

CITY OF SHERWOOD

PAVEMENT REHABILITATION (SUNSET, MEINECKE ROUNDABOUT, AND TIMBREL)

APRIL 2024

(BID DOCUMENT VOLUME 2 OF 2 - CONTRACT DRAWINGS)

PROJECT LOCATION:

SW SUNSET BOULEVARD BETWEEN SW EUCALYPTUS TERRACE
AND SW MAIN ST/SW LADD HILL RD

SW TIMBREL LN BETWEEN SW MIDDLETON RD AND SW SUNSET
BLVD

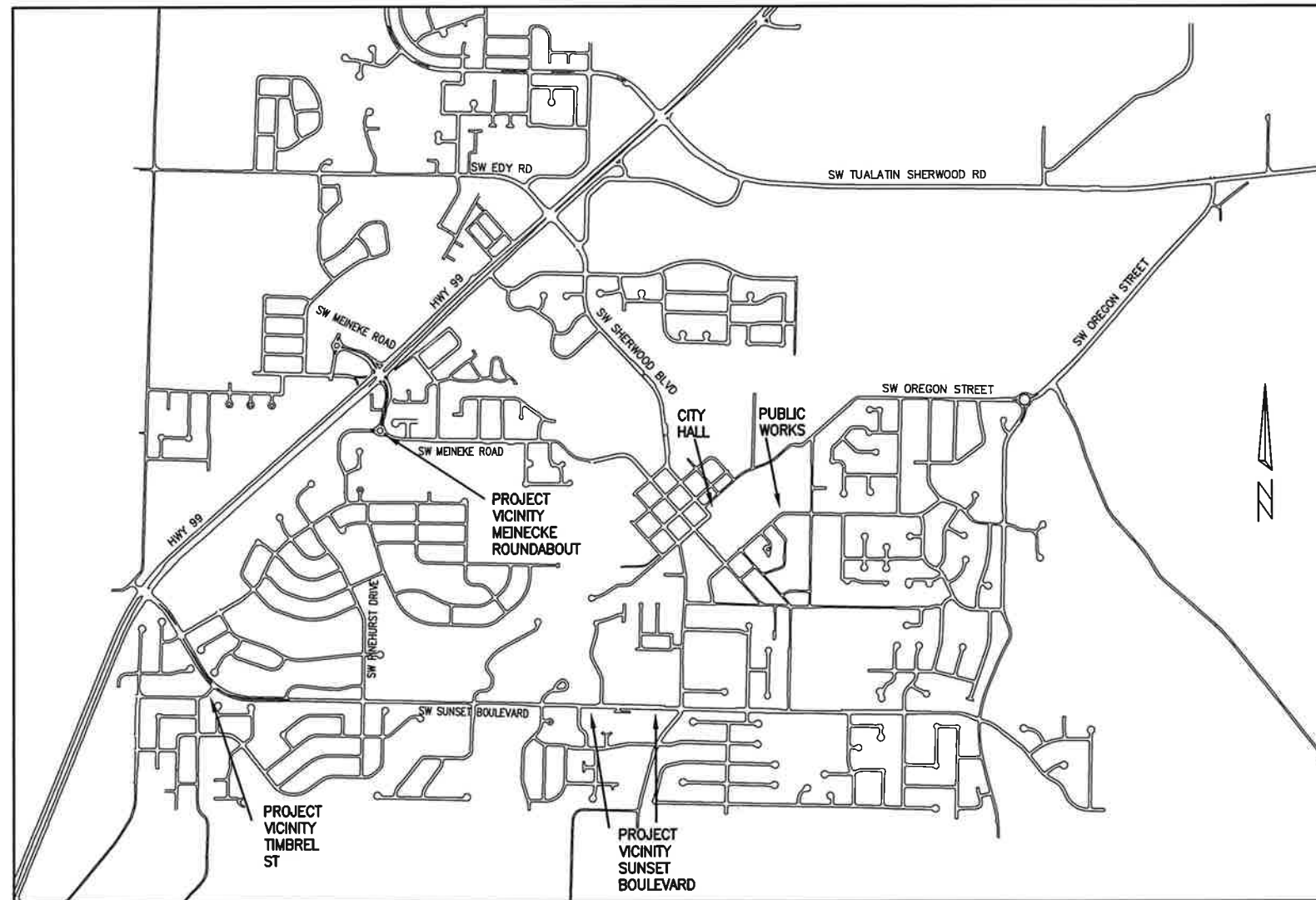
MEINECKE ROUNDABOUT ADJACENT TO SW MEINECKE PKWY, SW
MEINECKE RD, AND SW DEWEY DR

DEVELOPER/OWNER:

CITY OF SHERWOOD
22560 SW PINE ST
SHERWOOD, OREGON 97140
CONTACT: CRAIG CHRISTENSEN, P.E.
PH. 503-925-2301
CHRISTENSEN@SHERWOODOREGON.GOV

SHEET INDEX:

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10. CITY OF SHERWOOD DETAILS (1)
11. CITY OF SHERWOOD DETAILS (2)



VICINITY MAP

NOT TO SCALE

INSPECTOR INFORMATION:

CITY OF SHERWOOD: ANDY STIRLING (503) 925-2307
CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

SURVEY INFORMATION:

HORIZONTAL
AERIAL TOPOGRAPHIC MAP (2004) AND VISUAL
OBSERVATION.

VERTICAL
AERIAL TOPOGRAPHIC MAP (2004)

THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY
INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A
SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR
PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION
CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY
UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED
AREA.

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON
UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR
952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503)
232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU
MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL
(503) 246-6699.

COVER SHEET

PAVEMENT REHABILITATION (SUNSET,
MEINECKE ROUNDABOUT, TIMBREL)

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE
CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

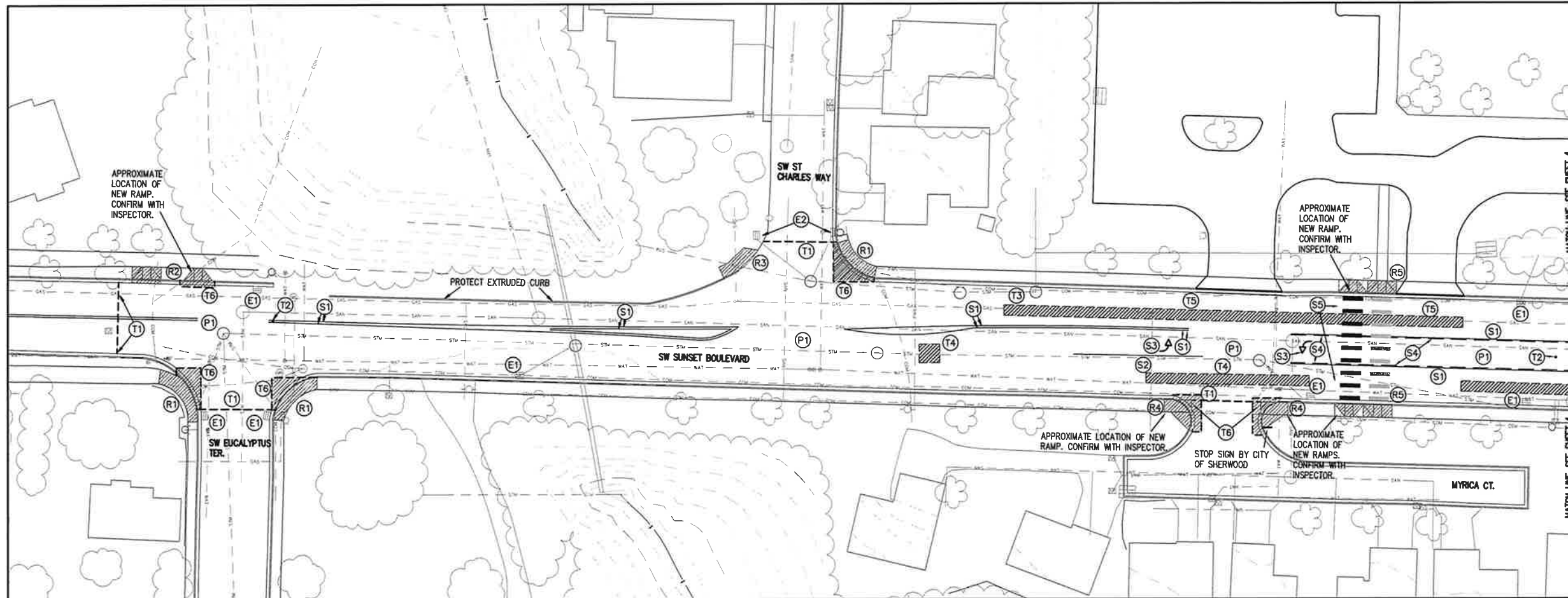
4-28-24



DESIGNED BY:	JMC
DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	AS NOTED
DATE:	APRIL, 2024
SUNSET_MEINECKE_TIMBREL_SHEETS.DWG	

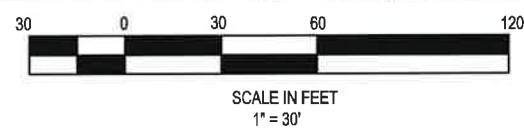
REVISIONS	
NO.	DESCRIPTION

JOB NO.	
SHEET NO.	
	1
	of 11



SIDEWALK RAMP KEY NOTES

- (R1) REMOVE AND INSTALL NEW SIDEWALK RAMP. REMOVAL LIMITS SHOWN ARE APPROXIMATE—CONTACT ENGINEER TO GO OVER REMOVAL LIMITS OF CONCRETE AND ASPHALT. CONTRACTOR TO CONTACT ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW. CONCRETE SIDEWALK RAMP THAT ARE POURED WITHOUT ENGINEER REVIEW ARE SUBJECT TO REMOVAL AT THE CONTRACTOR'S EXPENSE
- (R2) REMOVE AND INSTALL NEW SIDEWALK RAMP APPROXIMATELY 20' EAST OF OLD LOCATION. CONTACT ENGINEER TO GO OVER REMOVAL LIMITS. CONTRACTOR TO CONTACT ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW. INSTALL SIDEWALK IN OLD RAMP LOCATION
- (R3) REMOVE EXISTING ASPHALT AND APPROXIMATELY 8' OF CURB. INSTALL CONCRETE—CONTACT ENGINEER FOR DIRECTION. INSTALL TRUNCATED DOME PAD
- (R4) REMOVE AND INSTALL NEW SIDEWALK RAMP (CONFIGURATION CHANGE). CONTACT ENGINEER TO GO OVER REMOVAL LIMITS. CONTRACTOR TO CONTACT ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW
- (R5) REMOVE AND INSTALL NEW SIDEWALK RAMP APPROXIMATELY 15' WEST. CONFIRM REMOVAL LIMITS AND NEW LOCATION WITH ENGINEER. INSTALL SIDEWALK IN OLD RAMP LOCATION



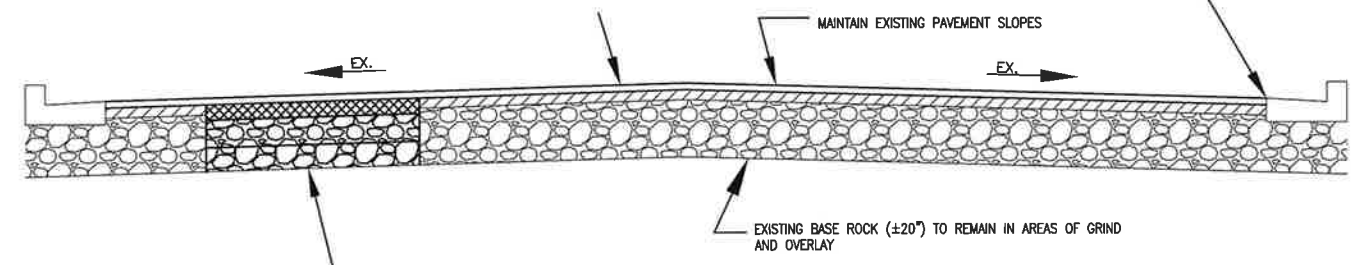
TIMING NOTES: SIDEWALK RAMP WORK MAY NOT BEGIN UNTIL SCHOOL IS OUT FOR SUMMER (JUNE 17, 2024). GRIND AND INLAY WORK MAY NOT BEGIN UNTIL NW NATURAL HAS COMPLETED PAVEMENT RESTORATION OVER GAS LINE (EXPECTED NW NATURAL START DATE JUNE 17, 2024 WITH APPROXIMATELY 3 WEEKS' CONSTRUCTION TIME).

TYPICAL STREET RECONSTRUCTION SECTION

NOT TO SCALE

GRIND 1-1/2" THICKNESS OF EXISTING ASPHALT (±5%). CLEAN AND TACK COAT REMAINING ASPHALT SURFACE (INCIDENTAL) AND INSTALL 1-1/2" THICKNESS OF LEVEL 3, 1/2-INCH DENSE, ACP FIBER MIX - COMPACT TO 92% OF MAMD PER AASHTO T209 - CONTACT ENGINEER TO LOOK AT REMAINING ASPHALT TO DETERMINE IF FULL DEPTH REPAIR IS NECESSARY

EXISTING CURB AND GUTTER TO REMAIN - PLACE NEW ASPHALT TO MATCH FRONT EDGE OF GUTTER - DO NOT DAMAGE - TRIM FRONT EDGE OF CONCRETE GUTTER WHERE JAGGED TO SMOOTH (INCIDENTAL)



SUBGRADE REPAIR: AFTER GRINDING, CONTACT ENGINEER FOR REVIEW OF REMAINING PAVEMENT CONDITIONS. REMOVE REMAINING DEPTH OF EXISTING ASPHALT AT LOCATIONS AUTHORIZED BY THE ENGINEER. REMOVE BASE ROCK TO DEPTH DIRECTED BY ENGINEER - INSTALL GEOGRID ON BOTTOM AND ADDITIONAL LAYER OF GEOGRID IF ROCK SECTION IS GREATER THAN 16" IN DEPTH. CONTRACTOR TO CUT AND INSTALL GEOGRID (INCIDENTAL). CITY OF SHERWOOD WILL DELIVER GEOGRID TO SITE. LEFT OVER GEOGRID TO BE GIVEN BACK TO CITY OF SHERWOOD. INSTALL 3/4"-0" AGGREGATE BASE UP TO 4" BELOW GRINDED ASPHALT SURFACE (COMPACT TO 95% DENSITY OF AASHTO T-180 TEST METHOD) - INSTALL 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE - HATCHED AREA - CONFIRM LIMITS WITH ENGINEER.

NOTE: CONTRACTOR TO COORDINATE WITH ENGINEER TO EVALUATE PAVEMENT CONDITION AFTER GRIND TO DETERMINE EXTENT OF PAVEMENT SUBGRADE REPAIR. QUANTITIES IN BID SCHEDULE ARE BASED UPON AREAS SHOWN ON PLAN. ACTUAL AREAS WILL BE DETERMINED IN FIELD BY ENGINEER.

STRIPING KEY NOTES

PAVEMENT MARKINGS TO MATCH EXISTING EXCEPT WHEN DIRECTED DIFFERENTLY BY ENGINEER (NO PAVEMENT REFLECTORS REQUIRED)

- (S1) INSTALL YELLOW THERMOPLASTIC LINE (4" WIDTH)
- (S2) INSTALL WHITE THERMOPLASTIC LINE (8" WIDTH)
- (S3) INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- (S4) INSTALL DASHED YELLOW THERMOPLASTIC LINE (4" WIDTH)
- (S5) INSTALL WHITE THERMOPLASTIC CONTINENTAL CROSSWALK (24" WIDTH)

EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN FRONT OF CURB OPENING
- (E2) INSTALL SILT SACK IN CATCH BASIN

STREET KEY NOTES

- (T1) SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM LOCATION WITH INSPECTOR (INCIDENTAL).
- (T2) BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS BY CITY OF SHERWOOD
- (T3) CAUTION: EXISTING SHALLOW GAS LINE. TO BE REPLACED BY NW NATURAL. GRIND AND INLAY SHALL NOT BE INSTALLED UNTIL NW NATURAL HAS FINISHED TRENCH
- (T4) AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY
- (T5) AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY. THESE AREAS WILL LIKELY BE COMPLETED BY NW NATURAL AFTER SCHOOL IS OUT FOR SUMMER
- (T6) AREA OF FULL DEPTH REMOVAL FOR BLENDING AC TO MATCH NEW GUTTER. REGRADE BASE AS NECESSARY AND INSTALL 3" THICKNESS OF LEVEL 3, 1/2" ACP FIBER MIX (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE FOR THE FIRST LIFT, WITH 1-1/2" ASPHALT OVERLAY. CONFIRM LIMITS WITH ENGINEER

PAVING KEY NOTES

- (P1) GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. WORK DONE DURING DAYTIME WITH 2 LANES REMAINING OPEN FOR TRAFFIC AND FLAGGERS AS NEEDED.

A-29-24



DESIGNED BY:	JMC
DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	1"=30'
DATE:	APRIL 2024
SUNSET_MENCKE_TMBREL_SHEETS.DWG	

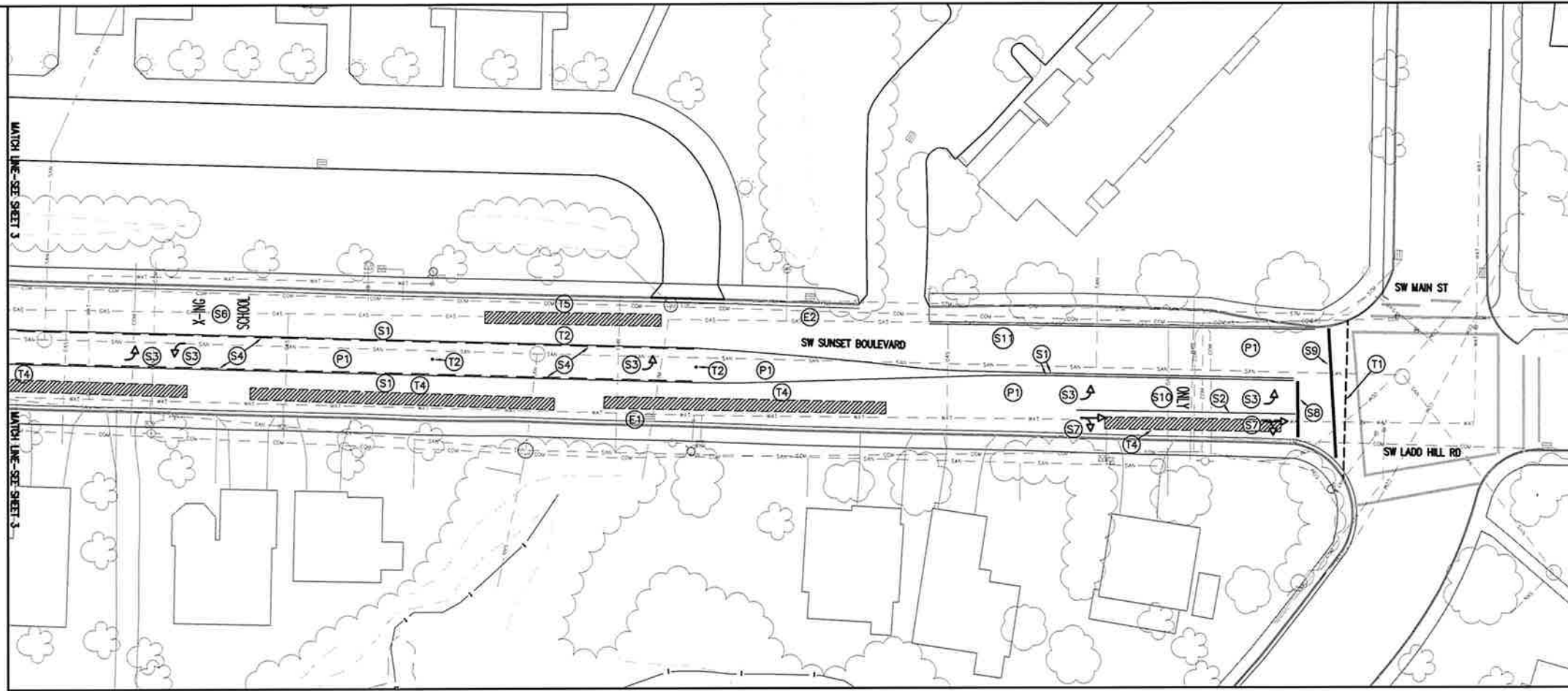
REVISIONS	

**SW SUNSET BOULEVARD
(WEST PLAN VIEW)**

SUNSET PLAN VIEW (WEST)

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 22560 SW PINE STREET
 SHERWOOD, OREGON 97140
 PHONE: (503) 925-2309
 FAX: (503) 625-0629
 E-MAIL: engineering@sherwoodoregon.gov



TIMING NOTES: SIDEWALK RAMP WORK MAY NOT BEGIN UNTIL SCHOOL IS OUT FOR SUMMER (JUNE 17, 2024). GRIND AND INLAY WORK MAY NOT BEGIN UNTIL NW NATURAL HAS COMPLETED PAVEMENT RESTORATION OVER GAS LINE (EXPECTED NW NATURAL START DATE JUNE 17, 2024 WITH APPROXIMATELY 3 WEEKS CONSTRUCTION TIME).

STREET KEY NOTES

- T1 SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM LOCATION WITH INSPECTOR (INCIDENTAL).
- T2 BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS BY CITY OF SHERWOOD
- T3 CAUTION: EXISTING SHALLOW GAS LINE. TO BE REPLACED BY NW NATURAL. GRIND AND INLAY SHALL NOT BE INSTALLED UNTIL NW NATURAL HAS FINISHED TRENCH
- T4 AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY
- T5 AFTER GRINDING, AS DIRECTED BY ENGINEER, REMOVE REMAINING DEPTH OF ASPHALT TO BASE ROCK TO ALLOW FOR PLACEMENT OF 4" THICKNESS OF LEVEL 3, 1/2-INCH, ACP FIBER MIX IN 2 LIFTS (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE PRIOR TO 1-1/2" OVERLAY. THESE AREAS WILL LIKELY BE COMPLETED BY NW NATURAL AFTER SCHOOL IS OUT FOR SUMMER
- T6 AREA OF FULL DEPTH REMOVAL FOR BLENDING AC TO MATCH NEW GUTTER. REGRADE BASE AS NECESSARY AND INSTALL 3" THICKNESS OF LEVEL 3, 1/2" ACP FIBER MIX (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE FOR THE FIRST LIFT, WITH 1-1/2" ASPHALT OVERLAY. CONFIRM LIMITS WITH ENGINEER

PAVING KEY NOTES

- P1 GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. WORK DONE DURING DAYTIME WITH 2 LANES REMAINING OPEN FOR TRAFFIC AND FLAGGERS AS NEEDED.

EROSION CONTROL KEY NOTES

- E1 INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN FRONT OF CURB OPENING
- E2 INSTALL BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN CURB INLET OPENING

STRIPING KEY NOTES

PAVEMENT MARKINGS TO MATCH EXISTING EXCEPT WHEN DIRECTED DIFFERENTLY BY ENGINEER (NO PAVEMENT REFLECTORS REQUIRED)

- S1 INSTALL YELLOW THERMOPLASTIC LINE (4" WIDTH)
- S2 INSTALL WHITE THERMOPLASTIC LINE (8" WIDTH)
- S3 INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- S4 INSTALL DASHED YELLOW THERMOPLASTIC LINE (4" WIDTH)
- S6 INSTALL WHITE THERMOPLASTIC "X-ING SCHOOL"
- S7 INSTALL WHITE THERMOPLASTIC RIGHT TURN STRAIGHT ARROW
- S8 INSTALL WHITE THERMOPLASTIC STOP BAR (12" WIDTH)
- S9 INSTALL WHITE THERMOPLASTIC CROSSWALK LINE (12" WIDTH)
- S10 INSTALL WHITE THERMOPLASTIC "ONLY"
- S11 INSTALL WHITE THERMOPLASTIC LINE (4" WIDTH)

**SW SUNSET BOULEVARD
(EAST PLAN VIEW)**

SUNSET PLAN VIEW (EAST)

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE
CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

**CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140**

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

EXPIRES: 12-31-25

DESIGNED BY:	JMC
DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	1"=30'
DATE:	APRIL, 2024
SUNSET_MENECKE_TMBREL_SHEETS.DWG	

REVISIONS

JOB NO.	
SHEET NO.	4
	11

4-29-24

STREET KEY NOTES

- (T1) EDGE OF STAGING WORK LIMITS
- (T2) UNGRINDED EXISTING SURFACE
- (T3) CLEAN EDGE FROM GRIND MACHINE
- (T4) DO NOT DAMAGE CONCRETE CURB WITH GRINDER. CLEAN EDGE MANUALLY (INCIDENTAL)
- (T5) DO NOT DAMAGE EXISTING STRIPES

PAVING KEY NOTES

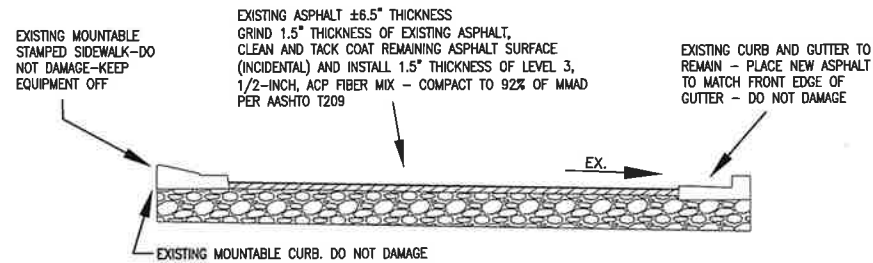
- (P1) GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. MAINTAIN LIP ON INTERIOR WRB

EROSION CONTROL KEY NOTES

- (E1) INSTALL BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM IN FRONT OF CURB OPENING
- (E2) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM IN FRONT OF CURB OPENING

MEDIAN REMOVAL KEY NOTES

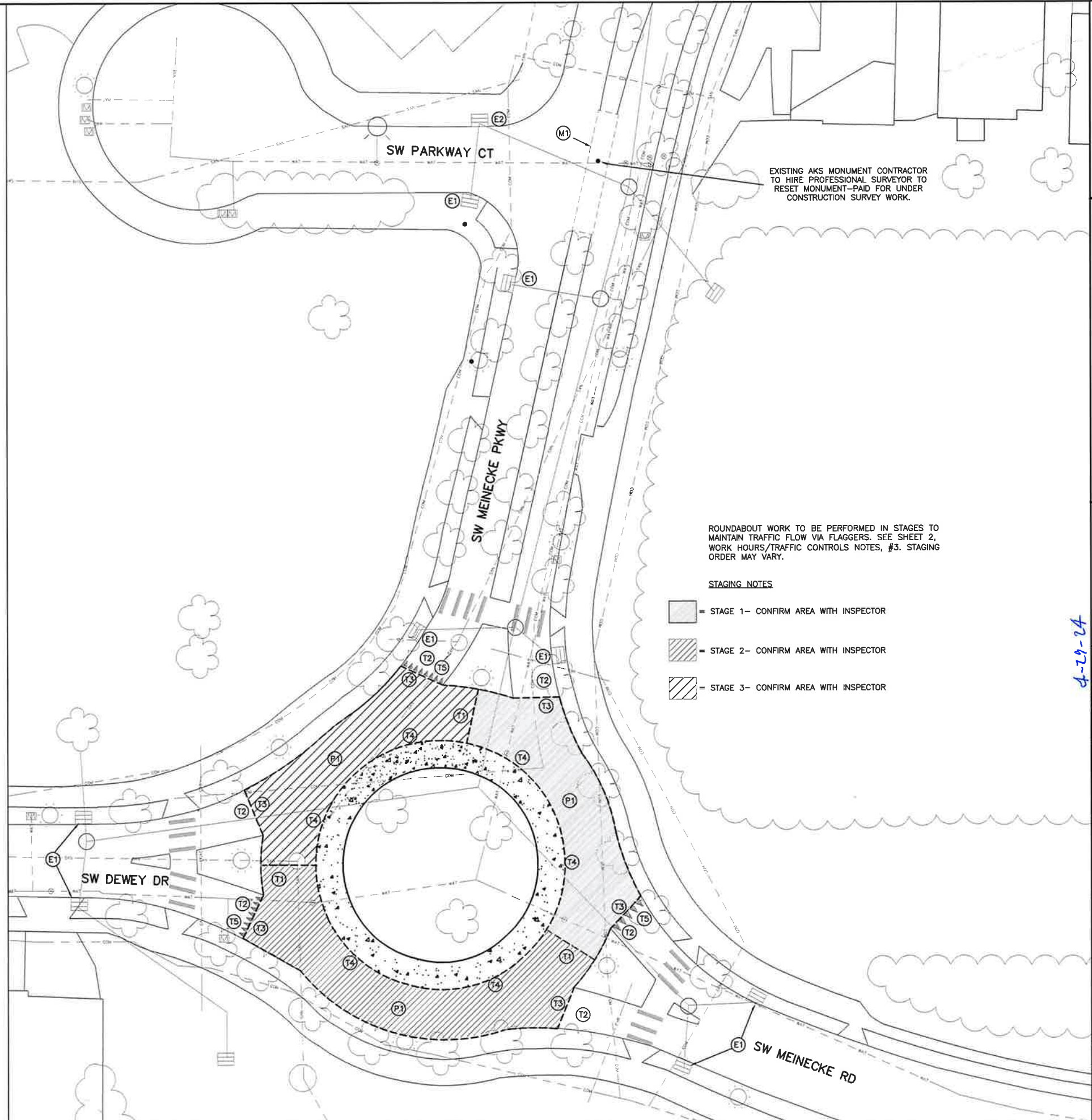
- (M1) AREA OF TEMPORARY MEDIAN REMOVAL FOR TRAFFIC CONTROL (±45FT LENGTH). MEDIAN TO BE RECONSTRUCTED TO SAME OR BETTER CONDITION. REINFORCE VERTICAL CURB WITH REBAR (SEE DETAIL RD-21-M ON SHEET 11). CONTRACTOR TO INSTALL EITHER PLATE OR ASPHALT HOT MIX ON BASE ROCK AND MAINTAIN FOR DURATION OF WORK. DO NOT DAMAGE EXISTING IRRIGATION—COORDINATE IRRIGATION SHUT OFF WITH CITY. MEDIAN WORK TO BE PERFORMED BETWEEN 9AM-4PM VIA FLAGGING TO KEEP BOTH DIRECTION OF TRAFFIC OPEN (MAY NOT HOLD TRAFFIC MORE THAN 5 MINUTES OR QUEUE SUCH THAT 99W IS IMPACTED). OPTION FOR CONTRACTOR TO REMOVE AND PRESERVE OR DISPOSE OF EXISTING PLANTS



TYPICAL GRIND AND INLAY STREET SECTION
NOT TO SCALE



SCALE IN FEET
1" = 20'



EXISTING AKS MONUMENT CONTRACTOR TO HIRE PROFESSIONAL SURVEYOR TO RESET MONUMENT—PAID FOR UNDER CONSTRUCTION SURVEY WORK.

ROUNDABOUT WORK TO BE PERFORMED IN STAGES TO MAINTAIN TRAFFIC FLOW VIA FLAGGERS. SEE SHEET 2, WORK HOURS/TRAFFIC CONTROLS NOTES, #3. STAGING ORDER MAY VARY.

STAGING NOTES

- [Solid Grey Box] = STAGE 1- CONFIRM AREA WITH INSPECTOR
- [Diagonal Lines Box] = STAGE 2- CONFIRM AREA WITH INSPECTOR
- [Cross-hatched Box] = STAGE 3- CONFIRM AREA WITH INSPECTOR

MEINECKE RAB PLAN VIEW

MEINECKE ROUNDABOUT OVERLAY

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22550 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2308
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL ENGINEER
112711
C. CHRIS...
EXPIRES: 12-31-25

DESIGNED BY: JMC
DRAWN BY: JMC
CHECKED BY: CCC
FULL SIZE SCALE: 1"=20'
DATE: APRIL, 2024
SUNSET_MEINECKE_ROUNDABOUT_SHEETS.DWG

Tuesday, April 23, 2024 5:31:51 PM

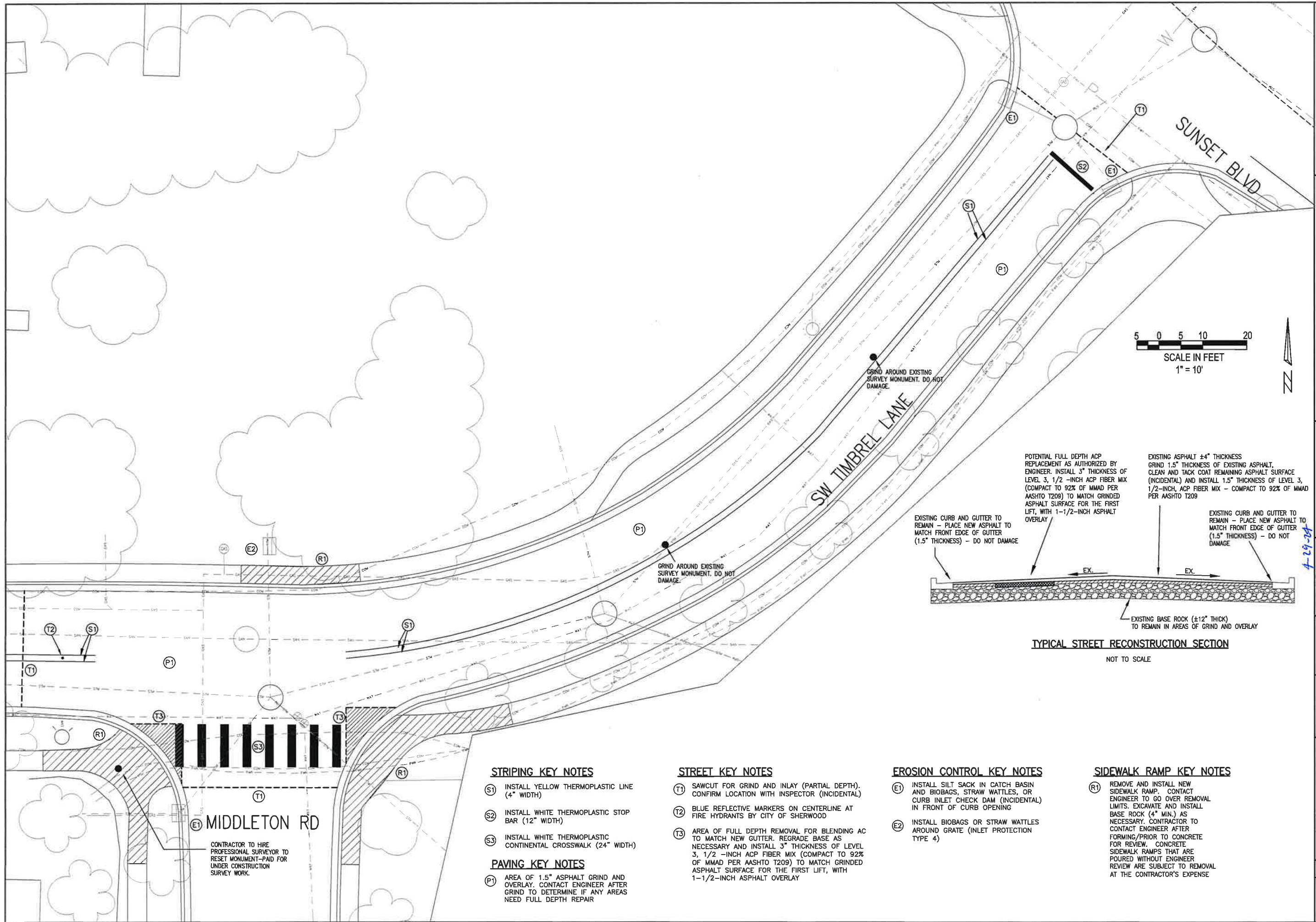
JOB NO.

SHEET NO. **5**

REVISIONS

11

4-29-24



**SW TIMBRE LANE
(PLAN VIEW)**

**TIMBRE PAVEMENT OVERLAY
(SUNSET BLVD - MIDDLETON RD)**
 LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M.
 IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
 STATE OF OREGON

CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 22560 SW PINE STREET
 SHERWOOD, OREGON 97140
 PHONE: (503) 925-2309
 FAX: (503) 625-0629
 E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL SEAL
 CRAIG C. CHARLES
 REGISTERED PROFESSIONAL ENGINEER
 No. 12345
 EXPIRES: 12-31-25

DESIGNED BY: JMC	DRAWN BY: JMC	CHECKED BY: RS	FULL SIZE SCALE: 1"=10'	DATE: APRIL, 2024
SUNSET_MJENCOE_TIMBREL_SHEETS.DWG				

	REVISIONS

JOB NO. _____

SHEET NO. **6**
of 11

STRIPING KEY NOTES

- (S1) INSTALL YELLOW THERMOPLASTIC LINE (4" WIDTH)
- (S2) INSTALL WHITE THERMOPLASTIC STOP BAR (12" WIDTH)
- (S3) INSTALL WHITE THERMOPLASTIC CONTINENTAL CROSSWALK (24" WIDTH)

PAVING KEY NOTES

- (P1) AREA OF 1.5" ASPHALT GRIND AND OVERLAY. CONTACT ENGINEER AFTER GRIND TO DETERMINE IF ANY AREAS NEED FULL DEPTH REPAIR

STREET KEY NOTES

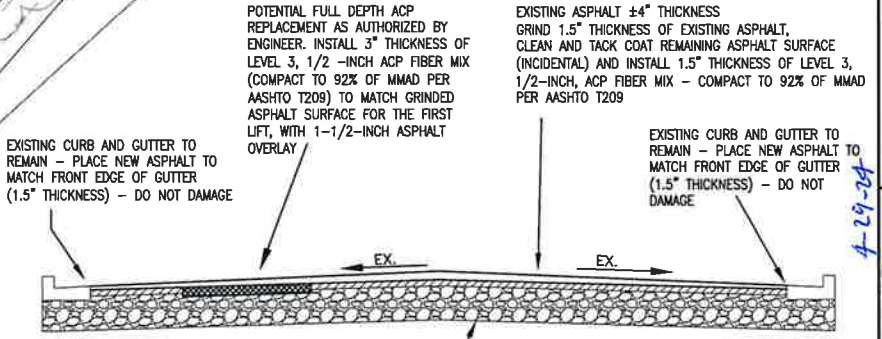
- (T1) SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM LOCATION WITH INSPECTOR (INCIDENTAL)
- (T2) BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS BY CITY OF SHERWOOD
- (T3) AREA OF FULL DEPTH REMOVAL FOR BLENDING AC TO MATCH NEW GUTTER. REGRADE BASE AS NECESSARY AND INSTALL 3" THICKNESS OF LEVEL 3, 1/2 -INCH ACP FIBER MIX (COMPACT TO 92% OF MMAD PER AASHTO T209) TO MATCH GRINDED ASPHALT SURFACE FOR THE FIRST LIFT, WITH 1-1/2-INCH ASPHALT OVERLAY

EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS, STRAW WATTLES, OR CURB INLET CHECK DAM (INCIDENTAL) IN FRONT OF CURB OPENING
- (E2) INSTALL BIOBAGS OR STRAW WATTLES AROUND GRATE (INLET PROTECTION TYPE 4)

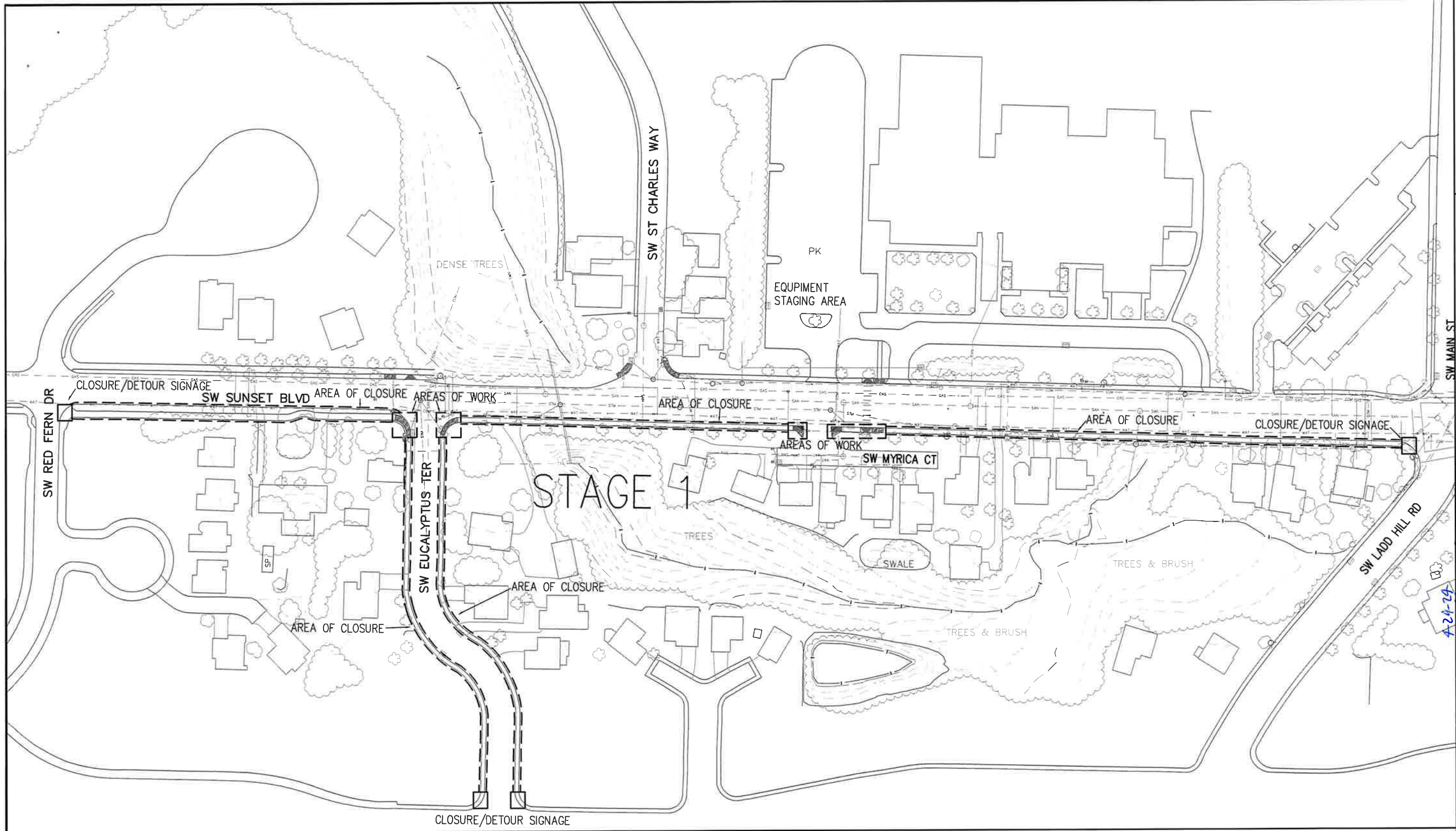
SIDEWALK RAMP KEY NOTES

- (R1) REMOVE AND INSTALL NEW SIDEWALK RAMP. CONTACT ENGINEER TO GO OVER REMOVAL LIMITS. EXCAVATE AND INSTALL BASE ROCK (4" MIN.) AS NECESSARY. CONTRACTOR TO CONTACT ENGINEER AFTER FORMING/PRIOR TO CONCRETE FOR REVIEW. CONCRETE SIDEWALK RAMP THAT ARE POURED WITHOUT ENGINEER REVIEW ARE SUBJECT TO REMOVAL AT THE CONTRACTOR'S EXPENSE



MIDDLETON RD

CONTRACTOR TO HIRE PROFESSIONAL SURVEYOR TO RESET MONUMENT—PAID FOR UNDER CONSTRUCTION SURVEY WORK.



SW SUNSET BOULEVARD

SUNSET STAGING AND PEDESTRIAN CLOSURE (STAGE 1)
 LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

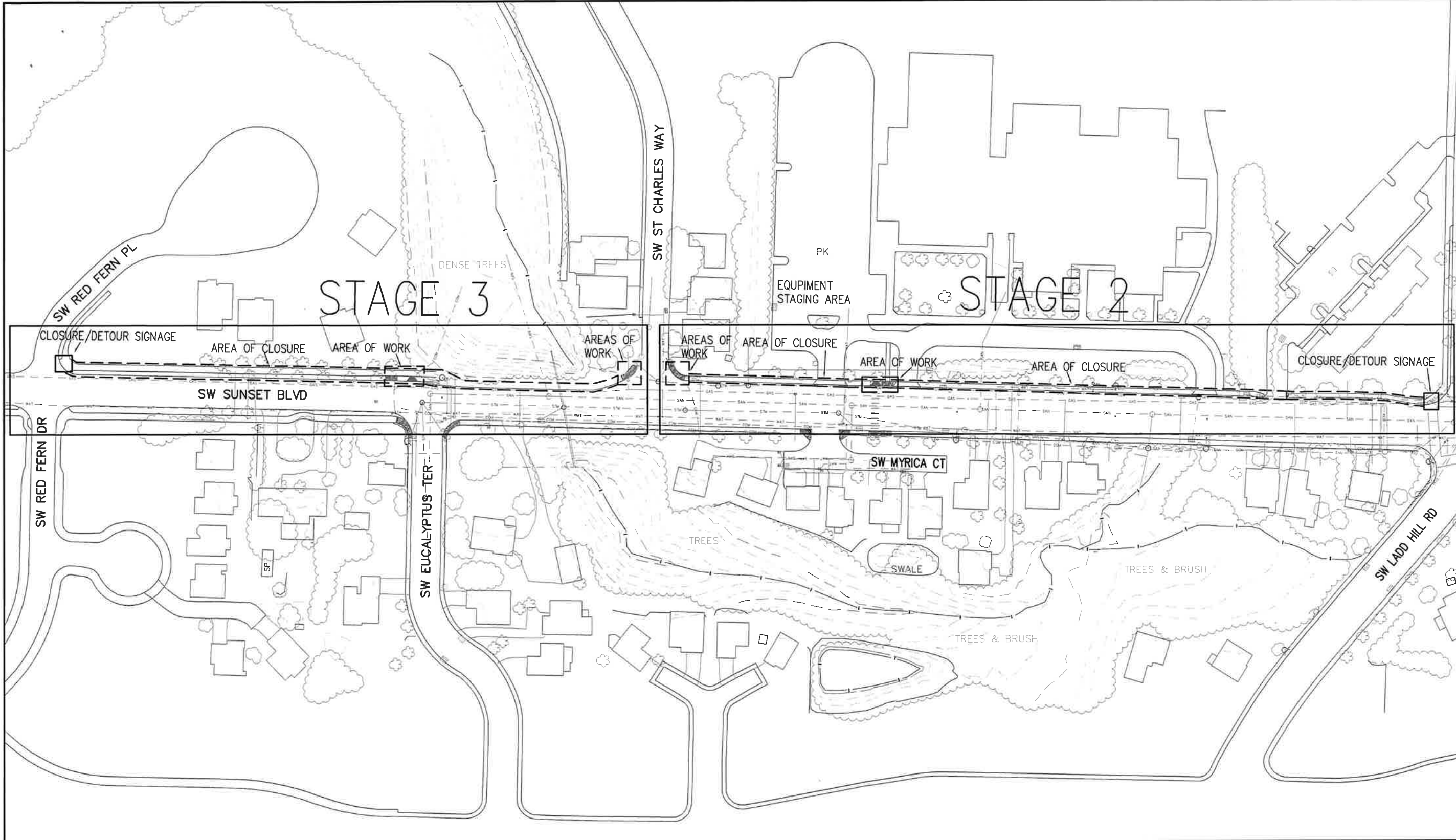
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 E-MAIL: engineering@shermwoodoregon.gov



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CHECKED BY:	CCC
FULL SIZE SCALE:	NOT TO SCALE
DATE:	APRIL, 2024
SUNSET_JADNECKE..._MURIEL...SHEETS.DWG	

REVISIONS	

JOB NO.	
SHEET NO.	7
OF	11



SW SUNSET BOULEVARD

SUNSET STAGING AND PEDESTRIAN
CLOSURE (STAGE 2 & 3)
LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

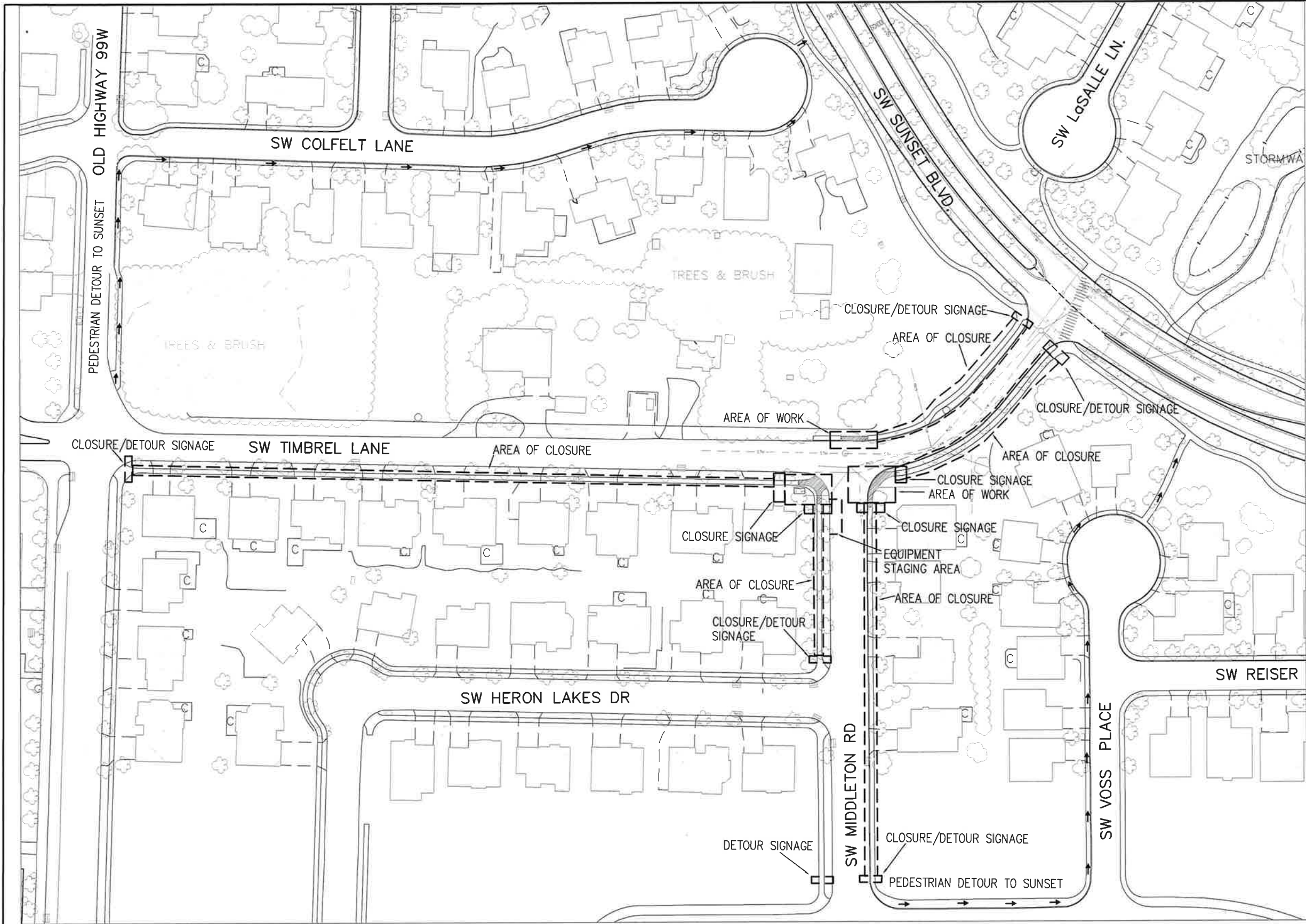
CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
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E-MAIL: engineering@sherwoodoregon.gov



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DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	NOT TO SCALE
DATE:	APRIL, 2024
SUNSET_MJNECKE_TMBREL_SHEETS.DWG	

NO.	DATE	REVISIONS

JOB NO.	
SHEET NO.	8
TOTAL SHEETS	11



SW TIMBREL LANE
(PLAN VIEW)

TIMBRELSIDEWALK
CLOSURE

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov



DESIGNED BY:	JMC
DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	NOT TO SCALE
DATE:	APRIL, 2024
SUNSET_JMENECKE_TIMBREL_SHEETS.DWG	

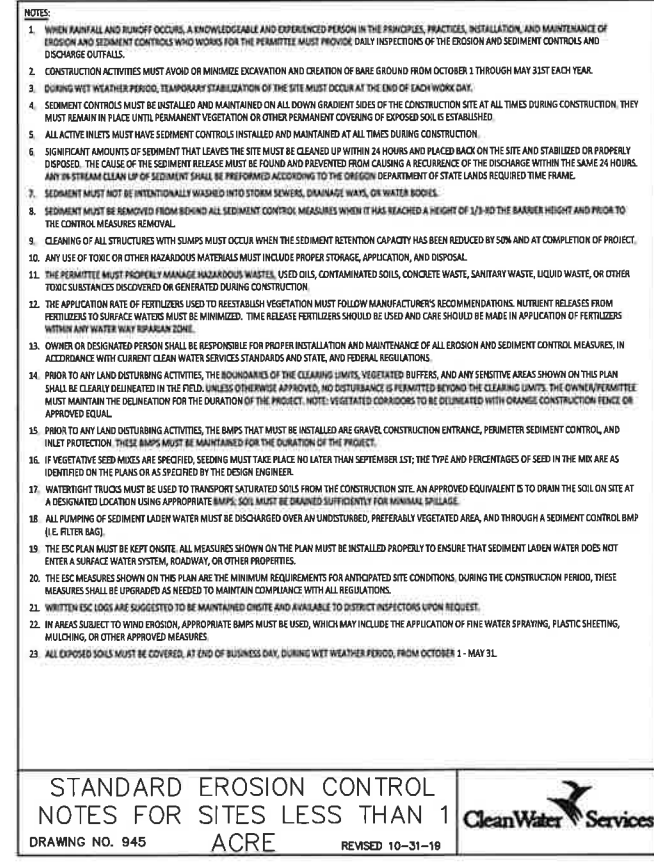
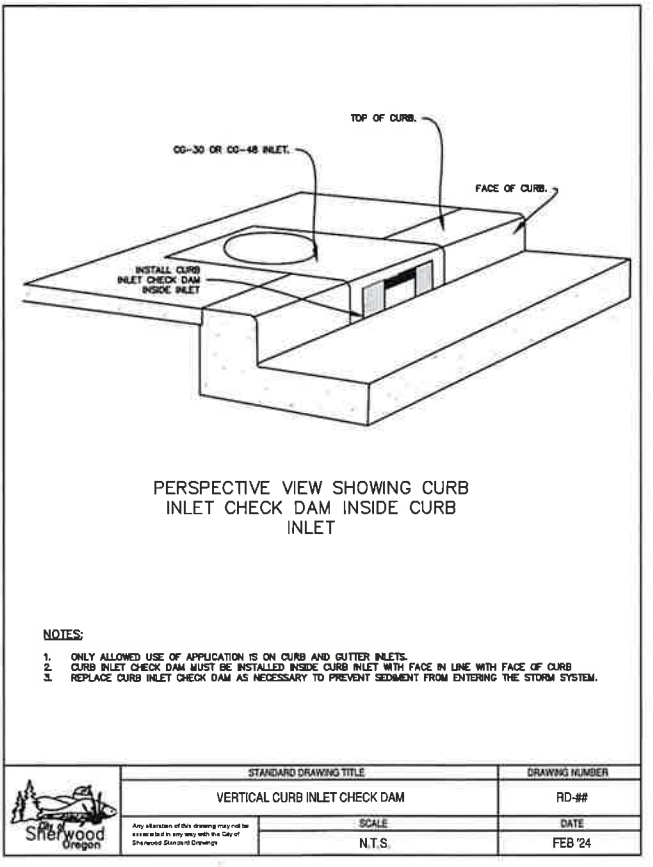
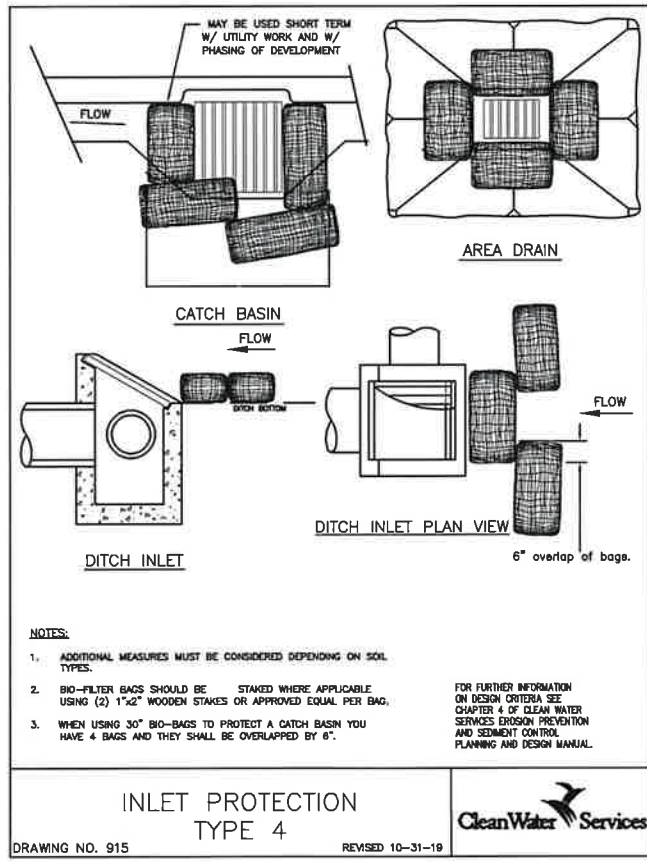
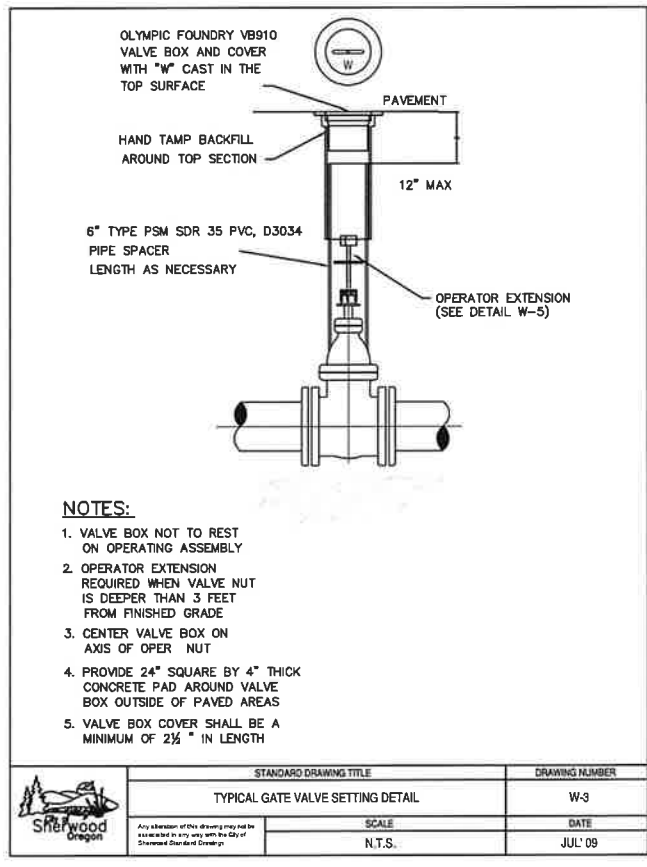
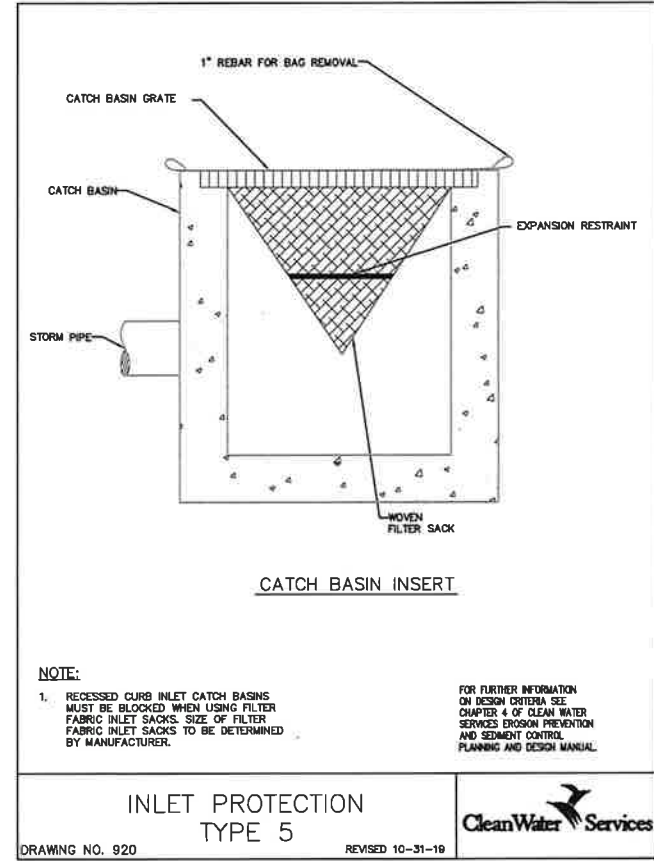
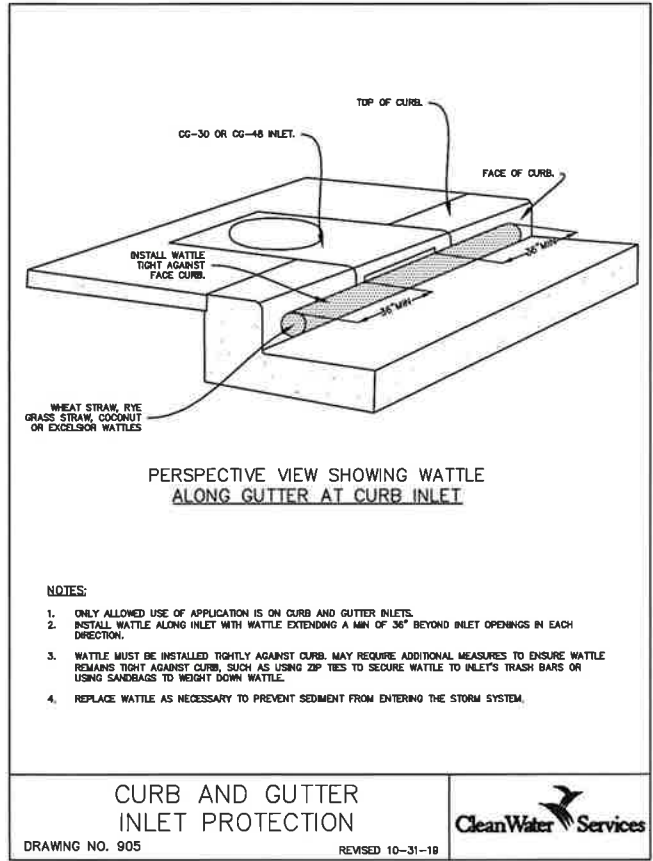
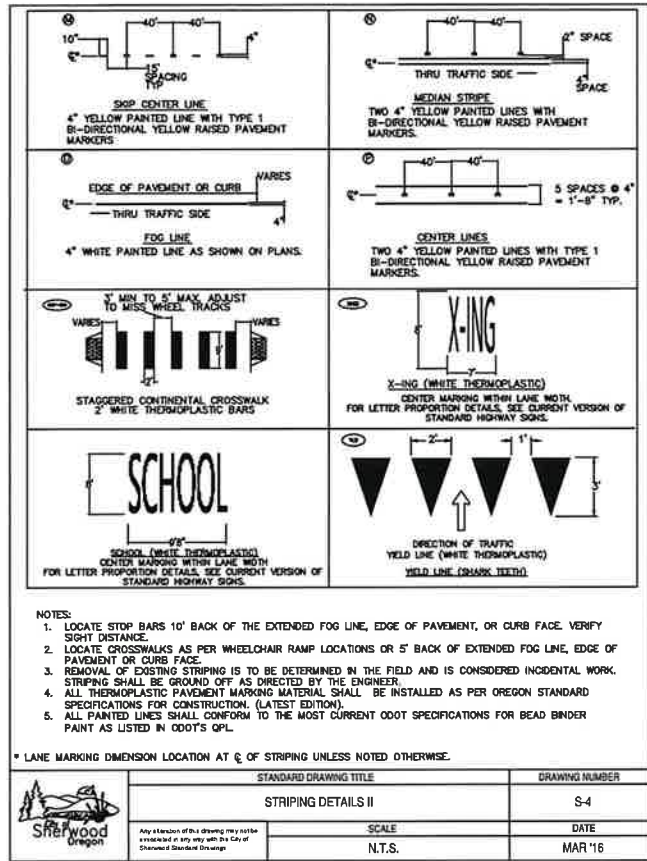
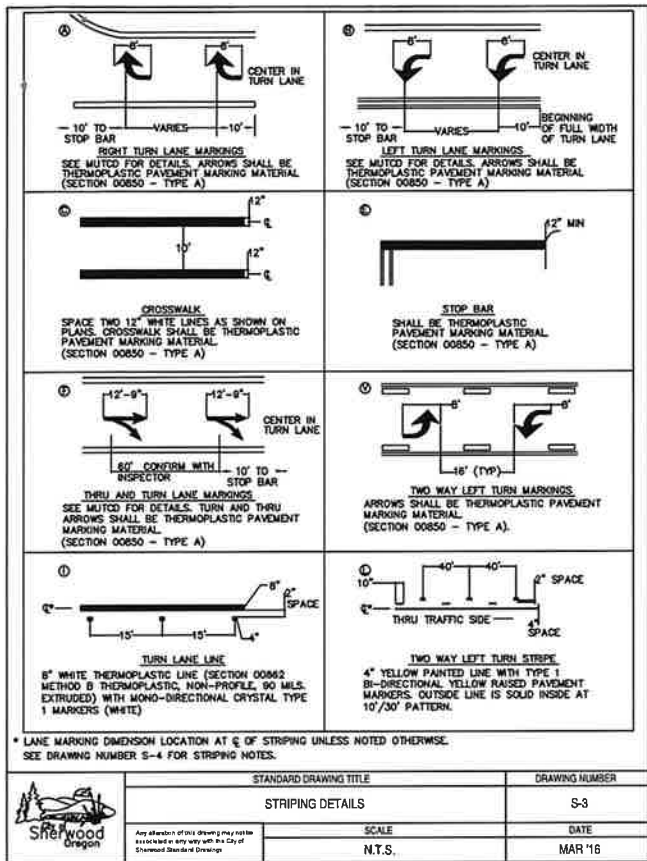
NO.	DESCRIPTION

JOB NO.	
SHEET NO.	9
TOTAL SHEETS	11

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

4-29-24





CITY OF SHERWOOD DETAILS

SUNSET, TIMBREL, MEINECKE RAB PAVEMENT OVERLAY DETAILS

LOCATED IN SECTIONS 31A, 31C, & 32D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, OREGON

CITY OF SHERWOOD ENGINEERING DEPARTMENT
22580 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@shermoregon.gov

EXPIRES: 12-31-25

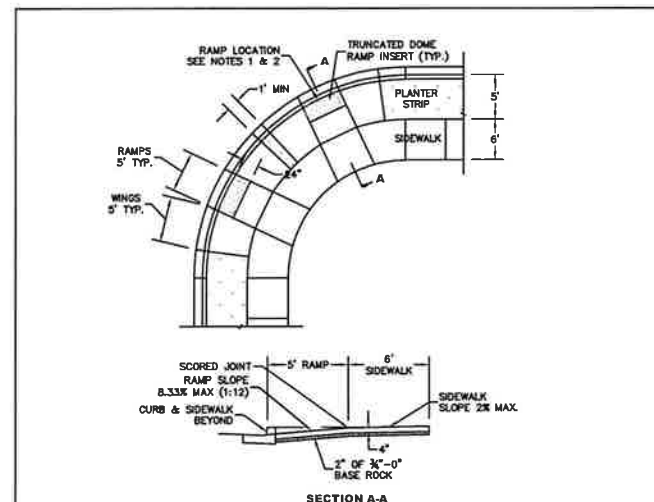
DESIGNED BY: JMC
DRAWN BY: JMC
CHECKED BY: CCC
FULL SIZE SCALE: AS NOTED
DATE: APRIL 2024

SUNSET_MEINECKE_TIMBREL_SHEETS.DWG

REVISIONS

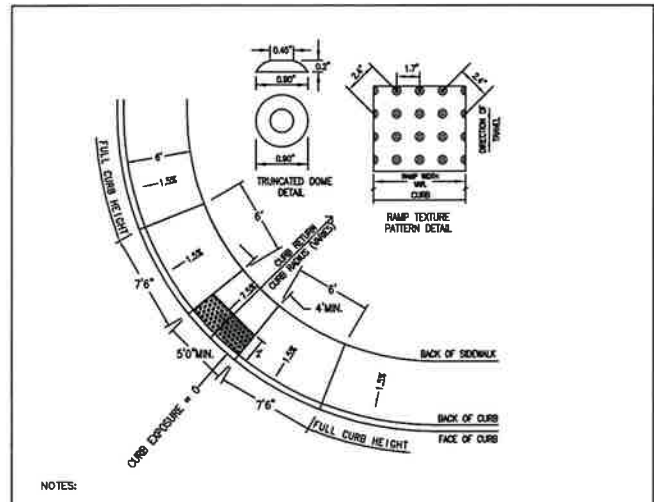
JOB NO.

SHEET NO. 10 OF 11



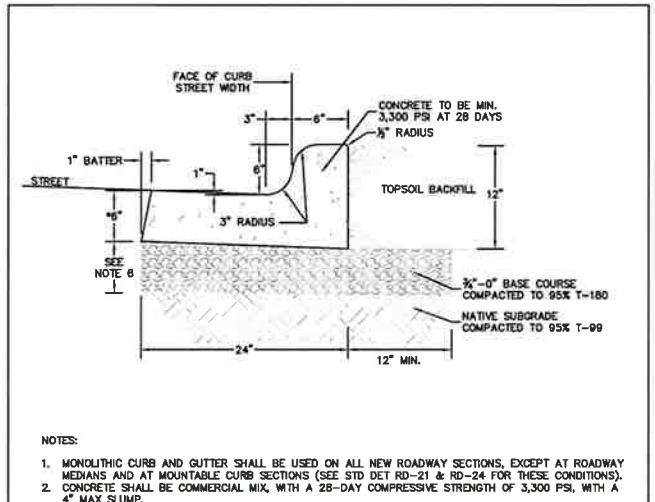
- NOTES:
- PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
 - PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
 - SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
 - DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
 - CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
 - CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3,300 PSI, WITH A 4" MAX SLUMP.
 - ¾"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
 - BEVEL THE CURB OUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
 - SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
 - CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
 - PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

STANDARD DRAWING TITLE		DRAWING NUMBER
DOUBLE CROSSING CURB RETURN & RAMP DETAIL		RD-44
SCALE	DATE	
N.T.S.	MAR '16	



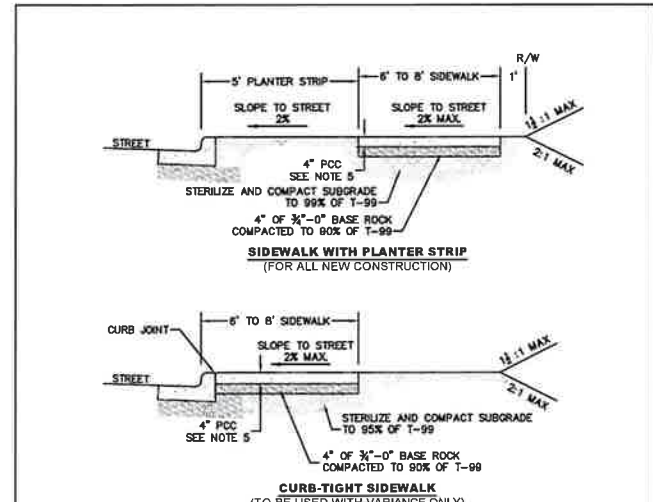
- NOTES:
- PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
 - PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
 - SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
 - PLANTER STRIP DETAIL - REFER TO DOT STANDARD DRAWING RD 755 (SIDEWALK RAMP DETAIL).
 - DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
 - CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
 - CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3,300 PSI WITH A 4" MAX SLUMP.
 - ¾"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
 - BEVEL THE CURB OUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
 - SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
 - CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
 - PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

STANDARD DRAWING TITLE		DRAWING NUMBER
CURB-TIGHT CURB RETURN & RAMP DETAIL		RD-43
SCALE	DATE	
N.T.S.	MAR '16	



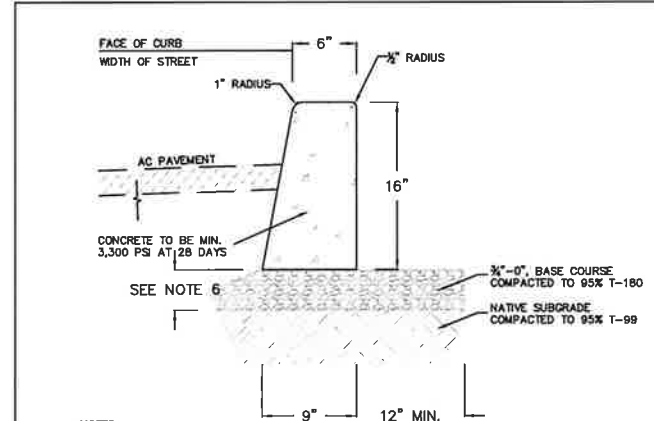
- NOTES:
- MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
 - CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PSI, WITH A 4" MAX SLUMP.
 - EXPANSION JOINTS TO BE PROVIDED AT EACH:
 - POINT OF TANGENCY.
 - COLD JOINT.
 - SIDE OF INLET STRUCTURES.
 - SIDE OF DRIVEWAYS.
 - EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF ½".
 - CONTRACTION JOINTS SHALL HAVE:
 - SPACING OF NOT MORE THAN 15 FEET.
 - DEPTH OF JOINT OF AT LEAST 1½".
 - BASE ROCK SHALL BE ¾"-0", COMPACTED TO 90% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.
 - FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23.
 - COMMERCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.

STANDARD DRAWING TITLE		DRAWING NUMBER
MONOLITHIC CURB AND GUTTER		RD-22
SCALE	DATE	
N.T.S.	MAR '16	



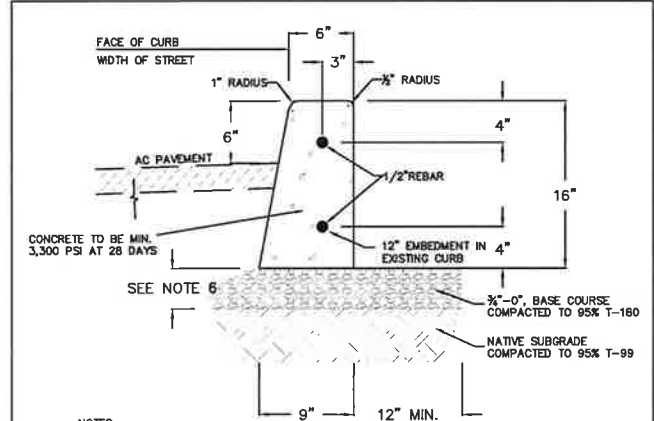
- NOTES:
- CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI @ 28 DAYS, WITH A 4" MAX SLUMP.
 - SIDEWALK PANELS TO BE SQUARE (6' LONG x 6' WIDE TYP.).
 - EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, CURB RAMPS, AND/OR POINTS OF TANGENCY IN CURB AS SHOWN ON THE STANDARD DRAWINGS FOR SIDEWALK RAMPS, AND AT SPACING NOT TO EXCEED 45'.
 - FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROWELED JOINT WITH A MINIMUM ½" RADIUS.
 - SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4". IF MOUNTABLE CURB IS USED, OR IF SIDEWALK IS INTENDED AS PORTION OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6" MINIMUM THICKNESS, COMMERCIAL 8".
 - CONCRETE SHALL HAVE A BROOM FINISH, ALL JOINTS SHALL BE EDGED WITH 3" SHINE.
 - WIDTH OF PLANTER STRIP AND SIDEWALK IS MEASURED FROM FACE OF CURB.
 - IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 1" PAST BACK OF SIDEWALK WITH A 3" DIAMETER ADS PIPE. CONTRACTION JOINT SHALL BE PLACED OVER PIPE.

STANDARD DRAWING TITLE		DRAWING NUMBER
SIDEWALK DETAIL		RD-26
SCALE	DATE	
N.T.S.	MAR '16	



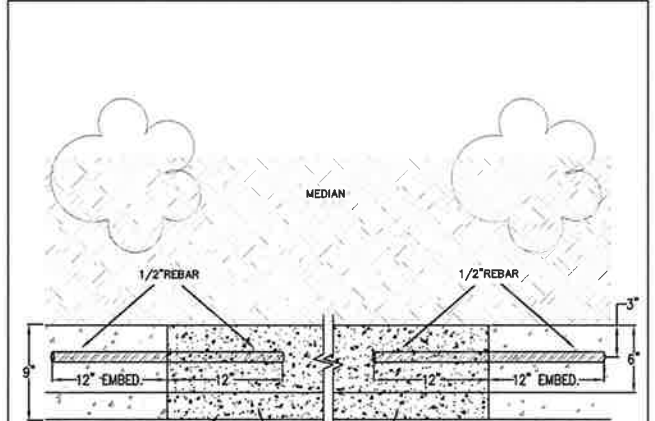
- NOTES:
- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
 - CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
 - EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
 - MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF ½" INCH.
 - CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1½" IN DEPTH.
 - BASE ROCK: ¾"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

STANDARD DRAWING TITLE		DRAWING NUMBER
VERTICAL CURB		RD-21
SCALE	DATE	
N.T.S.	MAR '16	



- NOTES:
- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
 - CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
 - EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
 - MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF ½" INCH.
 - CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1½" IN DEPTH.
 - BASE ROCK: ¾"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

STANDARD DRAWING TITLE		DRAWING NUMBER
REBAR CONNECTION DETAIL (PROFILE VIEW)		RD-24
SCALE	DATE	
N.T.S.	APRIL '24	



- NOTES:
- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
 - CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
 - EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
 - MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF ½" INCH.
 - CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1½" IN DEPTH.
 - BASE ROCK: ¾"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

STANDARD DRAWING TITLE		DRAWING NUMBER
REBAR CONNECTION DETAIL (PLAN VIEW)		RD-24
SCALE	DATE	
N.T.S.	APRIL '24	

CITY OF SHERWOOD DETAILS
SUNSET, TIMBREL, MEINECKE RAB
PAVEMENT OVERLAY DETAILS
 LOCATED IN SECTIONS 31A, 31C, & 32D, 12S, R1W,
 W.M. IN THE CITY OF SHERWOOD, WASHINGTON
 COUNTY OF WASHINGTON
 STATE OF OREGON

CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 22560 SW PINE STREET
 SHERWOOD, OREGON 97140
 PHONE: (503) 925-2309
 FAX: (503) 625-0629
 E-MAIL: engineering@sherwoodoregon.gov
 EXPIRES: 12-31-25

4-24-24

DESIGNED BY:	JMC
DRAWN BY:	JMC
CHECKED BY:	CCC
FULL SIZE SCALE:	AS NOTED
DATE:	APRIL, 2024

SUNSET_MEINECKE_TIMBREL_SHEETS.DWG

REVISIONS	

JOB NO.

SHEET NO.	11
OF	11