

CITY OF SHERWOOD

2023 CATCH BASIN REPLACEMENT

SEPTEMBER 2023

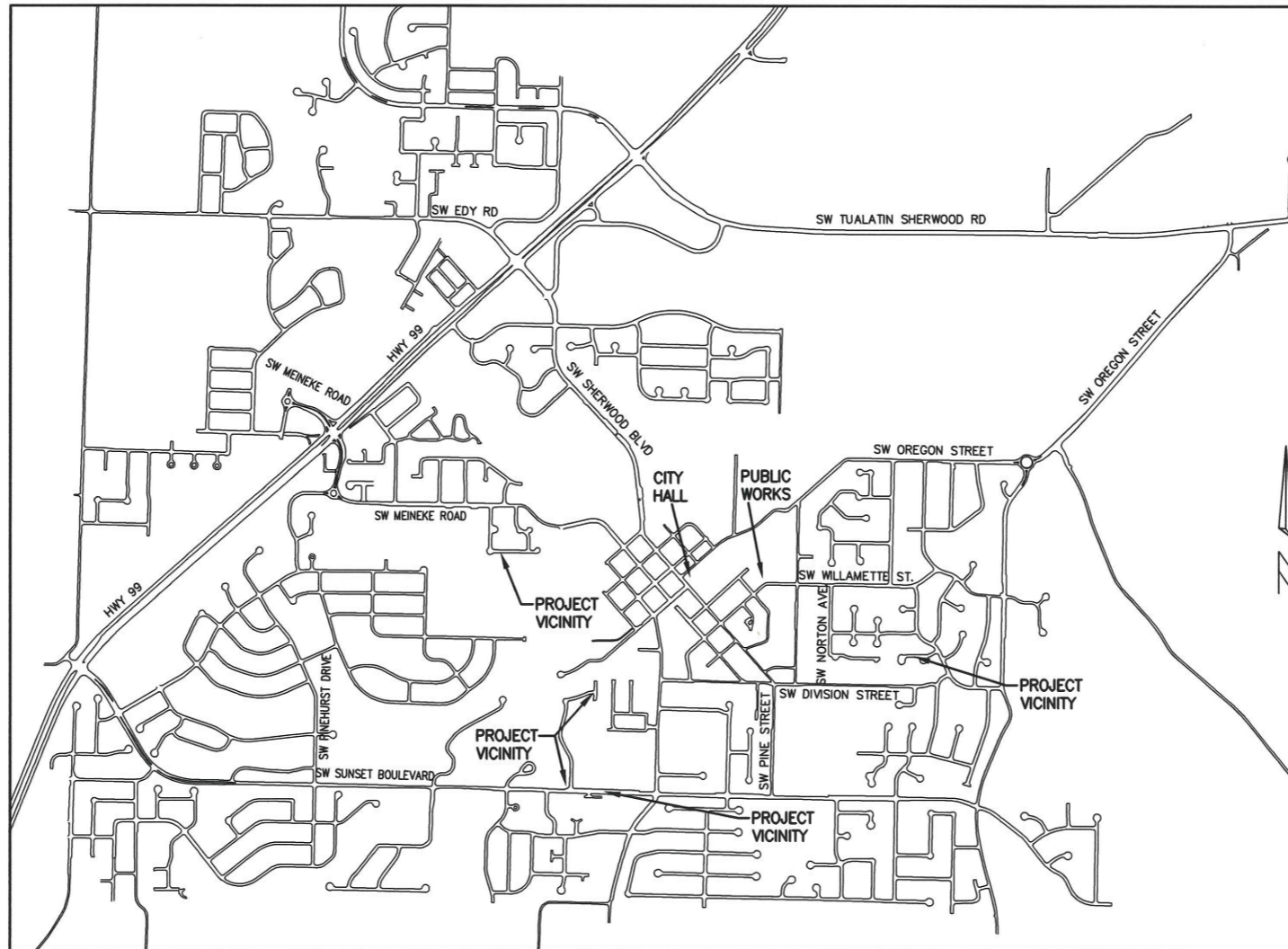
(CONTRACT DRAWINGS)

PROJECT LOCATION:
 SW COCHRAN STREET
 SW ST. CHARLES WAY
 SW SUNSET BLVD. EAST OF SW MYRICA CT.
 SW TRAVIS COURT

DEVELOPER/OWNER:
 CITY OF SHERWOOD
 22560 SW PINE ST
 SHERWOOD, OREGON 97140
 CONTACT: CRAIG CHRISTENSEN
 PH. 503-925-2301
 CHRISTENSEN@SHERWOODOREGON.GOV

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THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED AREA.



VICINITY MAP
 NOT TO SCALE

INSPECTOR INFORMATION:

CITY OF SHERWOOD: ANDY STIRLING (503) 925-2307
 CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL (503) 246-6699.

COVER SHEET	
2023 CATCH BASIN REPLACEMENT LOCATED IN SECTION 31A, 31C & 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON	
CITY OF SHERWOOD ENGINEERING DEPARTMENT 22560 SW PINE STREET SHERWOOD, OREGON 97140 PHONE: (503) 925-2309 FAX: (503) 625-0629 E-MAIL: engineering@sherwoodoregon.gov	
DESIGNED BY: CCC DRAWN BY: CCC CHECKED BY: RS/AS FULL SIZE SCALE: AS NOTED DATE: SEPTEMBER 2023	2023 CB REPLACEMENT_SHEETS.DWG REVISIONS
JOB NO.	
SHEET NO.	
SHEET 1 OF 7	

9-7-23

GENERAL NOTES

- ALL CONSTRUCTION WORK, MATERIALS AND TESTING SHALL CONFORM TO THE APPLICABLE SECTION OF THE CITY OF SHERWOOD ENGINEERING DESIGN AND STANDARD DETAILS MANUAL. CONTRACTOR AND SUBCONTRACTOR(S) SHALL HAVE A MINIMUM OF ONE SET OF APPROVED PLANS AND CITY OF SHERWOOD CONSTRUCTION SPECIFICATIONS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROCURE ALL APPLICABLE PERMITS, LICENSES, AND CERTIFICATES RELATIVE TO THE TRADES TO COMPLETE THE PROJECT AND FOR THE USE OF SUCH WORK WHEN COMPLETED. COMPLIANCE SHALL BE AT ALL LEVELS, FEDERAL, STATE, COUNTY, AND LOCAL, RELATING TO THE PERFORMANCE OF THIS WORK. A CITY OF SHERWOOD OR A METRO BUSINESS LICENSE IS REQUIRED FOR THE CONTRACTOR AND ALL SUBCONTRACTORS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION AND ARRANGE FOR PROTECTION OF UTILITIES IN CONFLICT WITH PROPOSED WORK. THE LOCATIONS, DEPTH AND DESCRIPTION OF EXISTING UTILITIES SHOWN WERE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. ADDITIONAL UTILITIES MAY EXIST WITHIN THE WORK AREA. POTHOLING PRIOR TO CONSTRUCTION MAY BE NECESSARY. ALL WORK TO VERIFY UTILITY LOCATIONS AND DEPTHS IS INCIDENTAL.
- OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT WWW.STATE.OR.US/GOVERNMENT.HTM. THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL (503) 246-6699.
- THE CONTRACTOR SHALL MAKE PROVISIONS TO KEEP ALL EXISTING UTILITIES IN SERVICE AND PROTECT THEM DURING CONSTRUCTION. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGED UTILITIES USING MATERIAL AND METHODS APPROVED BY THE UTILITY OWNER. NO SERVICE INTERRUPTIONS SHALL BE PERMITTED WITHOUT PRIOR WRITTEN AGREEMENT WITH THE UTILITY PROVIDER AND NOTIFICATION TO SERVICE CUSTOMER. PLEASE NOTE THE STORM SEWER IN THIS AREA IS SHALLOW - DO NOT DAMAGE. REPAIR TO UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PRUNE ALL VEGETATION, AS NECESSARY FOR EQUIPMENT CLEARANCES AWAY AND UP FROM THE STREET AND SIDEWALK. AS WELL AS ANY ROOT PRUNING AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING THAT IS TO REMAIN. DAMAGE TO TREES AND SHRUBS IS THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR. COST OF TREE PRUNING IS INCIDENTAL.
- CONTRACTOR SHALL NOTIFY THE CITY OF SHERWOOD 48 HOURS IN ADVANCE OF STARTING CONSTRUCTION AND 24 HOURS BEFORE RESUMING WORK AFTER SHUTDOWNS, EXCEPT FOR NORMAL RESUMPTION OF WORK FOLLOWING SATURDAYS, SUNDAYS, OR HOLIDAYS.
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, EXCESS TOPSOIL, AND OTHER EXCESS MATERIAL OFF SITE. MATERIALS SHALL BE DISPOSED OF IN SUCH A MANNER AS TO MEET ALL APPLICABLE REGULATIONS. DISPOSAL LOCATION INFORMATION TO BE PROVIDED TO CITY PRIOR TO BEGINNING ANY WORK.
- CONSTRUCTION VEHICLES SHALL PARK AT A LOCATION APPROVED BY THE INSPECTOR OR AT A LOCATION(S) INDICATED ON AN APPROVED PLAN. HOURS OF CONSTRUCTION SHALL BE AS SPECIFIED IN THE PLANS - UNLESS OTHERWISE APPROVED BY THE INSPECTOR. CONSTRUCTION IS PROHIBITED ON SATURDAY AND SUNDAY UNLESS WRITTEN APPROVAL IS OBTAINED FROM THE PUBLIC WORKS DIRECTOR AND CITY MANAGER. CONSTRUCTION ACTIVITIES INCLUDE ALL FIELD MAINTENANCE OF EQUIPMENT, REFUELING, AND PICK UP AND DELIVERY OF EQUIPMENT AS WELL AS THE ACTUAL CONSTRUCTION ACTIVITY.
- AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN UP THE PROJECT AREA AND LEAVE IT IN A NEAT AND SECURED MANNER. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL.
- THE CONTRACTOR SHALL KEEP AN APPROVED SET OF PLANS ON THE PROJECT SITE AT ALL TIMES WITH ALL KNOWN CHANGES REDLINED ON THE WORKING PLAN SET FOR THE SOLE PURPOSE OF RECORDING AS BUILT INFORMATION OF IMPROVEMENTS AND LOCATION AND DEPTH OF ANY EXISTING UTILITIES ENCOUNTERED.
- ANY ALTERATIONS OR VARIATIONS FROM THESE PLANS EXCEPT MINOR FIELD ADJUSTMENTS NEEDED TO MEET EXISTING FIELD CONDITIONS, SHALL BE APPROVED BY THE ENGINEER AND APPLICABLE REGULATORY AGENCY REPRESENTATIVE. CONTRACTOR SHALL KEEP RECORD OF ALL CHANGES AND NOTE ON AS BUILT PLANS. CONTRACTOR SHALL SUBMIT ACCURATE AND COMPLETE "AS-BUILT" INFORMATION TO THE ENGINEER UPON COMPLETION OF CONSTRUCTION.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THIS PROJECT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET THE INTENT OF THE PROJECT CONTRACT DOCUMENTS, APPLICABLE AGENCY REQUIREMENTS AND OTHER WORK AS NECESSARY TO PROVIDE A COMPLETE PROJECT.
- CONTRACTOR SHALL PROVIDE EFFECTIVE EROSION PROTECTION TO INCLUDE, BUT NOT BE LIMITED TO, GRADING, DITCHING, STRAW WATTLES, TRIANGULAR SILT DAMS, SILT FENCING, AND SEDIMENT BARRIERS TO MINIMIZE EROSION AND IMPACT TO ADJACENT PROPERTY. SEE EROSION AND SEDIMENT CONTROL NOTES AND PLANS.
- THE CONTRACTOR SHALL MAINTAIN AND COORDINATE ACCESS TO ALL AFFECTED PROPERTIES. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS A MINIMUM OF 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURES AND ASSIST PROPERTY OWNER WITH ALTERNATIVE PARKING AND ACCESS. PEDESTRIAN ACCESS TO ENTRANCES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL PROVIDE RAMPS OR OTHER APPROVED METHODS FOR MAINTAINING ACCESS TO ENTRANCES. RAMPS SHALL BE ADA COMPLIANT, APPROVED BY THE ENGINEER, AND INCIDENTAL TO THE CONTRACT. RESIDENTIAL DRIVEWAYS MAY ONLY BE CLOSED FOR A MAXIMUM OF ONE DAY (8:00 AM TO 6:00 PM).
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND VERIFY ALL EXISTING CONDITIONS BEFORE THE START OF WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY FIELD MEASUREMENTS AND OTHERWISE VERIFY ALL DIMENSIONS AND EXISTING CONSTRUCTION CONDITIONS INDICATED AND/OR SHOWN ON THE PLANS. SHOULD ANY ERROR OR INCONSISTENCY EXIST, THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK AFFECTED UNTIL REPORTED TO THE PROJECT ENGINEER FOR CLARIFICATION OR CORRECTION.
- ANY INSPECTION BY THE CITY, COUNTY, STATE, FEDERAL AGENCY OR PROJECT ENGINEER SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN COMPLIANCE WITH THE APPLICABLE CODES, REGULATIONS, CITY STANDARDS AND PROJECT CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN CONFORMING TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION AND ACCORDING TO PROJECT SPECIFICATIONS. TRAFFIC CONTROL PLAN SHALL MATCH APPROVED PHASING/SEQUENCING PLAN AND SHALL BE SUBJECT TO THE APPROVAL BY THE CITY, WASHINGTON COUNTY AND/OR ODOT AS APPLICABLE.
- THE CONTRACTOR SHALL PROVIDE TO THE CITY PROJECT MANAGER AND INSPECTOR A 24 HOUR CONTACT PERSON AND CELL PHONE NUMBER.
- NO TRENCHES OR PITS WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT. ALL TRENCHES AND PITS SHALL BE COVERED WITH STEEL PLATES OR FILLED IN AT NIGHT.
- PROPERTY LINES SHOWN ON ALL PLAN SHEETS ARE FOR GENERAL DELINEATION ONLY AND ARE, BY NO MEANS, MEANT TO REPRESENT THE ACTUAL BOUNDARIES.

- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL CONSTRUCTION SAFETY, HEALTH AND OTHER RULES AND REGULATIONS FROM OSHA, DEQ, STATE, AND LOCAL REGULATING AGENCIES FOR SAFETY AND INSTALLATION OF THE WORK INCLUDING BUT NOT LIMITED TO SHORING, BRACING, ERECTION/INSTALLATION, FALL PROTECTION, GUARDRAILS, ETC.
- CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING STRUCTURES AND UTILITIES NOT SHOWN TO BE REMOVED. CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING STRUCTURES (SIDEWALKS, DRIVEWAYS, CURB, FENCE, STREET TREES, ETC.) DAMAGED DURING CONSTRUCTION, IN ACCORDANCE WITH CITY STANDARDS.
- ALL LINework SHOWN ON THE PLAN IS FROM A 2004 AERIAL MAP AND FROM VISUAL OBSERVATION AND IS THEREFORE NOT GUARANTEED FOR ACCURACY AND SHOULD NOT BE USED FOR OTHER PURPOSES.

EROSION CONTROL NOTES

- THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION/LANDSCAPING IS ESTABLISHED.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED AFTER PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS RE-ESTABLISHED.
- IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH MAY 31ST, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL (CHAPTER 4) OF CLEAN WATER SERVICES FOR REQUIREMENTS.
- THE CONTRACTOR SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
- ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (FILTER BAG, DIRT BAG, ETC.)
- SITE EROSION CONTROL PLAN AND BMP'S MEETING CWS STANDARDS TO BE IN PLACE AND APPROVED PRIOR TO CONSTRUCTION.
- TRACKING OF MATERIALS OUTSIDE OF THE WORK ZONE, ONTO ADJACENT STREETS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE AND CLEAN TO THE SATISFACTION OF THE INSPECTOR. THIS INCLUDES BUT IS NOT LIMITED TO SOIL, AGGREGATE, ASPHALT DEBRIS, ETC.

STREET NOTES

- TACK COAT SHALL BE APPLIED AGAINST CONCRETE GUTTER EDGE, CURB FACE, CONCRETE STRUCTURES, EXISTING ASPHALT EDGE, MANHOLE FRAMES, BLOW-OFF FRAMES, CLEANOUT FRAMES AND OTHER SURFACES OR STRUCTURES THAT WILL BE PAVED AGAINST WITH NEW ASPHALT (INCIDENTAL) PRIOR TO PAVING.
- EQUIPMENT PARKED ON SITE SHALL BE PARKED AWAY FROM AN INTERSECTION AT A LOCATION APPROVED BY THE INSPECTOR.
- WATER FOR CONSTRUCTION MAY BE OBTAINED FROM CITY FIRE HYDRANTS. INSTALL FIRE HYDRANT METER PRIOR TO DRAWING WATER. FIRE HYDRANT METER TO BE OBTAINED FROM CITY OF SHERWOOD WATER DEPARTMENT. COORDINATE WITH INSPECTOR. WATER FOR CONSTRUCTION IS INCIDENTAL.
- REPAIR ALL DISTURBED NEIGHBORING PROPERTY BACK TO ORIGINAL OR BETTER CONDITION.
- ANY EXISTING SURFACE TO REMAIN (CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, ETC.) THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED AND THE CONTRACTOR'S EXPENSE.
- ASPHALT JOINTS BETWEEN NEW ASPHALT AND EXISTING ASPHALT AND BETWEEN NEW ASPHALT AND EXISTING OR NEW CURB SHALL BE SEALED WITH HOT RUBBERIZED ASPHALT SEALER OR APPROVED EQUIVALENT.

STORM SEWER NOTES

- NO TESTING OF STORM SEWER IS REQUIRED FOR THIS PROJECT.
- ANY NECESSARY COUPLINGS SHALL BE STRONG BACK FERRO OR APPROVED EQUIVALENT.

LEGEND

=====	= EXISTING CURB AND GUTTER	EX	= EXISTING
=====	= EXISTING CURB AND GUTTER	S/W	= SIDEWALK
⊗ WV	= EXISTING WATER VALVE	R/W	= RIGHT-OF-WAY
— W —	= EXISTING WATER LINE	D/W	= DRIVEWAY
— ST —	= EXISTING STORM LINE	F.H.	= FIRE HYDRANT
— SAN —	= EXISTING STORM LINE	WV	= WATER VALVE
— GAS —	= EXISTING GAS LINE	R	= RADIUS
— COM —	= EXISTING TELECOMMUNICATION LINE	CL	= CENTER LINE
— POW —	= EXISTING ELECTRICAL LINE	TOC	= TOP OF CURB
— — —	= CENTER LINE	FG	= FINISH GRADE
○	= EXISTING MANHOLE	G	= GUTTER
□	= EXISTING CATCH BASIN	RT	= RIGHT
□	= PROPOSED CATCH BASIN	LT	= LEFT
○	= EXISTING FIRE HYDRANT		
☀	= EXISTING STREET LIGHT		

WORK HOURS/TRAFFIC CONTROL NOTES

- WORK HOURS ARE FROM 8:00AM TO 6:00PM MONDAY THROUGH FRIDAY.
- STREETS SHALL HAVE ONE LANE OPEN TO TRAFFIC AT ALL TIMES. LANE CLOSURE MAY ONLY OCCUR BETWEEN THE HOURS OF 8:30AM TO 6:00PM. ALL LANES OF TRAFFIC SHALL BE REOPENED AT THE END OF EACH WORKDAY.
- TRENCHES SHALL BE BACKFILLED TO THE SURFACE AT THE END OF EACH WORK DAY PRIOR TO REOPENING THE STREET. COST IS INCIDENTAL. STEEL PLATES MAY BE USED WITH APPROPRIATE SIGNAGE IF APPROVED BY THE INSPECTOR.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EMERGENCY VEHICLE ACCESS TO ALL PROPERTIES AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS FOR MAIL, TRASH (FRIDAY) AND SCHOOL BUS SERVICES AT ALL TIMES.
- THE CONTRACTOR SHALL COORDINATE WITH NEIGHBORS AT LEAST 48 HOURS IN ADVANCE OF DOING WORK THAT WILL AFFECT THEIR DRIVEWAY ACCESS.
- TRAFFIC CONTROL SHALL INCLUDE ALL PAVEMENT MARKING REMOVAL, TEMPORARY PAVEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAVEMENT MARKINGS AND ALL INCIDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".

GENERAL NOTES

2023 CATCH BASIN REPLACEMENT

LOCATED IN SECTION 31A, 31C, & 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22860 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
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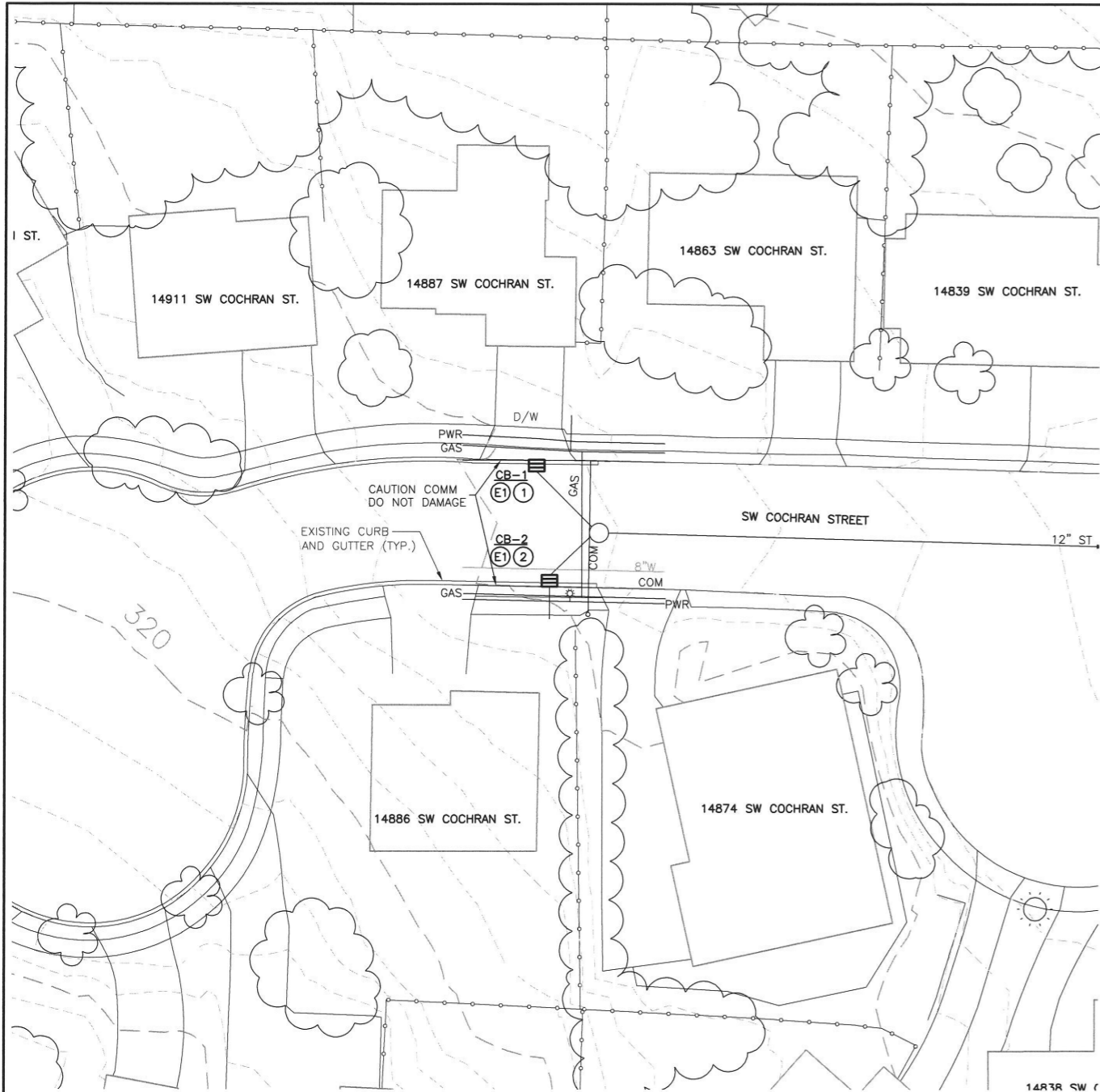


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DATE:	SEPTEMBER, 2023
2023 CB REPLACEMENT_SHEETS.DWG	

REVISIONS	

JOB NO.	
SHEET NO.	2
	7



STORM KEY NOTES

CB-1
 REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±4.3' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.

CB-2
 REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 ±4.4' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B. RECONNECT EX. WEEP HOLE PIPE TO NEW C.B.

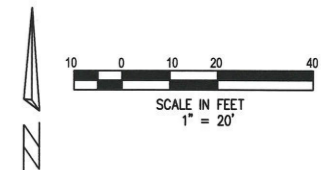
PLAN VIEW KEY NOTES

- ① REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. DRIVEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ② REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ③ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ④ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 5" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).

BID SCHEDULE NOTES

1. "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE" SHALL INCLUDE SIGNAGE FOR PEDESTRIAN TRAFFIC CONTROL ALONG THE SIDEWALK.
2. "REMOVAL OF STRUCTURES AND OBSTRUCTIONS" INCLUDES ALL REMOVAL/REPLACEMENT OF ALL ITEMS NECESSARY WHICH ARE NOT COVERED UNDER OTHER BID ITEMS. SAW CUTTING IS COVERED UNDER THIS BID ITEM.
3. "REMOVAL OF INLETS" INCLUDES ALL WORK TO REMOVE AND DISPOSE OF THE EXISTING INLET AND ANY OTHER MATERIAL NECESSARY FOR THE REMOVAL OF THE INLET WHICH ARE NOT COVERED UNDER OTHER BID ITEMS.
4. "CATCH BASINS, TYPE CG-2" SHALL INCLUDE EXCAVATION NECESSARY TO INSTALL, ALL WORK AND MATERIALS TO CONNECT THE CATCH BASIN TO EXISTING PIPES, ALL GRANULAR BACKFILLING AROUND THE NEW CATCH BASIN AND ALL CONCRETE CURB/CURB AND GUTTER WORK TO MATCH EXISTING. CONTRACTOR TO ENSURE THAT GUTTER FLOWS TO NEW CATCH BASIN WITHOUT PONDING.
5. "YARD RESTORATION" INCLUDES ALL WORK TO RESTORE THE SURROUNDING AREA TO EQUAL OR BETTER CONDITION THEN WHAT EXISTED PRIOR TO CONSTRUCTION. THIS ITEM INCLUDES THE SALVAGING AND REPLANTING OF EXISTING SHRUBS.

PROTECTION OF EXISTING FACILITIES TO REMAIN (CONCRETE, ASPHALT, UTILITIES, ETC.) IS INCIDENTAL.



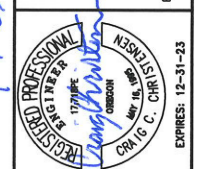
EROSION CONTROL KEY NOTES

- ① INSTALL SILT SACK IN CATCH BASIN.
- ② INSTALL SILT SACK IN CATCH BASIN WITH WATTLES IN FRONT OF CURB OPENING.

PLAN VIEW
 (COCHRAN STREET)

2023 CATCH BASIN REPLACEMENT
 LOCATED IN SECTION 31A, 32A & 32C, T2S, R1W, W.M. OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 22560 SW PINE STREET
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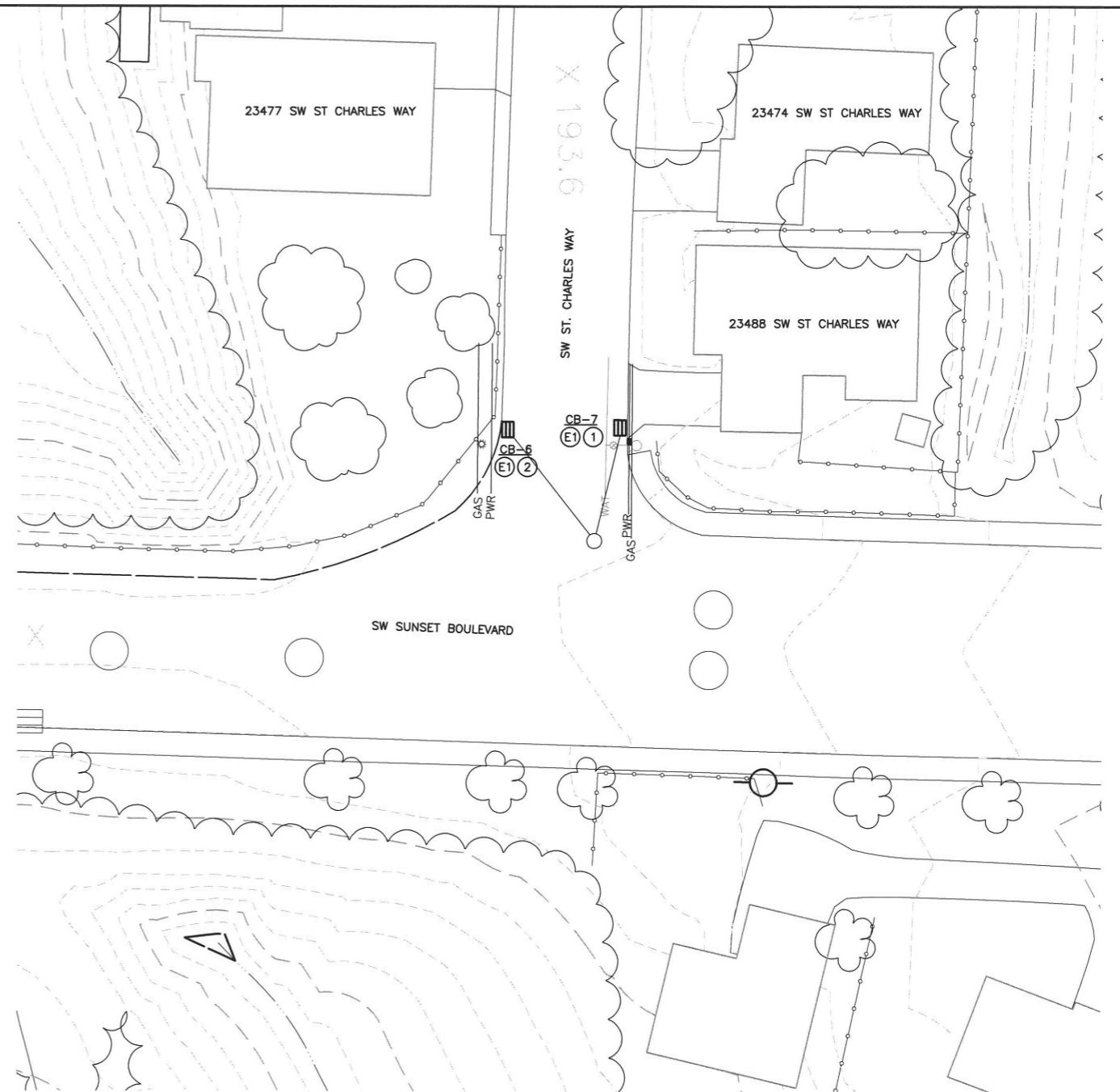
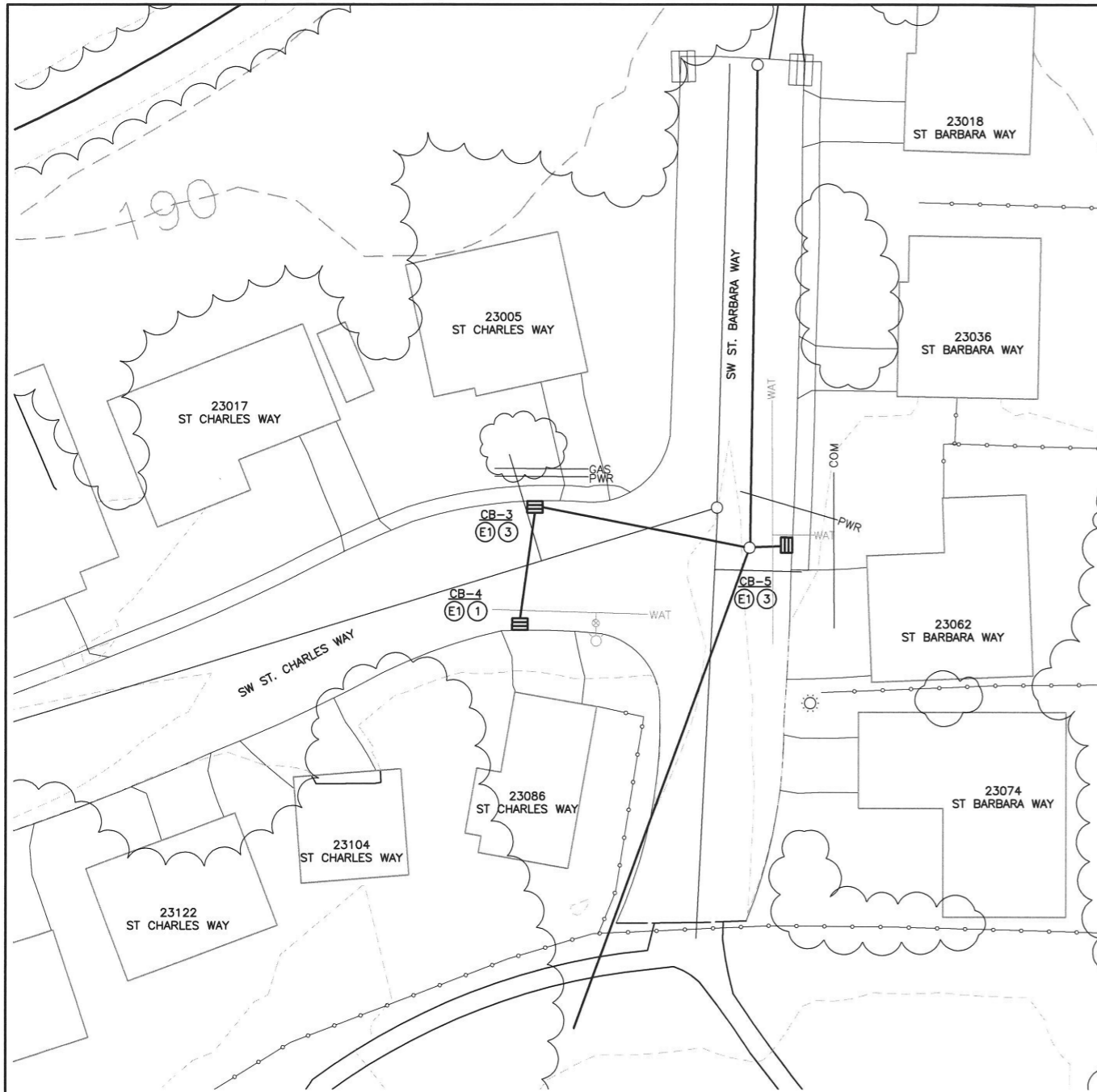


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CHECKED BY:	RS/AS
FULL SIZE SCALE:	1"=20'
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2023 CB REPLACEMENT SHEETS	

REVISIONS	

JOB NO.	
SHEET NO.	
SHEET	3
OF	7

9-7-23

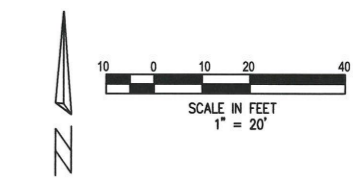


STORM KEY NOTES

- CB-3**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±2.6' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.
- CB-4**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±2.2' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.
- CB-5**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±4.0' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.
- CB-6**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±2.6' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.
- CB-7**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2 (NO CURB OPENING) ±2.1' DEEP TO PIPE I.E. (VERIFY PIPE SIZE) RECONNECT EX. PIPE TO NEW C.B.

PLAN VIEW KEY NOTES

- ① REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. DRIVEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ② REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ③ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ④ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 5" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).



EROSION CONTROL KEY NOTES

- ⓔ1 INSTALL SILT SACK IN CATCH BASIN.
- ⓔ2 INSTALL SILT SACK IN CATCH BASIN WITH WATTLES IN FRONT OF CURB OPENING.

**PLAN VIEW
(ST. CHARLES WAY)**

2023 CATCH BASIN REPLACEMENT

LOCATED IN SECTION 31A, 32A, & 32C, T2S, R1W,
W.M. IN THE CITY OF SHERWOOD,
WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

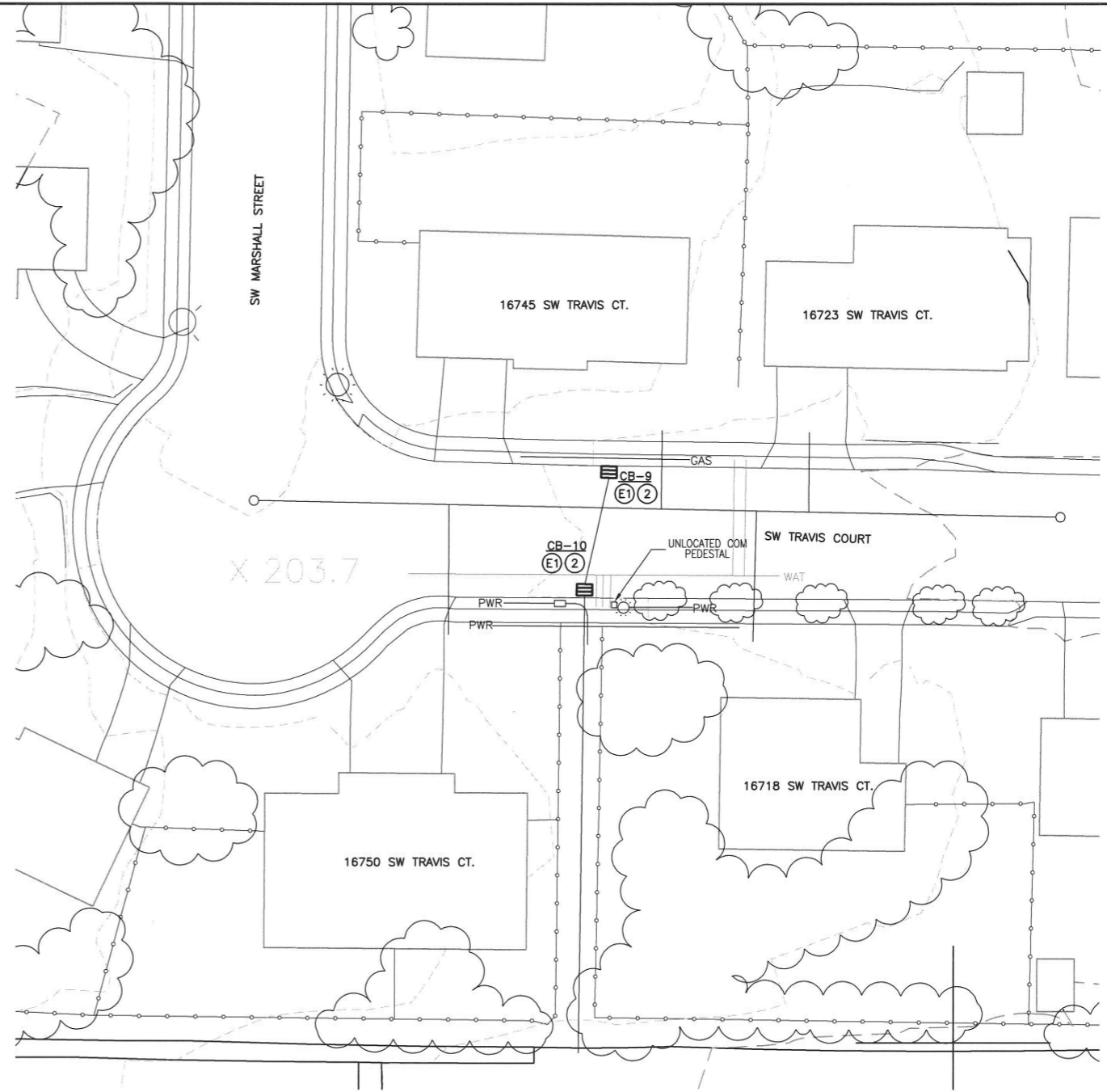
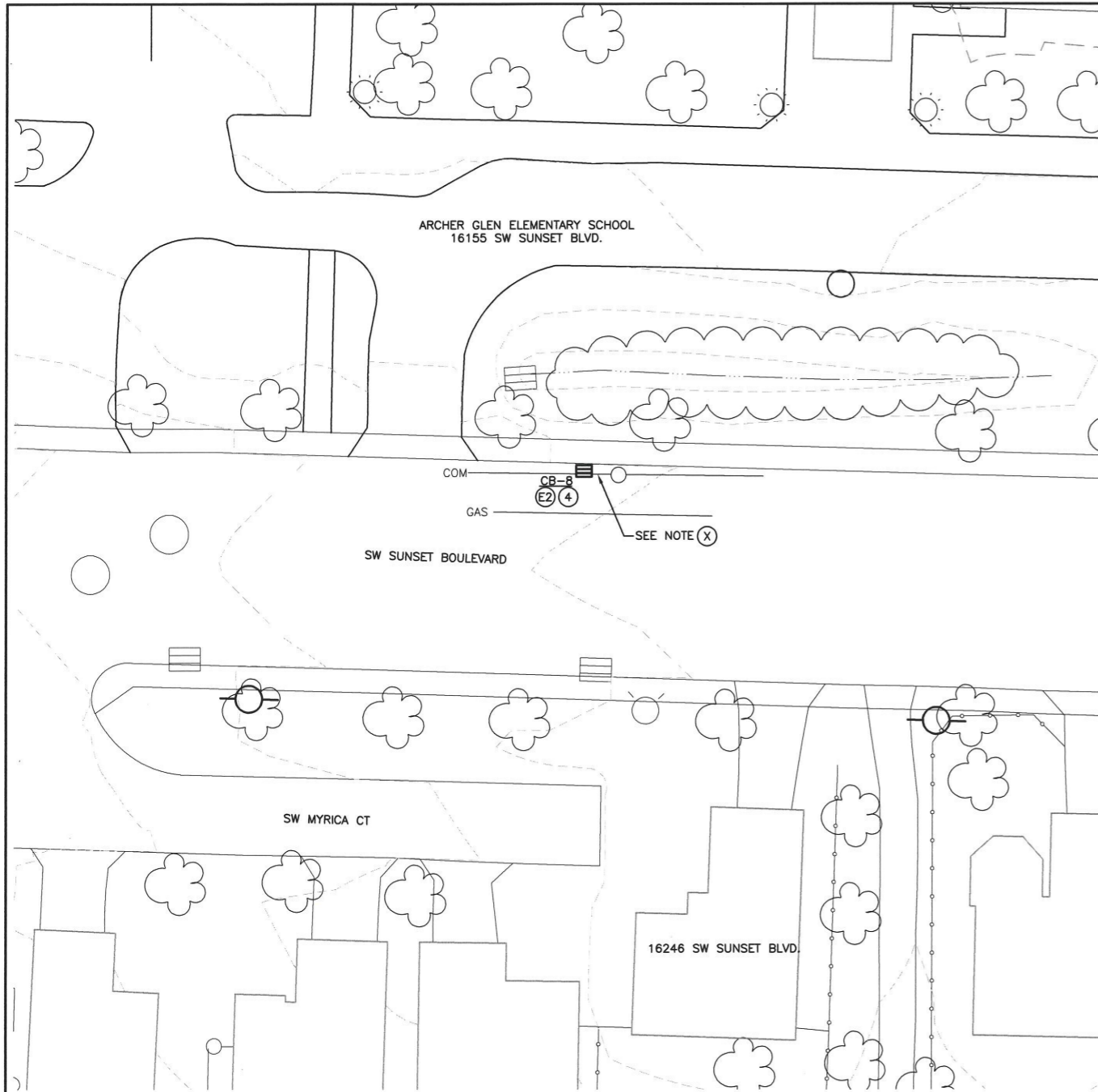
9-7-23

PROFESSIONAL ENGINEER
MAY 15, 2018
CRAIG C. CHRISTENSEN
EXPIRES: 12-31-23

DESIGNED BY:	CCC	DRAWN BY:	CCC
CHECKED BY:	RS/AS	FULL SIZE SCALE:	1"=20'
DATE:	SEPTEMBER, 2023	2023 CB REPLACEMENT_SHEETS	

REVISIONS

JOB NO.	
SHEET NO.	4
	of 7



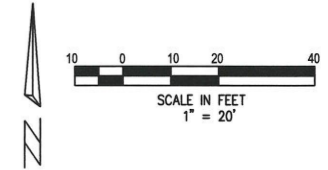
STORM KEY NOTES

- CB-8**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2
±1.2' DEEP TO PIPE I.E. (VERIFY PIPE SIZE)
RECONNECT EX. PIPE TO NEW C.B.
- CB-9**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2
±1.8' DEEP TO PIPE I.E. (VERIFY PIPE SIZE)
RECONNECT EX. PIPE TO NEW C.B.
- CB-10**
REMOVE EXISTING INLET AND REPLACE WITH NEW CATCH BASIN, TYPE CG-2
±2.5' DEEP TO PIPE I.E.S (VERIFY PIPE SIZE)
RECONNECT EX. PIPES (2) TO NEW C.B.

PLAN VIEW KEY NOTES

- ① REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. DRIVEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ② REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ③ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 4" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).
- ④ REMOVE EXISTING CURB AND ASPHALT AS NECESSARY TO REMOVE THE EXISTING INLET AND FOR INSTALLATION OF THE NEW CATCH BASIN. SIDEWAY TO REMAIN (PROTECT). RESTORE CURB IN KIND AND INSTALL 5" THICKNESS OF ACP (CONFIRM LIMITS WITH INSPECTOR PRIOR TO REMOVAL).

(X) CONTRACTOR TO VERIFY LOCATION OF EXISTING COMMUNICATION LINE AND CONTACT ENGINEER TO DISCUSS OPTIONS PRIOR TO ORDERING OF MATERIALS. ALTERNATE CATCH BASIN SELECTION MAY BE REQUIRED. INCLUDED IN COST OF CATCH BASINS, TYPE CG-2.



EROSION CONTROL KEY NOTES

- Ⓔ1 INSTALL SILT SACK IN CATCH BASIN.
- Ⓔ2 INSTALL SILT SACK IN CATCH BASIN WITH WATTLES IN FRONT OF CURB OPENING.

PLAN VIEW
(SUNSET BLVD. & TRAVIS CT.)

2023 CATCH BASIN REPLACEMENT

LOCATED IN SECTION 31A, 32A & 32C, T2S, R1W,
W.M. IN THE CITY OF SHERWOOD,
WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

REGISTERED PROFESSIONAL ENGINEER
No. 17706
CRA I.C. C. CHRIS
EXPIRES: 12-31-23

DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS/AS
FULL SIZE SCALE:	1"=20'
DATE:	SEPTEMBER, 2023
2023_CB_REPLACEMENT_SHEETS	

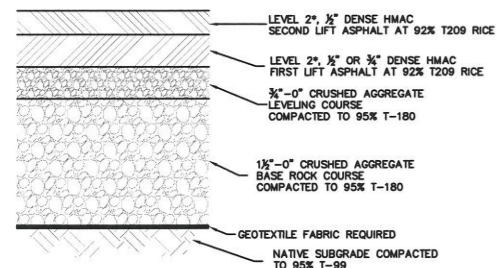
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SHEET NO. **5**

OF **7**

REVISIONS

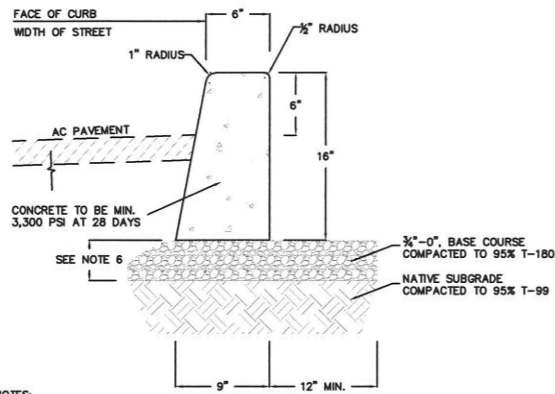
9-7-23



FUNCTIONAL CLASSIFICATION	SECOND LIFT HMAAC THICKNESS	FIRST LIFT HMAAC THICKNESS	LEVELING COURSE THICKNESS	BASE ROCK COURSE THICKNESS
LOCAL	2"	2"	2"	8"
NEIGHBORHOOD	2"	2"	2"	9"
COMMERCIAL	2"	3"	3"	9"
COLLECTOR	2"	3"	3"	9"
ARTERIAL	2"	3"	4"	10"

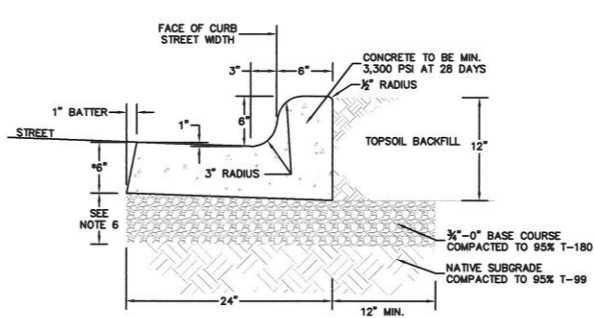
- NOTES:
- MATERIALS AND PLACEMENT OF THE HOT MIXED ASPHALT CONCRETE PAVEMENT (ACP) SHALL CONFORM TO THE REQUIREMENTS DELINEATED IN SECTION 00744 - ASPHALT CONCRETE PAVEMENT (ACP), OF THE ODOT/APWA, OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION), EXCEPT AS MODIFIED BY CITY AND NOTED IN THE CITY'S ENGINEERING DESIGN AND STANDARD DETAILS MANUAL (LATEST EDITION).
 - THE TOP LIFT OF HMAAC SHALL BE PLACED PRIOR TO CITY FINAL ACCEPTANCE OF PUBLIC INFRASTRUCTURE IMPROVEMENTS.
 - CRUSHED AGGREGATE USED FOR BASE ROCK AND LEVELING COURSE SHALL CONFORM TO THE REQUIREMENTS DELINEATED IN SECTION 02630 - BASE AGGREGATE, OF THE ODOT/APWA, OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION).
- * FOR ARTERIAL CLASSIFICATION USE LEVEL 3.

STANDARD DRAWING TITLE	DRAWING NUMBER
PAVEMENT SECTION	RD-20
SCALE	DATE
N.T.S.	MAR '16



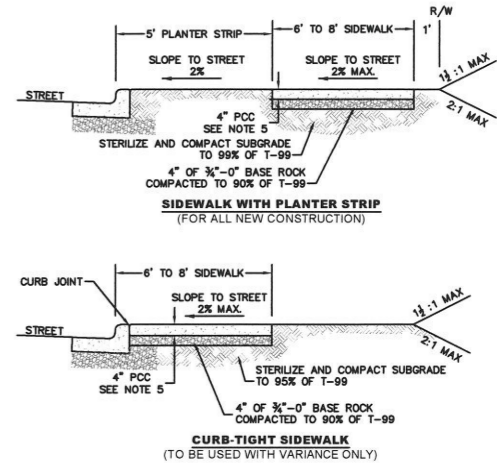
- NOTES:
- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
 - CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
 - EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
 - MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2 INCH.
 - CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1/2" IN DEPTH.
 - BASE ROCK: 3/4"-0", COMPACTED TO 95% MAX DENSITY, BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

STANDARD DRAWING TITLE	DRAWING NUMBER
VERTICAL CURB	RD-21
SCALE	DATE
N.T.S.	MAR '16



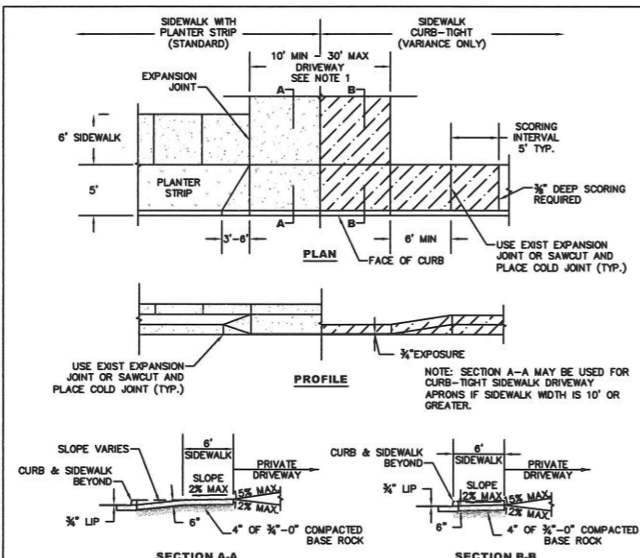
- NOTES:
- MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
 - CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PSI, WITH A 4" MAX SLUMP.
 - EXPANSION JOINTS TO BE PROVIDED AT EACH:
 - POINT OF TANGENCY.
 - COLD JOINT.
 - SIDE OF INLET STRUCTURES.
 - SIDE OF DRIVEWAYS.
 - EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2".
 - CONTRACTION JOINTS SHALL HAVE:
 - SPACING OF NOT MORE THAN 15 FEET.
 - DEPTH OF JOINT OF AT LEAST 1/2".
 - BASE ROCK SHALL BE 3/4"-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.
 - FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23.
 - COMMERCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.

STANDARD DRAWING TITLE	DRAWING NUMBER
MONOLITHIC CURB AND GUTTER	RD-22
SCALE	DATE
N.T.S.	MAR '16



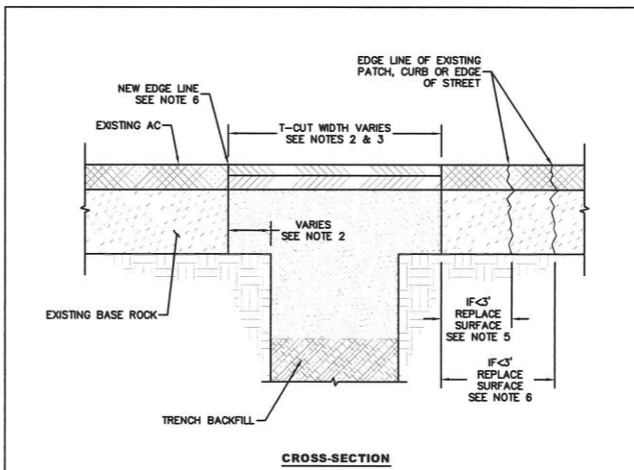
- NOTES:
- CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI @ 28 DAYS, WITH A 4" MAX SLUMP.
 - SIDEWALK PANELS TO BE SQUARE (6' LONG x 6' WIDE TYP.).
 - EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, CURB RAMP, AND/OR POINTS OF TANGENCY IN CURB AS SHOWN ON THE STANDARD DRAWINGS FOR SIDEWALK RAMP, AND AT SPACING NOT TO EXCEED 45'.
 - FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS.
 - SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4"; IF MOUNTABLE CURB IS USED, OR IF SIDEWALK IS INTENDED AS PORTION OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6" MINIMUM THICKNESS, COMMERCIAL 8".
 - CONCRETE SHALL HAVE A BROOM FINISH. ALL JOINTS SHALL BE EDGED WITH 3" SHINE.
 - WIDTH OF PLANTER STRIP AND SIDEWALK IS MEASURED FROM FACE OF CURB.
 - IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 1" PAST BACK OF SIDEWALK WITH A 3" DIAMETER ADS PIPE. CONTRACTION JOINT SHALL BE PLACED OVER PIPE.

STANDARD DRAWING TITLE	DRAWING NUMBER
SIDEWALK DETAIL	RD-26
SCALE	REVISION DATE
N.T.S.	2023.03.17



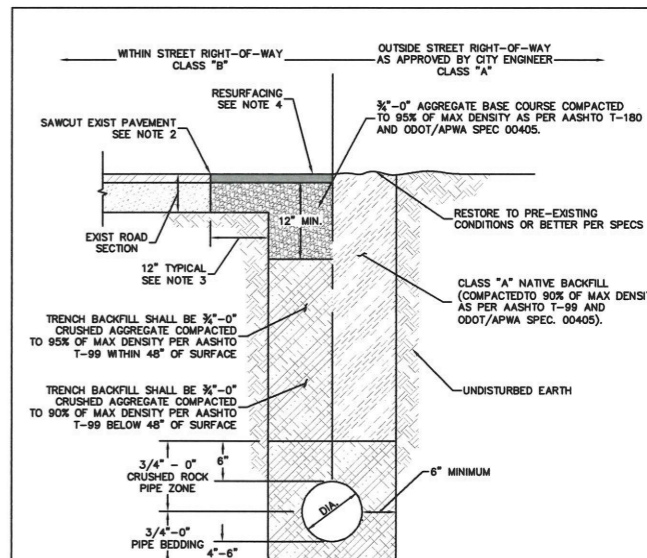
- NOTES:
- DRIVEWAY WIDTHS:
 - LOT FRONTAGE UP TO 60': MAX DRIVEWAY WIDTH - 24'
 - LOT FRONTAGE OF GREATER THAN 60': MAX DRIVEWAY WIDTH - 30'
 - DRIVEWAY CONCRETE SHALL HAVE A MINIMUM THICKNESS OF 6".
 - 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
 - CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3300 PSI @ 28 DAYS.
 - CURB JOINT SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS ALONG BACK OF CURB.
 - EXPANSION JOINTS SHALL BE 1/2" PRE-MOLDED ASPHALT IMPREGNATED MATERIAL, CEDAR OR EQUIVALENT EXTENDING FROM TOP OF BASE TO FINISHED GRADE.
 - FOR DRIVEWAYS GREATER THAN 24', CONCRETE SHALL BE INCREASED TO A 7" DEPTH.
 - CONCRETE SHALL HAVE A BROOM FINISH AND EDGE ALL JOINTS.
 - WEEDHOLES ARE NOT TO BE PLACED IN WINGS.
 - IF CURBING IS BEING REMOVED TO INSTALL DRIVEWAY AND GUTTER SHOULD BECOME SEPARATED FROM THE DRIVING SURFACE IN EXCESS OF 1/2", THEN THE GUTTER SHALL ALSO BE REMOVED AND REPLACED.
 - SLOPE OF THE DRIVEWAY MAY BE AWAY FROM THE CURB WHEN PRE-APPROVED BY THE CITY ENGINEER.

STANDARD DRAWING TITLE	DRAWING NUMBER
RESIDENTIAL DRIVEWAY DETAIL	RD-41
SCALE	REVISION DATE
N.T.S.	2023.03.17



- NOTES:
- THIS DRAWING APPLIES TO TRENCH CUTS AND OTHER KINDS OF STREET CUTS.
 - SEE STD DET RD-8 FOR TYPICAL STREET PAVEMENT SECTION.
 - T-CUT ASPHALT 6" FOR TRENCHES 12" WIDE OR LESS, ELSE T-CUT IS 12" MINIMUM FOR TRENCHES WIDER THAN 12".
 - ON ALL NON-LOCAL STREETS, WIDTH OF T-CUT SHALL BE A MINIMUM OF 3'. ON ALL LOCAL STREETS, WIDTH OF T-CUT SHALL BE A MINIMUM OF 12" PLUS THE TRENCH WIDTH.
 - IF NEW EDGE OF PAVEMENT IS LESS THAN 3' FROM ANOTHER PATCH, CURB OR EDGE OF STREET, REPLACE THE PAVEMENT IN BETWEEN.
 - IF MORE THAN ONE EXISTING PATCH EDGE IS WITHIN THE 3' ZONE, REMOVE PAVEMENT TO THE FAR EDGE OF THE PRE-EXISTING PATCH.
 - NEW EDGE OF PAVEMENT (EDGE LINE) SHALL NOT LIE IN A WHEEL PATH. WIDTH OF T-CUT SHALL BE WIDENED WHERE NECESSARY TO MOVE THE EDGE LINE OUT OF THE WHEEL PATH AND EITHER: (A) TO LOCATION THAT IS 6" FROM THE NEAREST LINE OR (B) TO THE LOCATION REQUIRED BY NOTE 3 OR 4 ABOVE AS APPLICABLE, WHICHEVER IS NEAREST OF (A) OR (B).

STANDARD DRAWING TITLE	DRAWING NUMBER
PIPE TRENCH RESTORATION	RD-45
SCALE	DATE
N.T.S.	MAR '16



- NOTES:
- THESE TRENCH BACKFILL REQUIREMENTS APPLY TO ALL PUBLIC UTILITY PIPES. FOR ADDITIONAL REQUIREMENTS, SEE CITY STANDARD DESIGN MANUAL SECTION 210.19.
 - SAWCUT EXISTING HMAAC PAVEMENT FULL DEPTH. SAWCUT EXISTING PCC PAVEMENT ACCORDING TO CITY STANDARD DETAILS.
 - 12" FOR TRENCHES WIDER THAN 12". 6" FOR TRENCHES LESS THAN 12".
 - MATCH EXISTING PAVEMENT MATERIAL(S). THICKNESS SHALL BE AS FOLLOWS:
 - FOR EXISTING HMAAC, RESURFACE TO A MINIMUM OF 3" OF LEVEL 2, 1/2" DENSE HMAAC OR EXISTING AC THICKNESS PLUS 2", WHICHEVER IS GREATER, BUT DO NOT EXCEED 6". COMPACT AC IN 2" MAX LIFTS TO 92% OF MAXIMUM DENSITY (RICE).
 - FOR EXISTING PCC, EXISTING PAVEMENT THICKNESS PLUS 2", BUT NOT LESS THAN 6". ON ARTERIAL AND COLLECTOR STREETS, CONCRETE PATCHING MATERIAL SHALL BE HIGH EARLY STRENGTH CLASS 5000 PSI PCC APPROVED BY CITY ENGINEER.
 - ALL CUT EDGES OF AC SHALL BE SAND SEALED WITH CRS-1 OR CRS-2 EMULSIFIED ASPHALT OR EQUIVALENT.

STANDARD DRAWING TITLE	DRAWING NUMBER
PIPE TRENCH BACKFILL	RD-47
SCALE	DATE
N.T.S.	MAR '16

DETAILS

2023 CATCH BASIN REPLACEMENT

LOCATED IN SECTION 31A, 32A & 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD ENGINEERING DEPARTMENT 22560 SW PINE STREET SHERWOOD, OREGON 97140

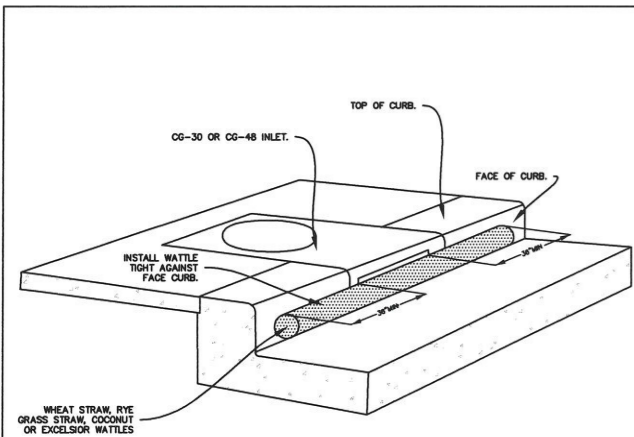
PHONE: (503) 925-2309 FAX: (503) 625-0629 E-MAIL: engineering@sherwoodoregon.gov



DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS/AS
DATE:	SEPTEMBER, 2023

REVISIONS

JOB NO.	
SHEET NO.	6
	7

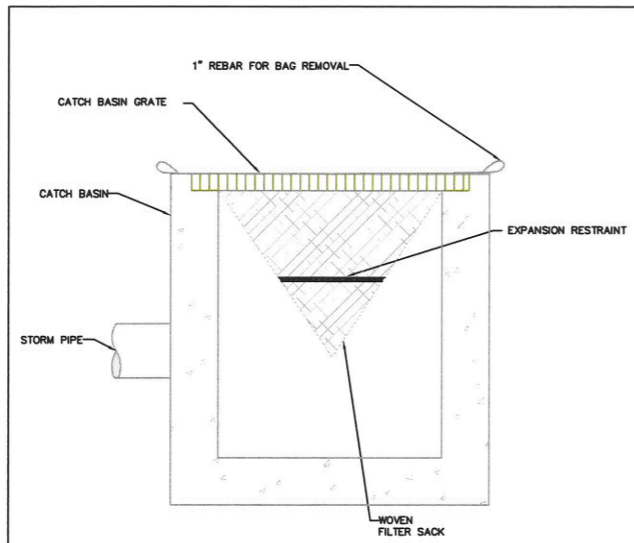


PERSPECTIVE VIEW SHOWING WATTLE ALONG GUTTER AT CURB INLET

- NOTES:
- ONLY ALLOWED USE OF APPLICATION IS ON CURB AND GUTTER INLETS.
 - INSTALL WATTLE ALONG INLET WITH WATTLE EXTENDING A MIN OF 36" BEYOND INLET OPENINGS IN EACH DIRECTION.
 - WATTLE MUST BE INSTALLED TIGHTLY AGAINST CURB. MAY REQUIRE ADDITIONAL MEASURES TO ENSURE WATTLE REMAINS TIGHT AGAINST CURB, SUCH AS USING ZIP TIES TO SECURE WATTLE TO INLET'S TRASH BARS OR USING SANDBAGS TO WEIGHT DOWN WATTLE.
 - REPLACE WATTLE AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING THE STORM SYSTEM.

CURB AND GUTTER INLET PROTECTION

DRAWING NO. 905 REVISED 10-31-19



CATCH BASIN INSERT

- NOTE:
- RECESSED CURB INLET CATCH BASINS MUST BE BLOCKED WHEN USING FILTER FABRIC INLET SACKS. SIZE OF FILTER FABRIC INLET SACKS TO BE DETERMINED BY MANUFACTURER.
- FOR FURTHER INFORMATION ON DESIGN CRITERIA SEE CHAPTER 4 OF CLEAN WATER SERVICES EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL.

INLET PROTECTION TYPE 5

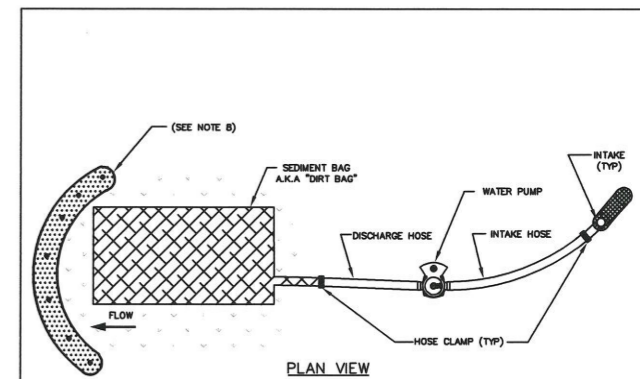
DRAWING NO. 920 REVISED 10-31-19



- NOTES:
- WHEN RAINFALL AND RUNOFF OCCURS, A KNOWLEDGEABLE AND EXPERIENCED PERSON IN THE PRINCIPLES, PRACTICES, INSTALLATION, AND MAINTENANCE OF EROSION AND SEDIMENT CONTROLS WHO WORKS FOR THE PERMITTEE MUST PROVIDE DAILY INSPECTIONS OF THE EROSION AND SEDIMENT CONTROLS AND DISCHARGE OUTFALLS.
 - CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND FROM OCTOBER 1 THROUGH MAY 31ST EACH YEAR.
 - DURING WET WEATHER PERIOD, TEMPORARY STABILIZATION OF THE SITE MUST OCCUR AT THE END OF EACH WORK DAY.
 - SEDIMENT CONTROLS MUST BE INSTALLED AND MAINTAINED ON ALL DOWN GRADIENT SIDES OF THE CONSTRUCTION SITE AT ALL TIMES DURING CONSTRUCTION. THEY MUST REMAIN IN PLACE UNTIL PERMANENT VEGETATION OR OTHER PERMANENT COVERING OF EXPOSED SOIL IS ESTABLISHED.
 - ALL ACTIVE INLETS MUST HAVE SEDIMENT CONTROLS INSTALLED AND MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
 - SIGNIFICANT AMOUNTS OF SEDIMENT THAT LEAVES THE SITE MUST BE CLEANED UP WITHIN 24 HOURS AND PLACED BACK ON THE SITE AND STABILIZED OR PROPERLY DISPOSED. THE CAUSE OF THE SEDIMENT RELEASE MUST BE FOUND AND PREVENTED FROM CAUSING A REOCCURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DEPARTMENT OF STATE LANDS REQUIRED TIME FRAME.
 - SEDIMENT MUST NOT BE INTENTIONALLY WASHED INTO STORM SEWERS, DRAINAGEWAYS, OR WATER BODIES.
 - SEDIMENT MUST BE REMOVED FROM BEHIND ALL SEDIMENT CONTROL MEASURES WHEN IT HAS REACHED A HEIGHT OF 1/3-RD THE BARRIER HEIGHT AND PRIOR TO THE CONTROL MEASURES REMOVAL.
 - CLEANING OF ALL STRUCTURES WITH SUMPS MUST OCCUR WHEN THE SEDIMENT RETENTION CAPACITY HAS BEEN REDUCED BY 50% AND AT COMPLETION OF PROJECT.
 - ANY USE OF TOXIC OR OTHER HAZARDOUS MATERIALS MUST INCLUDE PROPER STORAGE, APPLICATION, AND DISPOSAL.
 - THE PERMITTEE MUST PROPERLY MANAGE HAZARDOUS WASTES, USED OILS, CONTAMINATED SOILS, CONCRETE WASTE, SANITARY WASTE, LIQUID WASTE, OR OTHER TOXIC SUBSTANCES DISCOVERED OR GENERATED DURING CONSTRUCTION.
 - THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS. NUTRIENT RELEASES FROM FERTILIZERS TO SURFACE WATERS MUST BE MINIMIZED. TIME RELEASE FERTILIZERS SHOULD BE USED AND CARE SHOULD BE MADE IN APPLICATION OF FERTILIZERS WITHIN ANY WATERWAY RIPARIAN ZONE.
 - OWNER OR DESIGNATED PERSON SHALL BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES, IN ACCORDANCE WITH CURRENT CLEAN WATER SERVICES STANDARDS AND STATE, AND FEDERAL REGULATIONS.
 - PRIOR TO ANY LAND DISTURBING ACTIVITIES, THE BOUNDARIES OF THE CLEARING LIMITS, VEGETATED BUFFERS, AND ANY SENSITIVE AREAS SHOWN ON THIS PLAN SHALL BE CLEARLY DELINEATED IN THE FIELD. UNLESS OTHERWISE APPROVED, NO DISTURBANCE IS PERMITTED BEYOND THE CLEARING LIMITS. THE OWNER/PERMITTEE MUST MAINTAIN THE DELINEATION FOR THE DURATION OF THE PROJECT. NOTE: VEGETATED CORRIDORS TO BE DELINEATED WITH ORANGE CONSTRUCTION FENCE OR APPROVED EQUIV.
 - PRIOR TO ANY LAND DISTURBING ACTIVITIES, THE BMPs THAT MUST BE INSTALLED ARE GRAVEL CONSTRUCTION ENTRANCE, PERIMETER SEDIMENT CONTROL, AND INLET PROTECTION. THESE BMPs MUST BE MAINTAINED FOR THE DURATION OF THE PROJECT.
 - IF VEGETATIVE SEED MIXES ARE SPECIFIED, SEEDING MUST TAKE PLACE NO LATER THAN SEPTEMBER 1ST; THE TYPE AND PERCENTAGES OF SEED IN THE MIX ARE AS IDENTIFIED ON THE PLANS OR AS SPECIFIED BY THE DESIGN ENGINEER.
 - WATERTIGHT TRUCKS MUST BE USED TO TRANSPORT SATURATED SOILS FROM THE CONSTRUCTION SITE. AN APPROVED EQUIVALENT IS TO DRAIN THE SOIL ON LOC AT A DESIGNATED LOCATION USING APPROPRIATE BMPs. SOIL MUST BE DRAINED SUFFICIENTLY FOR MINIMAL SPILLAGE.
 - ALL PUMPING OF SEDIMENT LADEN WATER MUST BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (I.E. FILTER BAG).
 - THE ESC PLAN MUST BE KEPT ON SITE. ALL MEASURES SHOWN ON THE PLAN MUST BE INSTALLED PROPERLY TO ENSURE THAT SEDIMENT LADEN WATER DOES NOT ENTER A SURFACE WATER SYSTEM, ROADWAY, OR OTHER PROPERTIES.
 - IN AREAS SUBJECT TO WIND EROSION, APPROPRIATE BMPs MUST BE USED, WHICH MAY INCLUDE THE APPLICATION OF FINE WATER SPRAYING, PLASTIC SHEETING, MULCHING, OR OTHER APPROVED MEASURES.
 - ALL EXPOSED SOILS MUST BE COVERED, AT END OF BUSINESS DAY, DURING WET WEATHER PERIOD, FROM OCTOBER 1 - MAY 31.

STANDARD EROSION CONTROL NOTES FOR SITES LESS THAN 1 ACRE

DRAWING NO. 945 REVISED 10-31-19



- NOTES:
- THE SEDIMENT BAG SHALL BE MANUFACTURED USING A POLYPROPYLENE 8 OZ. NON-WOVEN GEOTEXTILE SEWN INTO A BAG WITH A DOUBLE NEEDLE, USING A HIGH STRENGTH THREAD.
 - EACH STANDARD SEDIMENT BAG MUST HAVE A FULL SPOUT LARGE ENOUGH TO ACCOMMODATE A 4" DISCHARGE HOSE. STRAPS ARE ATTACHED TO SECURE THE HOSE AND PREVENT PUMPED WATER FROM ESCAPING WITHOUT BEING FILTERED.
 - THE SEDIMENT BAG SHALL MEET OR EXCEED OVERALL BAG REMOVAL EFFICIENCY RATE OF 97.55%.
 - WATER BEING DISCHARGED FROM THE SEDIMENT BAG MUST BE FREE OF ALL SEDIMENT PRIOR TO LEAVING THE SITE OR ENTERING INTO THE STORM SYSTEM.
 - SEDIMENT BAG IS FULL WHEN IT NO LONGER CAN EFFICIENTLY FILTER SEDIMENT OR ALLOW WATER TO PASS AT A RATE LESS THAN 50% OF MANUFACTURER'S DESIGNED FLOW RATE.
 - DURING USE, THE SEDIMENT BAG MUST BE MONITORED.
 - DISPOSE OF USED SEDIMENT BAG OFF SITE OR AS APPROVED BY CWS.
 - WHEN APPROPRIATE, INSTALL DOWNSTREAM SEDIMENT CONTROL MEASURES PER CWS STANDARDS.
 - FOR BEST RESULTS, PLACE SEDIMENT BAG ON FLAT SURFACE.
 - SEDIMENT BAG SHOULD BE PLACED ON EXISTING VEGETATION, ROCK, OR BED OF STRAW. SEDIMENT BAG SHOULD NOT BE PLACED ON BARE GROUND.

SEDIMENT BAG

DRAWING NO. 950 REVISED 10-31-19

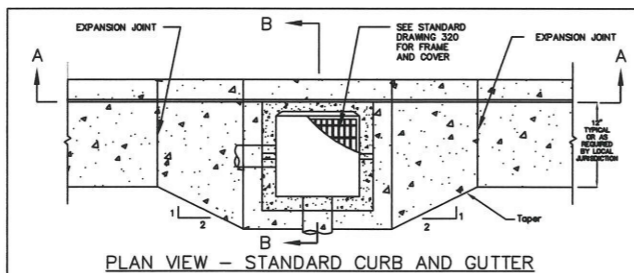


DETAILS

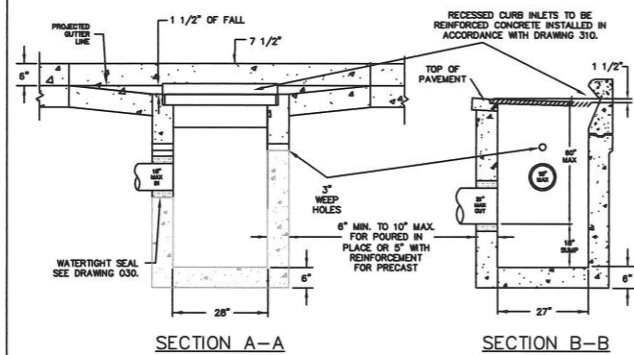
2023 CATCH BASIN REPLACEMENT

LOCATED IN SECTION 31A, 32A, & 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0829
E-MAIL: engineering@sherwoodoregon.gov



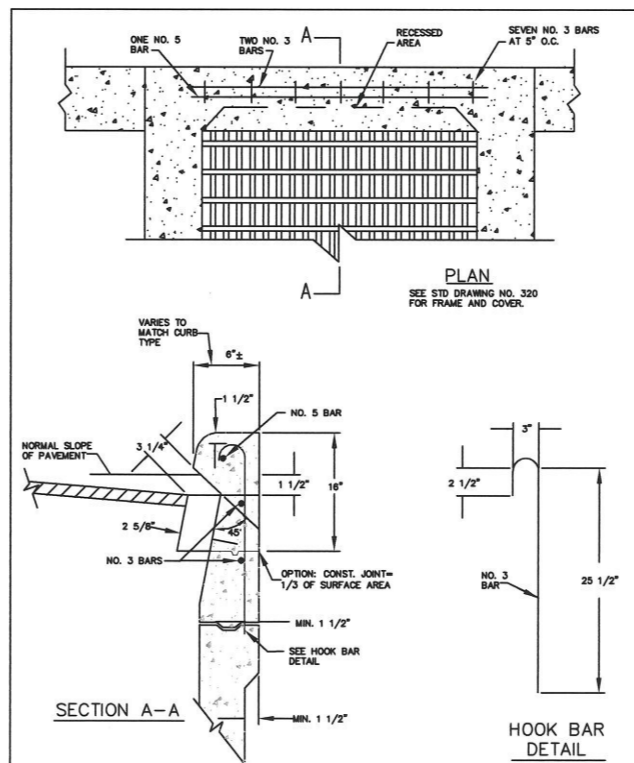
PLAN VIEW - STANDARD CURB AND GUTTER



- NOTES:
- CATCH BASIN SHALL BE CONSTRUCTED IN ACCORDANCE WITH ASTM C-478.
 - INSTALL STRUCTURE ON MINIMUM OF 8" OF 3/4"-0" COMPACTED BASE MATERIAL.
 - PRECAST STRUCTURE TO BE APPROVED BY DISTRICT OR CITY. REINFORCEMENT FOR PRECAST CATCH BASIN SHALL BE REBAR MEETING ASTM A-615 GRADE 60 OR WELDED WIRE MEETING ASTM A-497.
 - ALL POURED IN-PLACE CONCRETE SHALL HAVE A 28 DAY STRENGTH OF 3000 PSI AND A SLUMP OF 2" TO 4".
 - CHANNELS REQUIRED IN FLOW THROUGH APPLICATIONS, AS APPROVED. ALL OTHER APPLICATIONS REQUIRE AN 18" SLUMP BELOW LOWEST PIPE INVERT.
 - FULL CURB EXPOSURE REQUIRED CANNOT BE LOCATED IN SIDEWALK RAMPS OR RAMP WINGS.

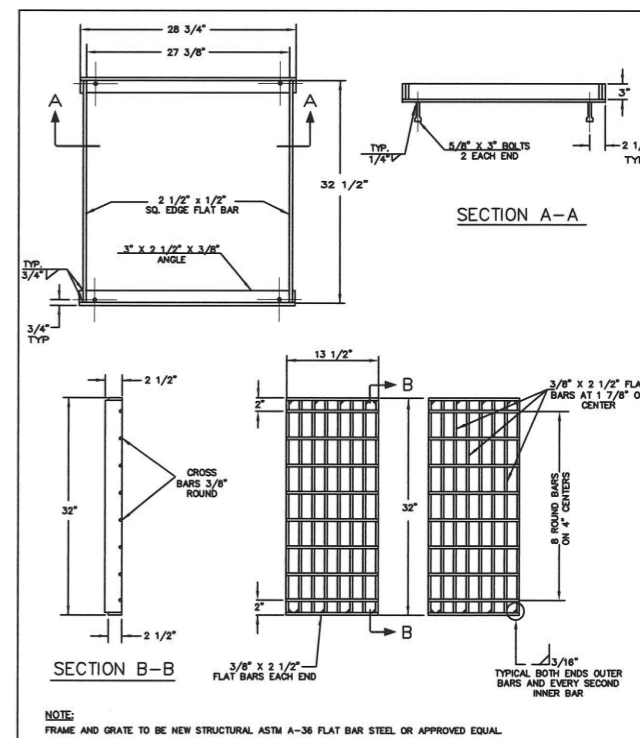
GUTTER & CURB INLET CATCH BASIN (CG-2)

DRAWING NO. 300 REVISED 10-31-19



GUTTER & CURB INLET CATCH BASIN (CG-2) REINFORCEMENT

DRAWING NO. 310 REVISED 10-31-19



CATCH BASIN FRAME AND GRATE (CG-2)

DRAWING NO. 320 REVISED 10-31-19



DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS/AS
FULL SIZE SCALE:	N/A
DATE:	SEPTEMBER, 2023
2023 CB REPLACEMENT SHEETS	

REVISIONS	
JOB NO.	
SHEET NO.	7
OF 7	