTION.
- 13. THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THIS PROJECT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET THE INTENT OF THE PROJECT ACT DOCUMENTS, APPLICABLE AGENCY REQUIREMENTS AND OTHER WORK AS NECESSARY TO PROVIDE A COMPLETE
- CONTRACTOR SHALL PROVIDE EFFECTIVE EROSION PROTECTION TO INCLUDE, BUT NOT BE LIMITED TO, GRADING, DITCHING, STRAW WAITLES, TRIANGULAR SILT DAMS, SILT FENCING, AND SEDIMENT BARRIERS TO MINIMIZE EROSION AND IMPACT TO ADJACENT PROPERTY. SEE EROSION AND SEDIMENT CONTROL NOTES AND PLANS.
- THE CONTRACTOR SHALL MAINTAIN AND COORDINATE ACCESS TO ALL AFFECTED PROPERTIES. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS A MINIMUM OF 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURES AND ASSIST PROPERTY OWNER WITH ALTERNATIVE PARKING AND ACCESS. PEDESTRIAN ACCESS TO ENTRANCES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL PROVIDE RAMPS OR OTHER APPROVED METHODS FOR MAINTAININING ACCESS TO ENTRANCES. RAMPS SHALL BE ADA COMPLIANT, APPROVED BY THE ENGINEER, AND INCIDENTAL TO THE CONTRACT. RESIDENTIAL DRIVEWAYS MAY ONLY BE CLOSED FOR A MAXIMUM OF ONE DAY (8:00 AM TO 5:00 PM).
- 16. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND VERIFY ALL EXISTING CONDITIONS BEFORE THE START OF WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY FIELD MEASUREMENTS AND OTHERWISE VERIFY ALL DIMENSIONS AND EXISTING CONSTRUCTION CONDITIONS INDICATED AND/OR SHOWN ON THE PLANS. SHOULD ANY ERROR OR INCONSISTENCY EXIST, THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK AFFECTED UNTIL REPORTED TO THE PROJECT ENGINEER
- ANY INSPECTION BY THE CITY, COUNTY, STATE, FEDERAL AGENCY OR PROJECT ENGINEER SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN COMPLIANCE WITH THE APPLICABLE CODES, REGULATIONS, CITY STANDARDS AND PROJECT CONTRACT DOCUMENTS.
- 18. CONTRACTOR TO VERIFY CENTERLINE AND GUTTER SLOPES PRIOR TO CONSTRUCTION TO ENSURE COMPLIANCE WITH THE CONSTRUCTION DRAWINGS AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN CONFORMING TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION AND ACCORDING TO PROJECT SPECIFICATIONS. TRAFFIC CONTROL PLAN SHALL MATCH APPROVED PHASING/SEQUENCING PLAN AND SHALL BE SUBJECT TO THE APPROVAL BY THE CITY, WASHINGTON COUNTY AND/OR ODOT AS APPLICABLE.
- 20. THE CONTRACTOR SHALL PROVIDE TO THE CITY PROJECT MANAGER AND INSPECTOR A 24 HOUR CONTACT PERSON AND
- NO TRENCHES OR PITS WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT. ALL TRENCHES AND PITS SHALL BE COVERED WITH STEEL PLATES OR FILLED IN AT NIGHT.
- 22. PROPERTY LINES SHOWN ON ALL PLAN SHEETS ARE FOR GENERAL DELINEATION ONLY AND ARE, BY NO MEANS, MEANT TO

- 23. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL CONSTRUCTION SAFETY, HEALTH AND OTHER RULES AND REGULATIONS FROM OSHA, DEQ, STATE, AND LOCAL REGULATING AGENCIES FOR SAFETY AND INSTALLATION OF THE WORK INCLUDING BUT NOT LIMITED TO SHORING, BRACING, ERECTION/INSTALLATION, FALL PROTECTION, GUARDRAILS, ETC.
- 24. IT IS THE CONTRACTOR'S RESPONSIBILITY TO REMOVE EXISTING AGGREGATE BASE AS NECESSARY TO CEMENT TREAT BASE AND ACHIEVE SURFACE BASE LEVEL FOR PLACEMENT OF NEW HMAC WHILE MEETING THE CURB AND DRIVEWAY EXPOSURE REQUIREMENTS IN THE PLANS.
- WHEN MATCHING EXISTING PAVEMENT, SAWCUT TO FIRM PAVEMENT. MINIMUM SAWCUT IS 1' FROM EXISTING EDGE OF PAVEMENT OR
  AS SHOWN ON PLANS. SAWCUT LOCATION TO BE APPROVED BY INSPECTOR.
- CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING STRUCTURES AND UTILITIES NOT SHOWN TO BE REMOVED. CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING STRUCTURES (SIDEWALKS, DRIVEWAYS, CURB, FENCE, STREET TREES, ETC.) DAMAGED DURING CONSTRUCTION, IN ACCORDANCE WITH CITY STANDARDS.
- ALL LINEWORK SHOWN ON THE PLAN IS FROM A 2004 AERIAL MAP AND FROM VISUAL OBSERVATION AND IS THEREFORE NOT GUARANTEED FOR ACCURACY AND SHOULD NOT BE USED FOR OTHER PURPOSES.

## **EROSION CONTROL NOTES**

- THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- 2. THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION /LANDSCAPING IS ESTABLISHED.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED AFTER PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM
- STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION
- IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH MAY 31ST, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL (CHAPTER 4) OF CLEAN WATER SERVICES FOR REQUIREMENTS.
- 9. THE CONTRACTOR SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
- 10. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (FILTER BAG, DIRT BAG, ETC.)
- 11. SITE EROSION CONTROL PLAN AND BMP'S MEETING CWS STANDARDS TO BE IN PLACE AND APPROVED PRIOR TO CONSTRUCTION
- TRACKING OF MATERIALS OUTSIDE OF THE WORK ZONE, ONTO ADJACENT STREETS, SHALL BE THE RESPONSIBILITY OF THE
  CONTRACTOR TO REMOVE AND CLEAN TO THE SATISFACTION OF THE INSPECTOR. THIS INCLUDES BUT IS NOT LIMITED TO SOIL,
  AGGREGATE, ASPHALT DEBRIS, ETC.
- 13. CONTRACTOR TO PROVIDE FOR CONCRETE WASHOUT. SUBMIT PLAN FOR CONCRETE WASHOUT TO INSPECTOR FOR APPROVAL

## WORK HOURS/TRAFFIC CONTROL NOTES

- 1. WORK HOURS ARE FROM 8:00AM TO 6:00PM MONDAY THROUGH FRIDAY FOR SW SUNSET AND SW TIMBREL. WORK HOURS FOR MEINECKE RAB ARE FROM 9:00AM TO 4:00PM FOR MEDIAN REMOVAL AND RESTORATION, AND 6:30PM THROUGH 11PM FOR GRIND AND INLAY.
- SW SUNSET BOULEVARD SHALL HAVE TWO LANES OPEN TO TRAFFIC AT ALL TIMES, FLAGGERS SHALL BE PROVIDED AS NEEDED TO MAINTAIN FUNCTIONING OF INTERSECTIONS. LANE CLOSURE MAY ONLY OCCUR BETWEEN THE HOURS OF 8:30AM TO 4:30PM.
- 3. MEINECKE ROUNDABOUT GRIND AND INLAY SHALL BE CONDUCTED IN SEPARATE STAGES TO ALLOW FOR TRAFFIC FLOW THROUGH THE ROUNDABOUT DURING CONSTRUCTION, MEDIAN REMOVAL WILL OCCUR DURING THE DAYTIME PRIOR TO GRIND AND INLAY, AND TEMPORARY FILL MUST BE ADDED FOR TRAFFIC CROSSING, MEDIAN WILL NEED TRAFFIC CONTROL MEASURES TO BLOCK OFF WHEN NOT IN USE AS A CROSSOVER. EXISTING IRRIGATION MUST BE PROTECTED, DO NOT DAMAGE EXISTING IRRIGATION, COORDINATE IRRIGATION SHUT OFF WITH CITY, ANY DAMAGED IRRIGATION MUST BE REPLACED TO CITY STANDARDS BY CONTRACTOR, GRINDING WILL OCCUR AT NIGHTIME IN THREE SEPARATE STAGES TO ALLOW FOR TRAFFIC FLOW THROUGH THE ROUNDABOUT DURING CONSTRUCTION. PAVING WILL ALSO OCCUR AT NIGHTIME IN THREE SEPARATE STAGES TO ALLOW FOR TRAFFIC FLOW THROUGH THE ROUNDABOUT DURING CONSTRUCTION. MEDIAN RESTORATION WILL OCCUR AFTER PAVING HAS BEEN COMPLETED. FLAGGERS SHALL BE POSTED AT ALL TRAFFIC CHANGE LOCATIONS AT ALL TIMES. FLAGGERS LIKELY NEEDED FOR MEDIAN REMOVAL/RESTORATION. DO NOT HOLD TRAFFIC FOR MORE THAN 5 MINUTES. TRAFFIC SHALL NOT BACK UP INTO PACIFIC HIGHWAY, ANY SIGNAGE WITHIN PACIFIC HIGHWAY RIGHT-OF-WAY WILL REQUIRE A PERMIT FROM ODOT. ANY SIGNAGE WITHIN SW ELMERT ROAD RIGHT-OF-WAY WILL REQUIRE A PERMIT FROM WASHINGTON COUNTY. PERMITS ARE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR TO COORDINATE TIMING
- 4. SW TIMBREL LANE SHALL HAVE ONE LANE OPEN TO TRAFFIC AT ALL TIMES VIA FLAGGERS, LANE CLOSURE MAY ONLY OCCUR BETWEEN THE
- 5. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EMERGENCY VEHICLE ACCESS TO ALL PROPERTIES AT ALL TIMES.
- 6. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS FOR MAIL AND TRASH (FRIDAY) AT ALL TIMES.
- TRAFFIC CONTROL SHALL INCLUDE ALL PAYEMENT MARKING REMOVAL, TEMPORARY PAYEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAYEMENT MARKINGS AND ALL INCEDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".
- B. CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION OF A TRAFFIC CONTROL PLAN AND ITS CONTINUED FUNCTIONING FOR THE PROTECTION OF CONSTRUCTION WORKERS, VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIANS. ALL TRAFFIC CONTROL DEVICES/SIGNAGE SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. APPROVAL OF THE TRAFFIC CONTROL PLAN BY THE CITY OF SHERWOOD DOES NOT NEGATE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN A SAFE WORK ZONE. THE CITY OF SHERWOOD BEARS NO LIABILITY FOR THE CONTRACTOR'S IMPLEMENTATION OF A TRAFFIC CONTROL PLAN.
- 9. SIDEWALK RAMP UPGRADES SHALL BE COMPLETED IN SEPARATE STAGES TO ALLOW FOR PEDESTRIAN PASSAGE.
- 10. CONTRACTOR TO SEND OUT WRITTEN NEIGHBORHOOD NOTICES TO ADJACENT RESIDENCES WITH DATES OF GRINDING AND ASPHALT WORK, CONTACT ENGINEER FOR NOTICE LIMITS AND WRITTEN NOTICE APPROVAL.

#### STREET NOTES

- 1. IN AREAS WHERE CURB AND GUTTER EXISTS, NEW ASPHALT WILL MATCH FRONT EDGE OF CONCRETE GUTTER,
- TACK COAT SHALL BE APPLIED AGAINST CONCRETE GUTTER EDGE, CURB FACE, CONCRETE STRUCTURES, EXISTING ASPHAL EDGE, MANHOLE FRAMES, BLOW-OFF FRAMES, CLEANOUT FRAMES AND OTHER SURFACES OR STRUCTURES THAT WILL BE PAVED AGAINST WITH NEW ASPHALT (INCIDENTAL) PRIOR TO PAVING.
- HOT RUBBERIZED ASPHALT SEALER REQUIRED AT ASPHALT JOINTS (INCIDENTAL). DO NOT OVERCUT ASPHALT. CITY HAS THE RIGHT TO HAVE OVERCUT ASPHALT REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
- 4. PAVEMENT CROSS SLOPES SHALL GENERALLY MATCH EXISTING CROSS SLOPES.
- NEW ASPHALT SHALL NOT COVER ANY WEEP HOLES (AT I.E. OR LOWER). CITY WILL PHOTO DOCUMENT PROJECT AREA.
   INCLUDING EXISTING CURBS. COVERED WEEPHOLES SHALL BE CLEARED AT CONTRACTORS EXPENSE AND STREET REPAYED.
- 6. TYPICAL FULL CURB EXPOSURE IS 6" AT TOP FACE OF CURB UNLESS OTHERWISE NOTED
- TYPICAL DRIVEWAY CURB EXPOSURE IS 3/4" AT TOP FACE OF CURB.
- ADJUST ALL EXISTING VALVES, BLOW-OFFS AND OTHER MISCELLANEOUS BOXES AS NECESSARY TO MATCH NEW ASPHALT SURFACE GRADE TO BE PAID FOR UNDER THE BID ITEM OF "ADJUSTING BOXES".
- THERE WILL BE NO ADJUSTMENT OF EXISTING MANHOLE FRAMES FOR THIS PROJECT. NEW ASPHALT TO MATCH EXISTING MANHOLE COVERS.
- 10. CONTRACTOR TO TEST ASPHALT SURFACE WITH 12-FOOT STRANGHT EDGE IN TRAVEL UNIES PARALLEL TO AND PERPENDICULAR TO THE CENTER LINE, AS DIRECTED BY THE INSPECTOR. THE ASPHALT SURFACE SHALL NOT VARY BY MORE THAN 1/4 NOTH INCLUDING WANHOLE COVER.
- 11. WATER FOR CONSTRUCTION MAY BE OBTAINED FROM CITY FIRE HYDRANTS. INSTALL FIRE HYDRANT METER PRIOR TO DRAWING WATER. FIRE HYDRANT METER TO BE OBTAINED FROM CITY OF SHERWOOD WATER DEPARTMENT, COORDINATE WITH INSPECTOR. WATER FOR CONSTRUCTION IS PAID FOR BY THE CONTRACTOR AND CONSIDERED INCIDENTAL.
- 12. REPAIR ALL DISTURBED NEIGHBORING PROPERTY BACK TO ORIGINAL OR BETTER CONDITION.
- 13. ANY SEGMENT OF STREET OPEN TO TRAFFIC THAT HAS A VERTICAL DROP (INCLUDING DRIVEWAYS) SHALL HAVE A TEMPORARY WEDGE INSTALLED. (INCIDENTAL).
- 14. ANY EXISTING SURFACE TO REMAIN (CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, ETC.) THAT IS DAMAGED OR MARRED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED AND THE CONTRACTOR'S EXPENSE.
- CONTRACTOR TO CLEAN UP JAGGED CONCRETE GUTTER EDGES MEETING THE APPROVAL OF THE INSPECTOR. SOME SAWCUTTING MAY BE REQUIRED. COST IS INCIDENTAL.
- 16. CONTRACTOR TO CLEAN AND TACK COAT GRINDED ASPHALT SURFACE PRIOR TO OVERLAY (INCIDENTAL).
- 17. CONTRACTOR TO INSTALL TEMPORARY PAVEMENT MARKING DELINEATION TABS UNTIL PERMANENT PAVEMENT MARKING IS INSTALLED (INCIDENTAL). CONTRACTOR TO REMOVE ONCE PERMANENT MARKING HAS BEEN INSTALLED (INCIDENTAL).
- 18. TRAFFIC CONTROL SHALL INCLUDE BUT IS NOT LIMITED TO PAYEMENT MARKING REMOVAL, TEMPORARY PAYEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAYEMENT MARKINGS AND ALL INCEDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".

## LEGEND

= EXISTING CURB AND GUTTER ---- = SAW CUT LOCATION

⊗ WV = EXISTING WATER VALVE

= EXISTING WATER METER

-STM- = EXISTING STORM LINE

-SAN- = EXISTING STORM LINE

- G -- = EXISTING GAS LINE

= EXISTING GAS VALVE

-COM- = EXISTING TELECOMMUNICATION LINE

— EXISTING STORMWATER MANHOLE

-PWR- = EXISTING POWER LINE

= EXISTING SANITARY MANHOLE

= EXISTING PHONELINE MANHOLE

= EXISTING CATCH BASIN

= FIRE HYDRANT

= FXISTING MONUMENT

= SIDEWALK CLOSURE DELINEATION

MEINECKE **PAVEMENT** 

REHABILITATION

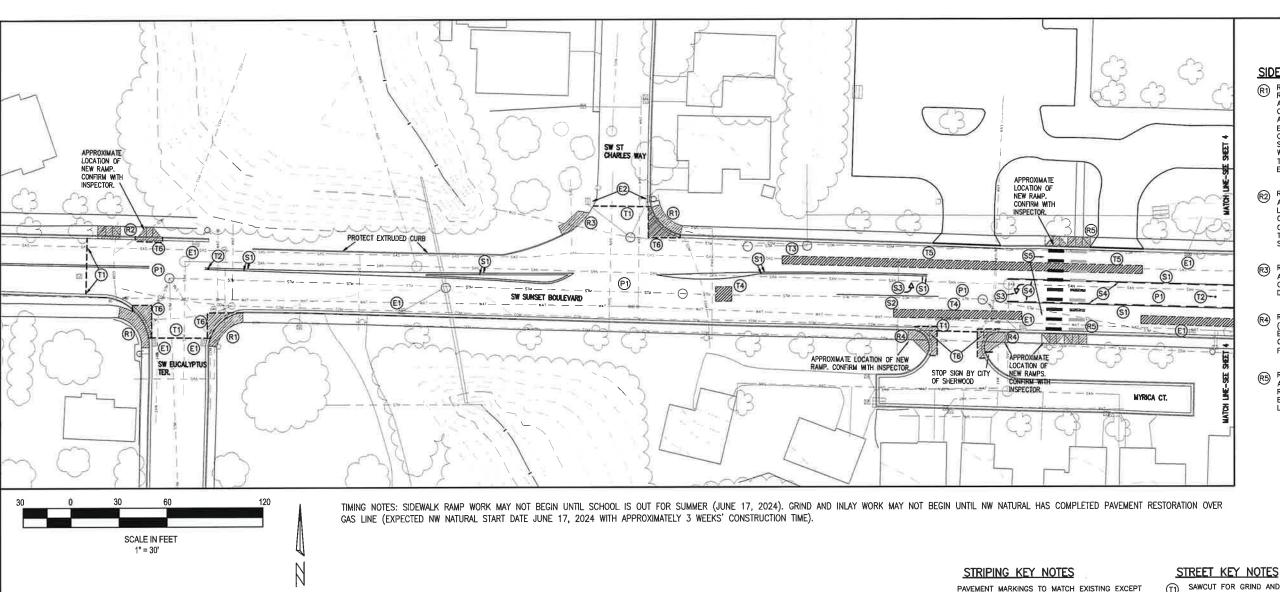
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IN SECTION

TIMBREL) (SUNSE

ROUNDABOUT,

SHERWOOD G DEPARTMENT PINE STREET OREGON 97140 무 CITY



### SIDEWALK RAMP KEY NOTES

- REMOVE AND INSTALL NEW SIDEWALK RAMP. REMOVAL LIMITS SHOWN ARE APPROXIMATE—CONTACT ENGINEER TO GO OVER REMOVAL LIMITS OF CONCRETE AND ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW. CONCRETE
  SIDEWALK RAMPS THAT ARE POURED
  WITHOUT ENGINEER REVIEW ARE SUBJECT TO REMOVAL AT THE CONTRACTOR'S
- REMOVE AND INSTALL NEW SIDEWALK RAMP APPROXIMATELY 20' EAST OF OLD LOCATION, CONTACT ENGINEER TO GO CONTACT ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW, INSTALL SIDEWALK IN OLD RAMP LOCATION
- R3 REMOVE EXISTING ASPHALT AND APPROXIMATELY 8' OF CURB. INSTALL CONCRETE—CONTACT ENGINEER FOR DIRECTION. INSTALL TRUNCATED DOME PAD
- REMOVE AND INSTALL NEW SIDEWALK RAMP (CONFIGURATION CHANGE). CONTACT ENGINEER TO GO OVER REMOVAL LIMITS. FORMING/PRIOR TO CONCRETE FOR REVIEW
- REMOVE AND INSTALL NEW SIDEWALK RAMP APPROXIMATELY 15' WEST. CONFIRM REMOVAL LIMITS AND NEW LOCATION WITH ENGINEER. INSTALL SIDEWALK IN OLD RAMP

PAVEMENT MARKINGS TO MATCH EXISTING EXCEPT WHEN DIRECTED DIFFERENTLY BY ENGINEER (NO PAVEMENT REFLECTORS REQUIRED)

- INSTALL YELLOW THERMOPLASTIC LINE (4" WIDTH) **(ST)**
- INSTALL WHITE THERMOPLASTIC LINE (8" WIDTH) (32)
- INSTALL WHITE THERMOPLASTIC LEFT (53)
- INSTALL DASHED YELLOW **S4**) THERMOPLASTIC LINE (4" WIDTH)
- (\$5) CONTINENTAL CROSSWALK (24" WIDTH)

#### **EROSION CONTROL KEY NOTES**

- INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN FRONT OF CURB OPENING
- INSTALL SILT SACK IN CATCH BASIN

- SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM **(T)** LOCATION WITH INSPECTOR (INCIDENTAL).
- BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS BY CITY OF SHERWOOD
- CALITION: EXISTING SHALLOW GAS LINE TO BE REPLACED BY (13) NW NATURAL. GRIND AND INLAY SHALL NOT BE INSTALLED UNTIL NW NATURAL HAS FINISHED TRENCH
- AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR
- AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY. THESE AREAS WILL LIKELY BE COMPLETED BY NW NATURAL AFTER SCHOOL IS OUT FOR
- AREA OF FULL DEPTH REMOVAL FOR BLENDING AC TO MATCH NEW GUTTER. REGRADE BASE AS NECESSARY AND INSTALL 3" THICKNESS OF LEVEL 3, 1/2" ACP FIBER MIX (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE FOR THE FIRST LIFT, WITH 1-1/2" ASPHALT OVERLAY, CONFIRM LIMITS WITH ENGINEER

#### PAVING KEY NOTES

GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. WORK DONE DURING DAYTIME WITH 2 LANES REMAINING OPEN FOR TRAFFIC AND FLAGGERS AS NEEDED.

T2S, R1W, W.N FON COUNTY, (WEST) VIEW PLAN SUNSET 일 IN SEC

V SUNSET BOULEVARD (WEST PLAN VIEW)

**SW** 

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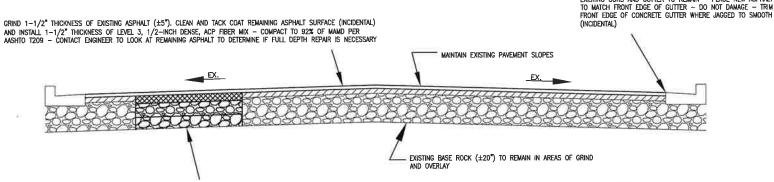
SHERWOOD

IG DEPARTMENT
W PINE STREET
OREGON 97140 P

CT ENGIN

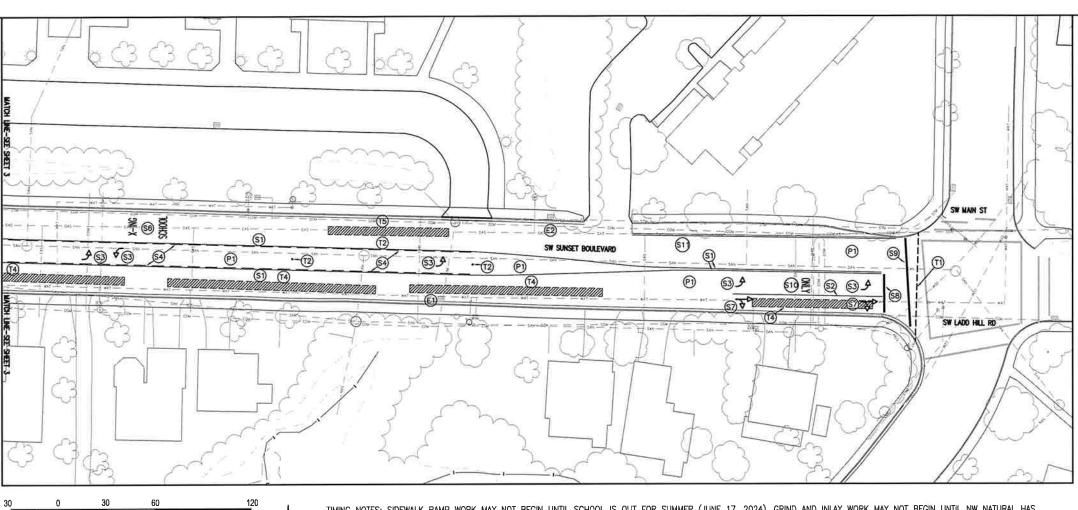
SHEET NO. or 11

TYPICAL STREET RECONSTRUCTION SECTION NOT TO SCALE EXISTING CURB AND GUTTER TO REMAIN - PLACE NEW ASPHALT



SUBGRADE REPAIR: AFTER GRINDING, CONTACT ENGINEER FOR REVIEW OF REMAINING PAVEMENT CONDITIONS. REMOVE REMAINING DEPTH OF EXISTING ASPHALT AT LOCATIONS AUTHORIZED BY THE ENGINEER. REMOVE BASE ROCK TO DEPTH DIRECTED BY ENGINEER.—INSTALL GEOGRID ON BOTTOM AND ADDITIONAL LYCER OF GEOGRID IF ROCK SECTION IS GREATER THAN 16" IN DEPTH. CONTRACTOR TO CUT AND INSTALL GEOGRID (INCIDENTAL), CITY OF SHEWOOD WILL DELIVER GEOGRID TO SITE. LEFT OVER GEOGRID TO BE GIVEN BACK TO CITY OF SHERWOOD. INSTALL 3/4"-O" AGGREGATE BASE UP TO 4" BELOW GRINDED ASPHALT SURFACE (COMPACT TO 95% DENSITY OF AASHTO T-180 TEST METHOD) - INSTALL 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE - HATCHED AREA - CONFIRM LIMITS WITH ENGINEER.

NOTE: CONTRACTOR TO COORDINATE WITH ENGINEER TO EVALUATE PAVEMENT CONDITION AFTER GRIND TO DETERMINE EXTENT OF PAVEMENT SUBGRADE REPAIR. QUANTITIES IN BID SCHEDULE ARE BASED UPON AREAS SHOWN ON PLAN. ACTUAL AREAS WILL BE DETERMINED IN FIELD BY ENGINEER.



SCALE IN FEET 1" = 30"

TIMING NOTES: SIDEWALK RAMP WORK MAY NOT BEGIN UNTIL SCHOOL IS OUT FOR SUMMER (JUNE 17, 2024). GRIND AND INLAY WORK MAY NOT BEGIN UNTIL NW NATURAL HAS COMPLETED PAVEMENT RESTORATION OVER GAS LINE (EXPECTED NW NATURAL START DATE JUNE 17, 2024 WITH APPROXIMATELY 3 WEEKS CONSTRUCTION TIME).

#### STREET KEY NOTES

- SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM LOCATION WITH INSPECTOR (INCIDENTAL).
- BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS BY CITY OF SHERWOOD
- CAUTION: EXISTING SHALLOW GAS LINE. TO BE REPLACED BY NW NATURAL. GRIND AND INLAY SHALL NOT BE INSTALLED UNTIL NW NATURAL HAS FINISHED TRENCH
- AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY
- AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY, THESE AREAS WILL LIKELY BE COMPLETED BY NW NATURAL AFTER SCHOOL IS OUT FOR
- AREA OF FULL DEPTH REMOVAL FOR BLENDING AC TO MATCH NEW GUTTER. REGRADE BASE AS NECESSARY AND INSTALL 3" THICKNESS OF LEVEL 3, 1/2" ACP FIBER MIX (COMPACT TO 92% OF MMAD PER AASHTO 1209) TO MATCH GRINDED ASPHALT SURFACE FOR THE FIRST LIFT, WITH 1-1/2" ASPHALT OVERLAY, CONFIRM LIMITS WITH ENGINEER

#### PAVING KEY NOTES

GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. WORK DONE DURING DAYTIME WITH 2 LANES REMAINING OPEN FOR TRAFFIC AND FLAGGERS AS NEEDED.

## EROSION CONTROL KEY NOTES

- INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN FRONT OF CURB
- INSTALL BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN CURB INLET OPENING

#### STRIPING KEY NOTES

PAVEMENT MARKINGS TO MATCH EXISTING EXCEPT WHEN DIRECTED DIFFERENTLY BY ENGINEER (NO

- INSTALL YELLOW THERMOPLASTIC LINE (4" WIDTH) **(S1)**
- INSTALL WHITE THERMOPLASTIC LINE (8" WIDTH) **(S2)**
- INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- INSTALL DASHED YELLOW THERMOPLASTIC LINE (4" WIDTH)
- INSTALL WHITE THERMOPLASTIC "X-ING **S**6
- INSTALL WHITE THERMOPLASTIC RIGHT TURN STRAIGHT ARROW **(S7)**
- INSTALL WHITE THERMOPLASTIC STOP BAR (12" WIDTH)
- INSTALL WHITE THERMOPLASTIC CROSSWALK LINE (12" WIDTH)
- INSTALL WHITE THERMOPLASTIC "ONLY"
- INSTALL WHITE THERMOPLASTIC LINE

SHERWOOD
IG DEPARTMENT
N PINE STREET
OREGON 97140 OF S CT ENGINE

SET BOULEVARD PLAN VIEW)

(EAST PL

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T2S, R1W, ON COUNTY

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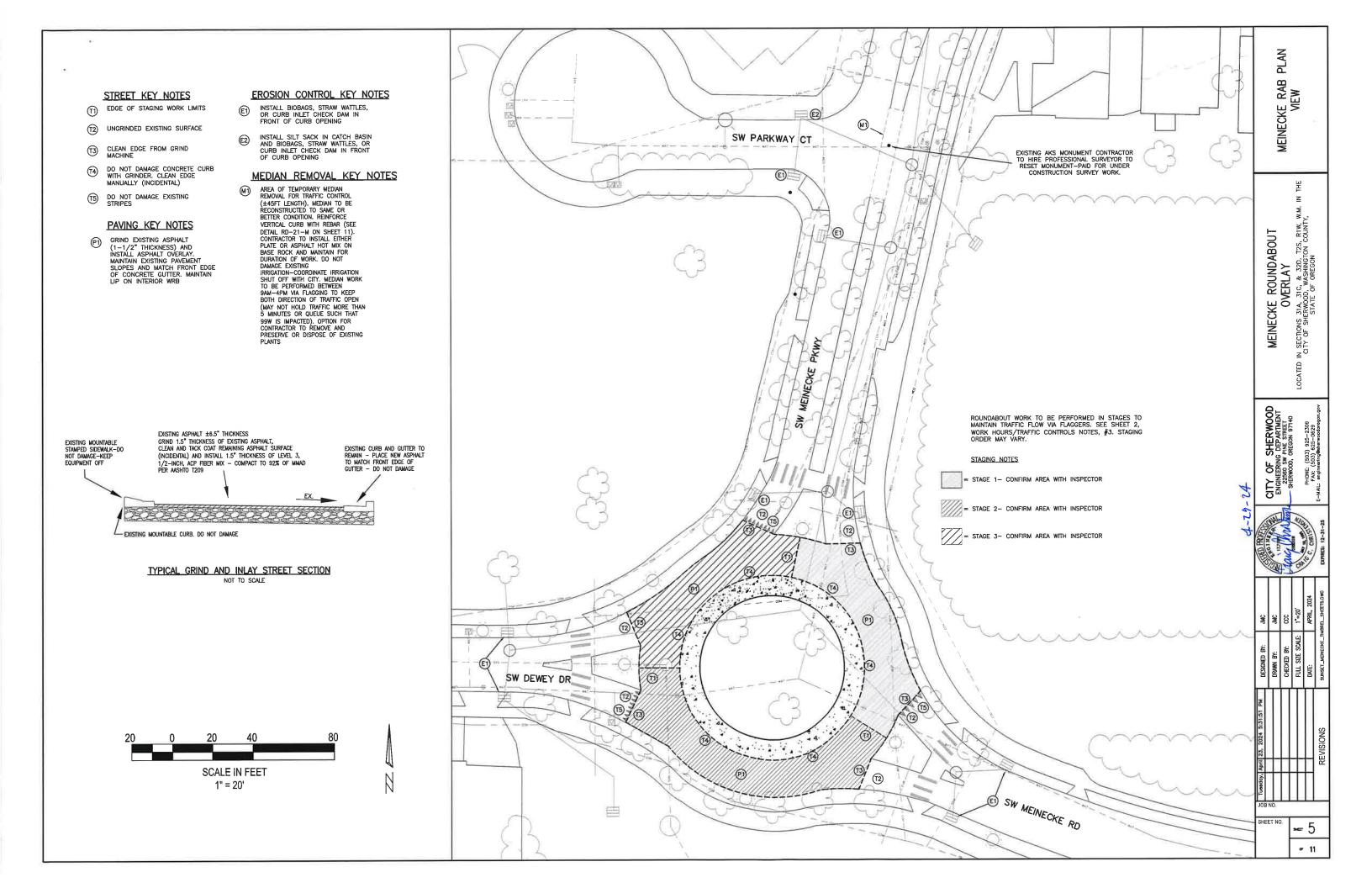
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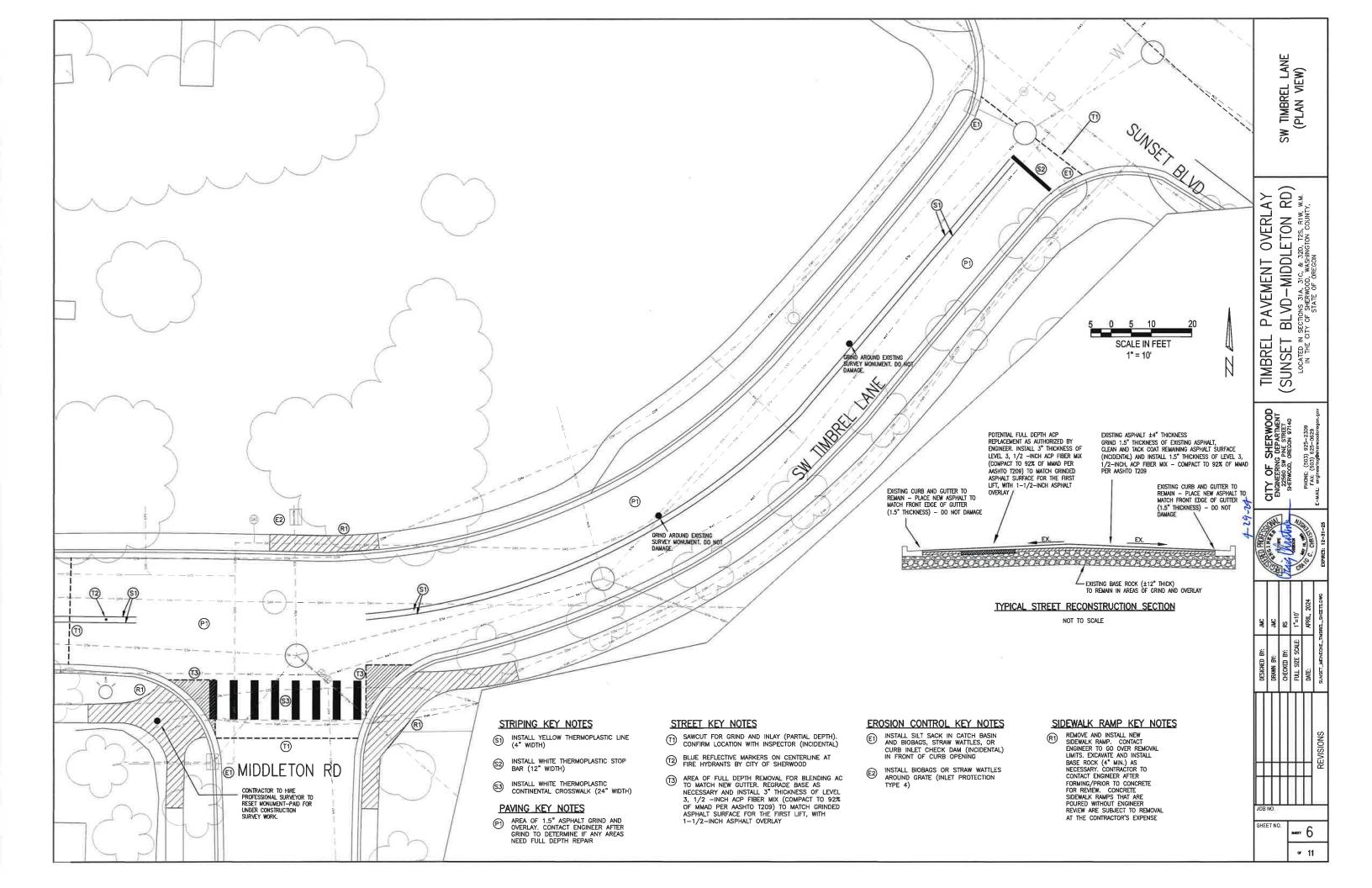
VIEW (EAST)

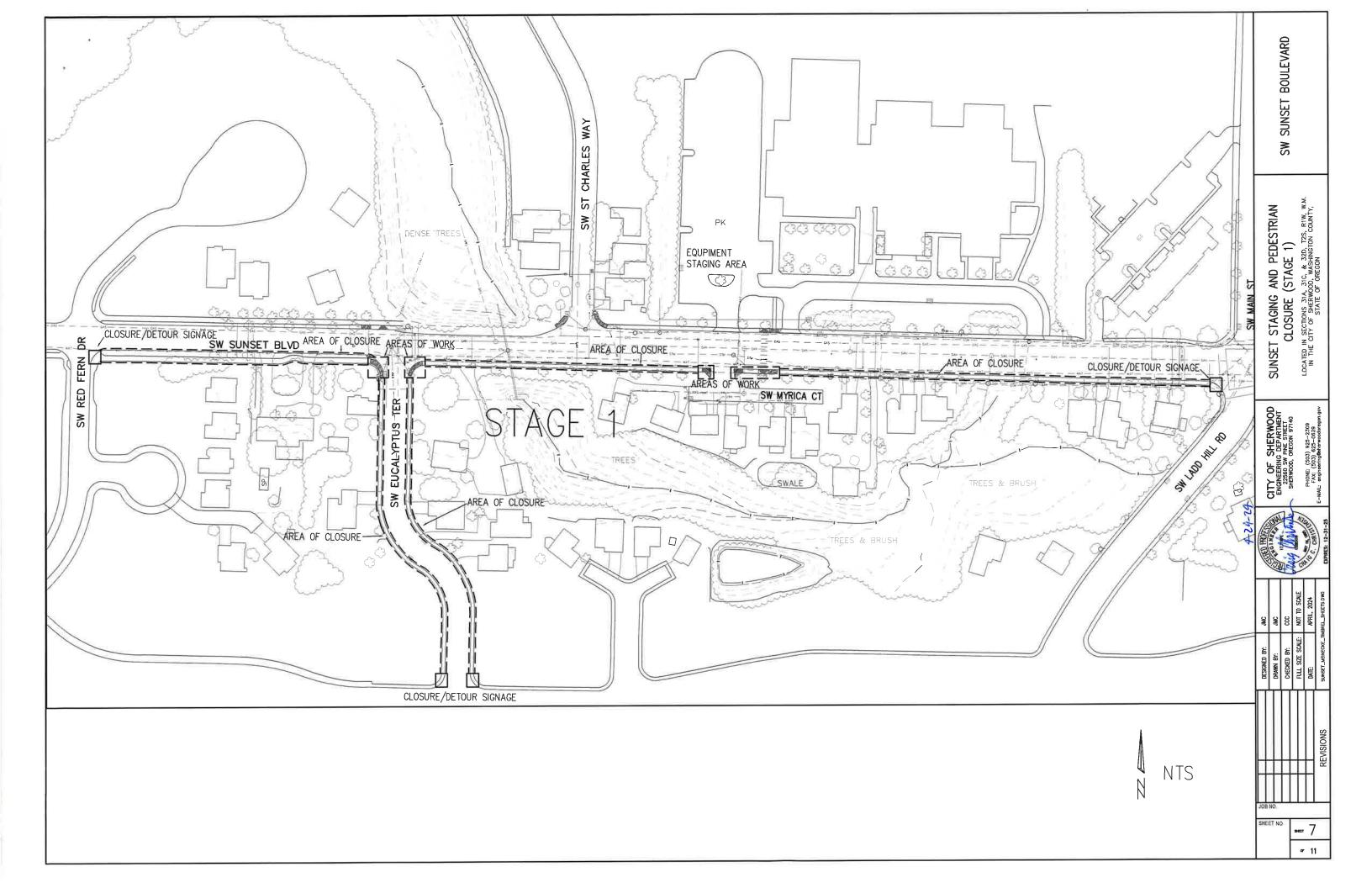
PLAN

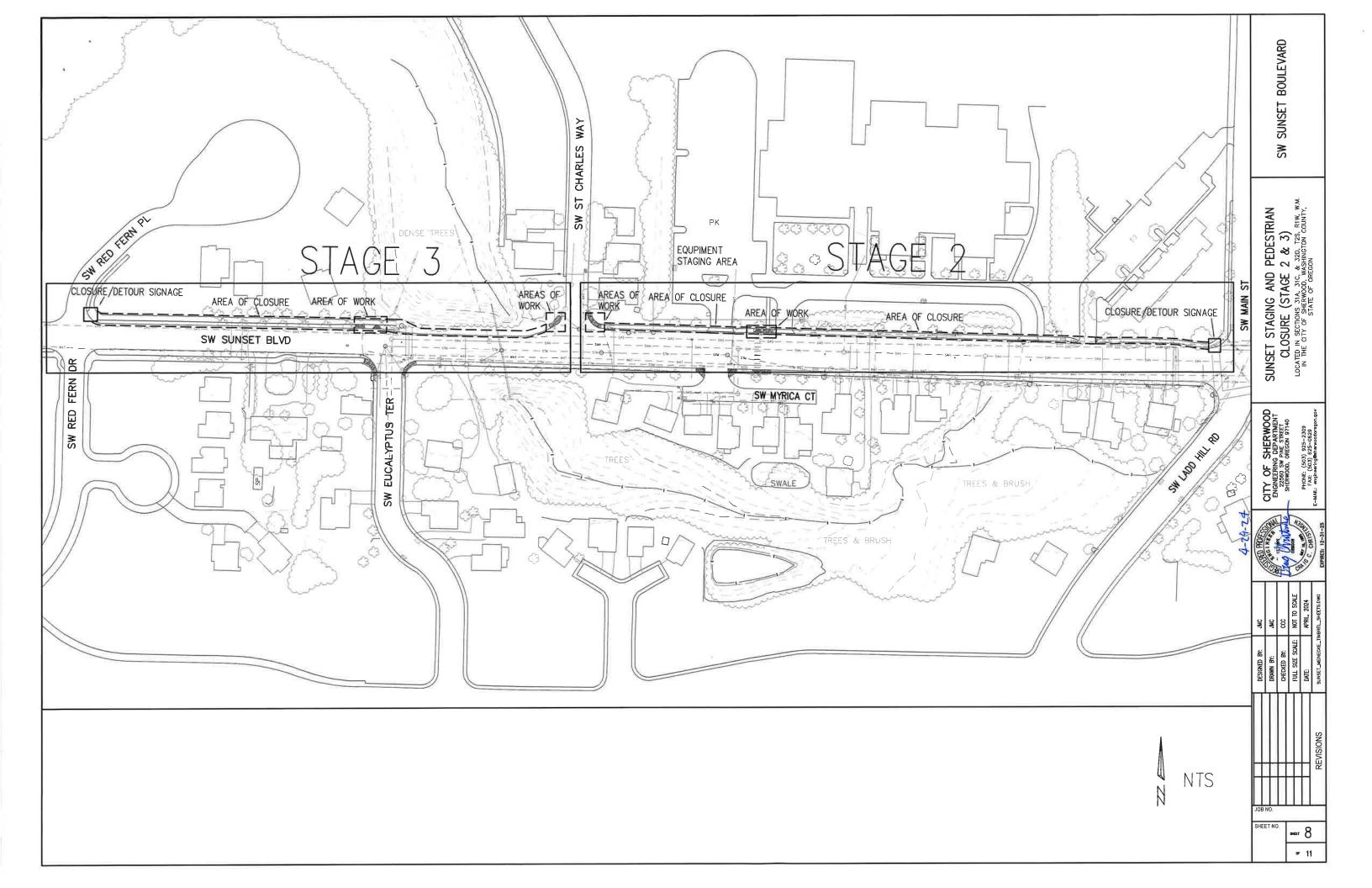
SUNSET

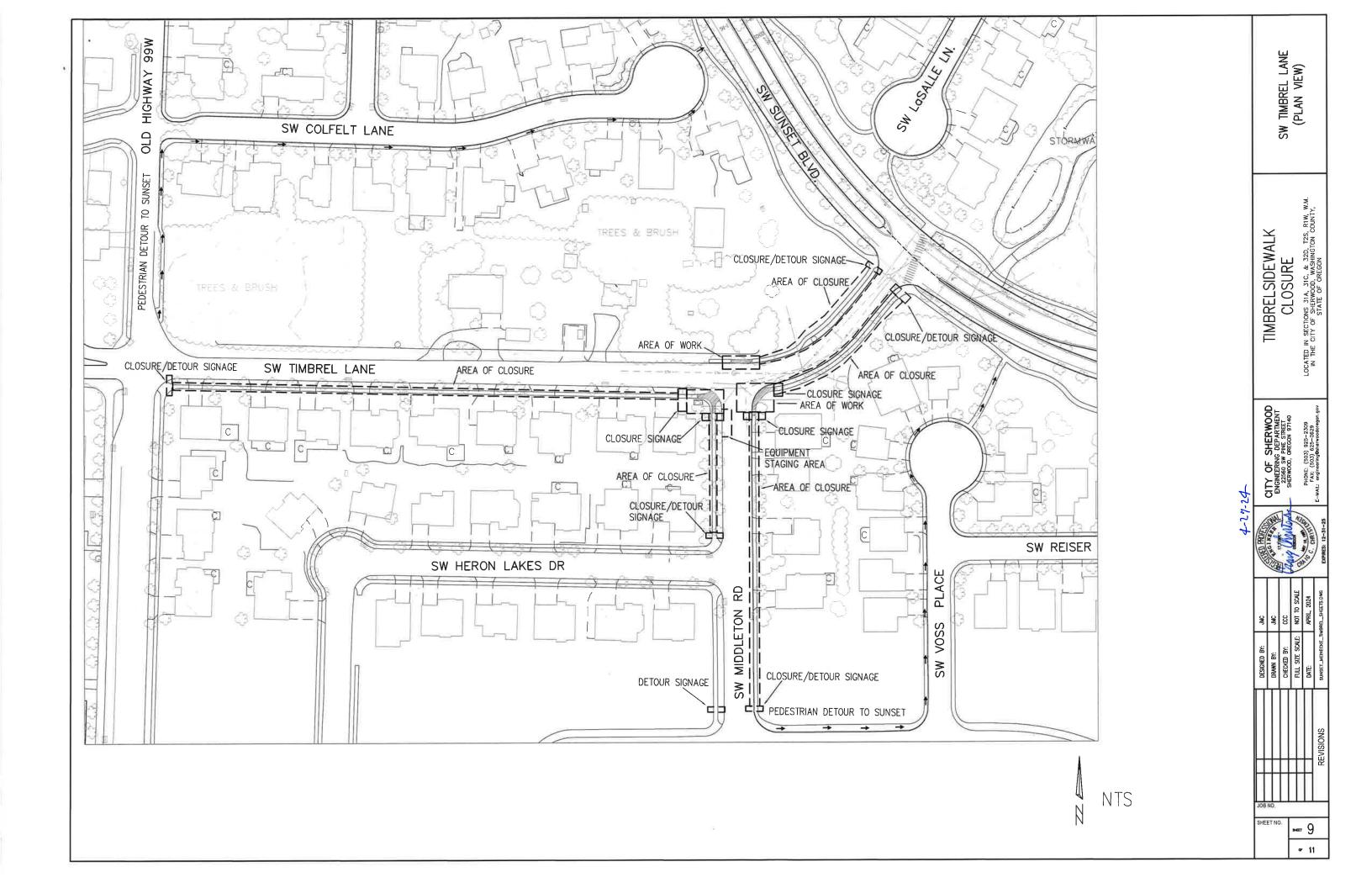
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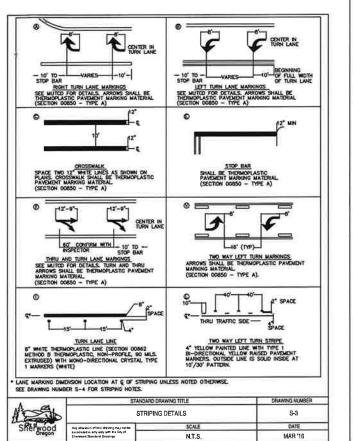


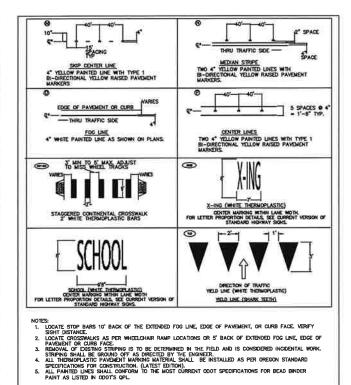












LANE MARKING DIMENSION LOCATION AT & OF STRIPING UNLESS NOTED OTHERWISE.

STANDARD DRAWING TITLE

STRIPING DETAILS II

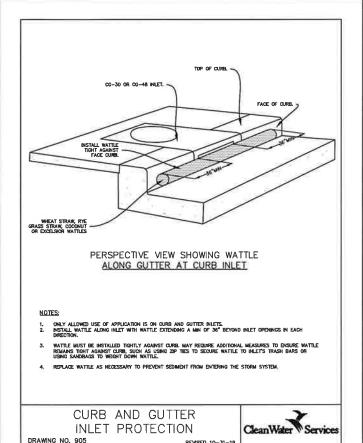
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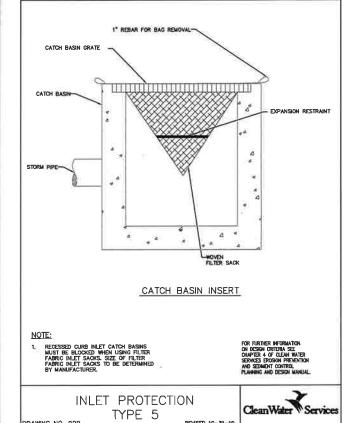
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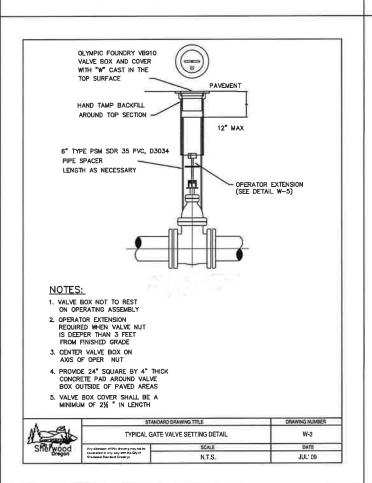
S-4

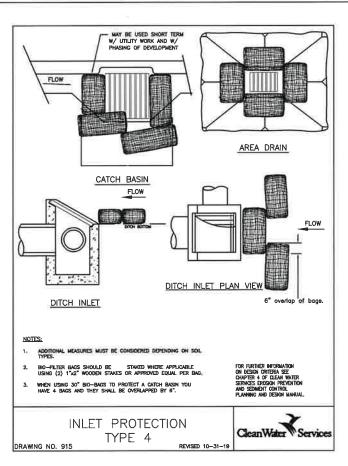
DATE

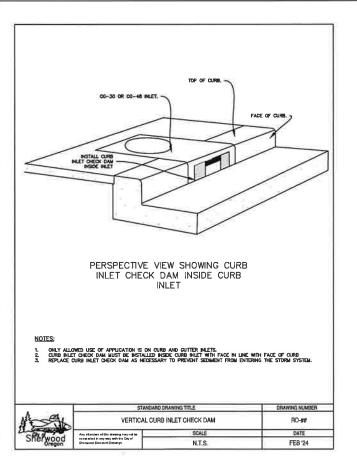
MAR '16

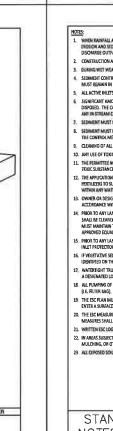




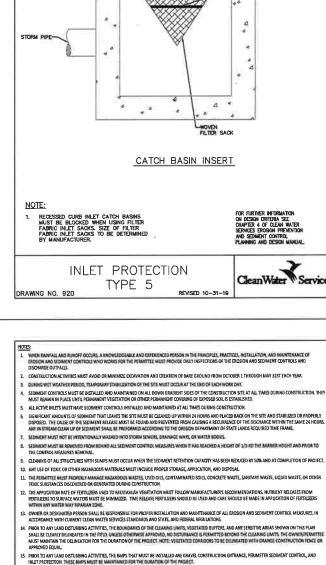








DRAWING NO. 945





SHEET NO

DETAILS

SHERWOOD

R

, MEINECKE RAB RLAY DETAILS 31C, & 32D, T2S, R1W, ERWOOD, WASHINGTON

SUNSET, TIMBREL, MEIN PAVEMENT OVERLAY

SHERWOOD
G DEPARTMENT
W PINE STREET
OREGON 97140

NEERING 2560 SW RWOOD,

LOCATED IN SECTIONS 31A, 31C, & W.M. IN THE CITY OF SHERWOOD, COUNTY, STATE OF OREGON

»EET 10 **∞** 11

NOTE:

1. PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE
A SITE SPECIFIC DRAWING FOR EACH RAMP.

2. PROJECT ENGINEER SHALL WEBTY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE
PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO
CROSSWALK OR STOP LIMITALL MEET CURRENT AND SHALL LOCATE EACH RAMP RELATIVE TO
CROSSWALK OR STOP LIMITALL MEET CURRENT AND SHANDARDS.

4. DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24\* LONG IN DRECTION OF TRAVEL AND FULL
WIGHTH OF RAMP, WITH DOMES ALLOHED ON A SOURME GROW WITH ITS GROTINES PARALLEL AND
PERPENDICULAR TO THE CENTERLINE OF THE RAMP, COLOR OF DETECTABLE WARNING SHALL BE
SAFETY YELLOW.

5. GUIRS INLET ON COTATS BASIN SHALL NOT BE ALLOHED IN PRONT OF RAMP.

6. A. A.\* WAN SLIUM.

7. \*\*A"-OF BASE ROCK SHALL BE COMPACTED TO BOX OF UNASHUM DEPISTY.

8. EVEL THE CURP CUT FROM GUITER TO BACK OF CURB AT 8.33X (11:2) MAX.

9. SCORE CONCRETE AT CRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS
SCOWL.

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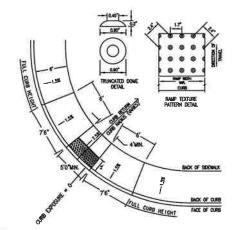
WIDTH OF STREET

AC PAVEMENT

SEE NOTE 6

1" RADIUS

CRADIATE	STANDARD DRAWING TITLE	
DOUBLE CROSSING CUI	RB RETURN & RAMP DETAIL	RD-44
Any alteration of this detention may not be	SCALE	DATE
strocketed in any way with the City of Shormand Standard Driveleys	N.T.S.	MAR '16



- 11, PROJECT ENGINEER SHALL USE THIS DRAWING AS A QUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.

  PROJECT ENGINEER SHALL VERIEY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSMALK OR STOP LINE.

  3. SIDEMALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.

  4. PLANTER STRP DETAIL REPER TO GOOD STANDARDS OF THE STANDARDS.

  5. DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24° LONG IN DIRECTION OF TRAVEL AND PLIL WORTH OF RAMP, WITH DOMES AUGINED ON A SQUARE GROW WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE
- SAFETY YELLOW.
  CURB BIEST OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
  CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PS WITH
  A 4" MAX SLUP.

A 4" MAX SLUMP.

4"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXMUM DENSTY.

BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.

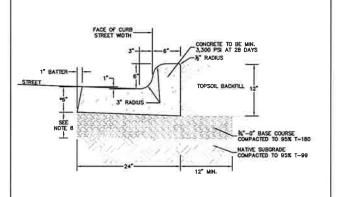
5. SCORE CONFORCE AT GRADE CHANGES, SHIRTACE TEXTURE CHANGES AND AT ALL OTHER POINTS.

9. BEVILT THE CURB CONTROL OF MACES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.

11. CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.

12. PROJECT ENGINEER SHALL ACCEPT FULL RESPONSEULTY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

4	STANDARD (	PAWING TITLE	DRAWING NUMBER
	CURB-TIGHT CURB F	RETURN & RAMP DETAIL	RD-43
Sherwood	Any attention of this drawing may not be	SCALE	DATE
Oragon	Sharanan Danbard Graumpa	N.T.S.	MAR '16



- NOTES:

  1. MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CUMB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).

  2. CONCRETE STALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PS, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PS, WITH A 28-DAY SOM JOINTS TO BE PROVIDED AT EACH:

  A. POINT OF TANCENCY.

  B. COLD JOINT.

  C. SIDE OF INLET STRUCTURES.

  DEPARTS OF DIVENMANS.

  THEORIESS OF STRENGTH SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THEORIESS OF STRENGTH AND SHALL BE PRE-MOLDED. ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THEORIESS OF STRENGTH AND SHALL BE PRE-MOLDED.

  A. SPACING OF NOT MORE THAN 15 FEET.

  B. DEPTH OF JOINT OF AT LEAST 11/5".

  B. BASE ROCK SHALL BE \$%-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STRENGT STRUCTURES OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.

  FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET ROAD SECTIONS, SEE STD DET ROAD SECTIONS, SEE STD DET COMPACTED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET COMPACTED SECTIONS.

DRAWING NUMBER

RD-22

MAR '16

EXISTING CURB

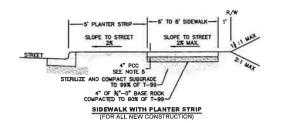
RCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.

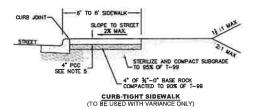
	STANDARD 0	RAWING TITLE
SAL.	MONOLITHIC CURB AND GUTTER	
erwood	Any all erabon of thire drawing may not be	SCALE
Oragon	Sharehad Sarahad Charrenge	N.T.S.

16. 5

-12" FMRFD-

EXISTING CURB





CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI © 28 DAYS. WITH A 4" MAX SLUMP.

CONCRETE SHALL BY A \* MAX SLIMP.

AT MAY SLIMP, A \* MAX SLIMP.

SIDEWALK PANELS TO BE SOLVARE (6) LONG x 6' MIDE TRY

SIDEWALK PANELS TO BE SOLVARE (6) LONG x 6' MIDE TRY

SIDEWALK PANELS TO BE SOLVARE (6) LONG x 6' MIDE TRY

LONG SAME SHALL PANELS AND FAT SIDES OF DRIVEWAY APPROACHES, UTILITY VALUETS,

OURS DAMPEN AND CORE TO TAY SIDEWALK PANELS AND AT SPACING NOT TO EXCEED 45'.

FOR SIDEWALK SHALL PANEL AND AT SPACING NOT TO EXCEED 45'.

FOR SIDEWALKS SHALL PANEL AND AT SPACING NOT THE SAME TIME AS THE CURB,

THE JOHT BETWEEN THEM SHALL BE A TROWELD JON'T WITH A MINIMUM X" RADIUS.

SIDEWALKS SHALL HAVE A MINIMUM THROWSES OF 4'. THE SAME TIME AS THE CURB,

THE JOHN SIDEWALK SHALL PANELS AND AT SPACING SOLVEN STATEMENT OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6''.

CONCRETE SHALL HAVE A BROOM FINSH, ALL JOINTS SHALL BE EDED WITH 3' SHINL

WOTH OF PLANTER STEPP AND SIDEWALK IS MEASURED TROW FACE OF CURB.

IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 'I PAST BACK OF SIDEWALK WITH A 3'' DIAMETER ADS PIPE CONTRACTION

JOINT SHALL BE PLACE OVER PIPE.

10 000	STANDARD DRAWING TITLE	
Sherwood Any stanton of the drawing may not be	SIDEWALK DETAIL	
		SCALE
Oregon	annorthind in any way with the City of Sherwood Standard Drewings	N.T.S.



DETAILS

SHERWOOD

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CITY

SHERWOOD
IG DEPARTMENT
W PINE STREET
OREGON 97140 유 CITY ENGINE

JOB NO

SHEET NO

<del>----</del> 11 **∞** 11

4 7 79%

DRAWING NUMBER

RD-26

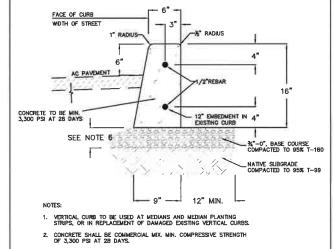
MAR '16

# -W RADIUS W"-0", BASE COURSE COMPACTED TO 95% T-180 12° MIN.

- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
- CONCRETE SHALL BE COMMERCIAL MIX. MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
- EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
- MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF ½ INCH.

- B BASE ROCK: ¾"-0", COMPACTED TO 85% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

	STANDARD DRAWING TITLE  VERTICAL CURB		RD-21
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
	6110clebrd in any way with the City of Shanwood Slandard Drawings	N.T.S.	MAR '16



- EXPANSION JOINTS TO BE PROMDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
- MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2 INCH.
- CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1½" IN DEPTH.
- 6 BASE ROCK: \*\*-0\*, COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6\* IN DEPTH, WHICHEVER IS GREATER.

Most	STANDARD DRAWING TITLE REBAR CONNECTION DETAIL (PROFILE VIEW)		DRAWING NUMBER
Sherwood	Any attention of this drawing may not be	SCALE	DATE
Oregon	Estociated in the way with the City of Sherwood Standard Drawings	NTS	APRIL '24

=	M-A
<b>=</b>	Sherwood

STANDARD DRAWING TITLE REBAR CONNECTION DETAIL (PLAN VIEW)		DRAWING NUMBER
Any exerction of this drawing may cot be	SCALE	DATE
associated in any very with the City of Sharewood Standard Drawings	N.T.S.	APRIL '24

NEW CURB

AC PAVEMENT