



Langer Drive Grind and Inlay

DATE April 6, 2022

ADDENDUM NO. 1

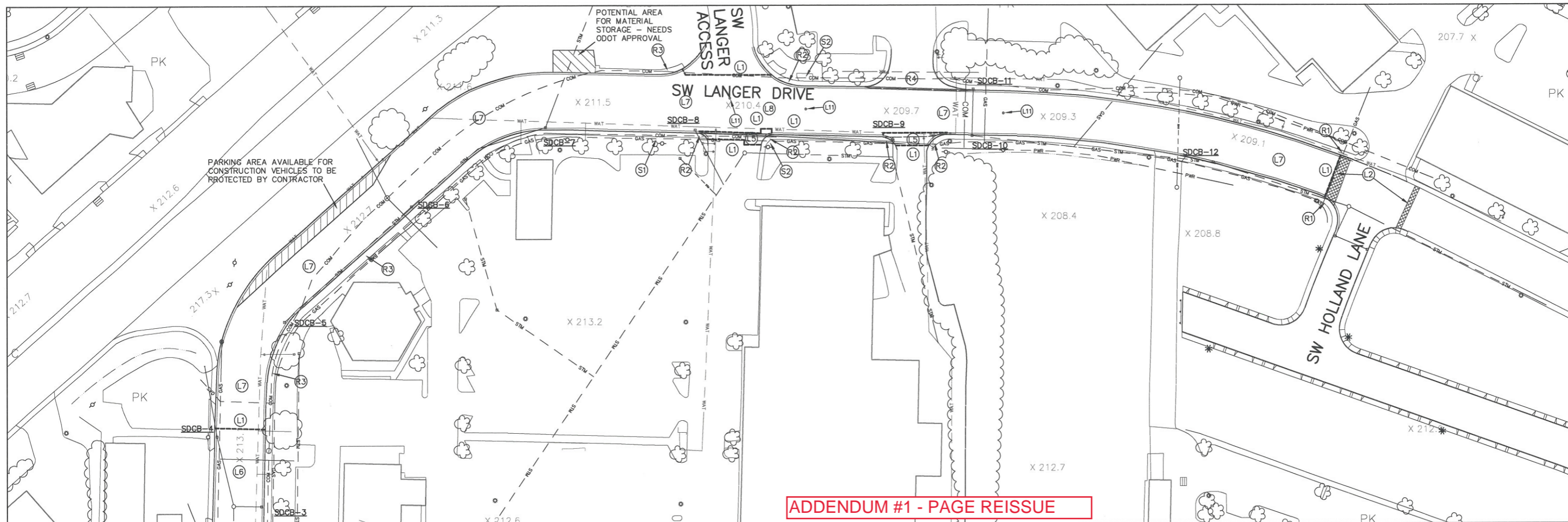
Addition/Change to the Contract Documents

The work provided for in this addendum shall become a part of the drawings and specifications for this project.

1. Note that key note L6 on Sheet 5 of Contract Drawings designates that section of street grinding/paving work to be done via night-time work with street closure.
2. Note that key note L7 on Sheet 5 of Contract Drawings designates that section of street grinding/paving work to be done via day-time work while keeping 2 lanes open to traffic. Text under key note L7 referring to night-time closure is removed.

This ADDENDUM shall be signed and attached to the Bidder's Proposal and shall subsequently become part of the Contract Documents.

Company Name	
Contractor Name	
Contractor Signature	
Date	



ADDENDUM #1 - PAGE REISSUE

STREET KEY NOTES

- (L1) SAWCUT FOR GRIND AND INLAY (PARTIAL DEPTH). CONFIRM LOCATION WITH INSPECTOR. (INCIDENTAL)
- (L2) EXISTING STAMPED CONCRETE CROSSWALK - DO NOT DAMAGE - CLEAN OFF TACK COAT AFTER PAVING
- (L3) SAWCUT TO STOP ON PROJECT SIDE OF SIDEWALK CROSSING.
- (L4) SAWCUT FOR FULL DEPTH ASPHALT REMOVAL.
- (L5) REMOVE FULL DEPTH OF ASPHALT AND BASE ROCK TO ALLOW FOR PLACEMENT OF A NEW 3-1/2" ASPHALT BASE LIFT (2 LIFTS) PRIOR TO 1-1/2" OVERLAY. (HATCHED AREA)
- (L6) GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. WORK DONE VIA NIGHT TIME CLOSURE.
- (L7) GRIND EXISTING ASPHALT (1-1/2" THICKNESS) AND INSTALL ASPHALT OVERLAY. MAINTAIN EXISTING PAVEMENT SLOPES AND MATCH FRONT EDGE OF CONCRETE GUTTER. ~~WORK DONE VIA NIGHT TIME CLOSURE.~~ WORK DONE DURING DAYTIME WITH 2 LANES REMAINING OPEN FOR TRAFFIC AND FLAGGERS AS NEEDED.
- (L8) EXISTING CONCRETE CROSSWALK TO BE REMOVED (UNKNOWN THICKNESS) - INSTALL AGGREGATE BASE UP TO 5 INCHES BELOW ASPHALT SURFACE AND COMPACT - INSTALL 3-1/2" OF ASPHALT BASE LIFT PRIOR TO 1-1/2" OVERLAY.
- (L9) INSTALL TRAFFIC LOOPS AT ORIGINAL LOCATIONS.
- (L10) RAISED CONCRETE MEDIAN TO REMAIN. DO NOT DAMAGE.
- (L11) PROTECT EXISTING MONUMENT.

NOTE NIGHT TIME CLOSURE - L6

DAYTIME WORK - L7

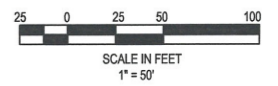
SIDEWALK RAMP KEY NOTES

- (R1) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE.
- (R2) CONTRACTOR TO CONTACT ENGINEER TO MEET ON SITE TO GO OVER CONCRETE REMOVAL LIMITS AND RAMP CONFIGURATION. CONTRACTOR TO CONTACT ENGINEER/INSPECTOR TO REVIEW FORMS BEFORE POURING CONCRETE. CONCRETE POURED WITHOUT FORM APPROVAL IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- (R3) CONTRACTOR TO CONTACT ENGINEER TO MEET ON SITE TO GO OVER CONCRETE REMOVAL LIMITS. EXISTING RAMP TO BE REMOVED AND REPLACE WITH FULL EXPOSURE CURB AND GUTTER WITH SIDEWALK MATCHING ADJOINING SIDEWALK.
- (R4) CONTRACTOR TO CONTACT ENGINEER TO MEET ON SITE TO GO OVER CONCRETE REMOVAL LIMITS AND RAMP/DRIVEWAY CONFIGURATION. CONTRACTOR TO CONTACT ENGINEER/INSPECTOR TO REVIEW FORMS BEFORE POURING CONCRETE. CONCRETE POURED WITHOUT FORM APPROVAL IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.

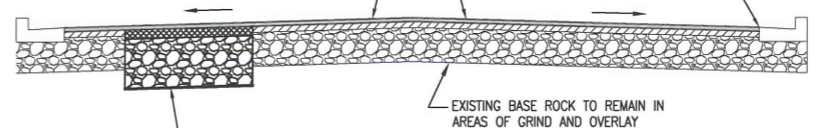
NOTE: NEW ASPHALT TO BE FLUSH WITH SIDEWALK RAMPS.

CATCH BASIN KEY NOTES

- SDCB-1 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 2.8'±
- SDCB-2 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPES IE DEPTH = 2.3'±
- SDCB-3 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 3.8'±
- SDCB-4 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPES IE DEPTH = 5.3'±
- SDCB-5 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 1.8'±
- SDCB-6 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPES IE DEPTH = 2.9'±
- SDCB-7 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPES IE DEPTH = 3.0'±
- SDCB-8 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 3.2'±
- SDCB-9 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW FIELD INLET AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 2.5'±
- SDCB-10 EXISTING CG-30 TO REMAIN DO NOT DAMAGE IE DEPTH = 4.0'±
- SDCB-11 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-30 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 4.0'±
- SDCB-12 EXISTING CG-30 TO REMAIN DO NOT DAMAGE IE DEPTH = 5.7'±
- SDCB-13 REMOVE EXISTING CATCH BASIN. REPLACE WITH NEW CG-2 CATCH BASIN AND RECONNECT TO EXISTING STORM PIPE IE DEPTH = 1.8'±



GRIND 1-1/2" THICKNESS OF EXISTING ASPHALT, CLEAN AND TACK COAT REMAINING ASPHALT SURFACE (INCIDENTAL) AND INSTALL 1-1/2" THICKNESS OF LEVEL 2, 1/2-INCH, FORTA-FI TREATED ACP OR APPROVED EQUIVALENT - COMPACT TO 92% OF MAMD PER AASHTO T209 - CONTACT ENGINEER TO LOOK AT REMAINING ASPHALT TO DETERMINE IF FULL DEPTH REPAIR IS NECESSARY



NOTE: CONTACT ENGINEER AFTER ASPHALT GRINDING TO DETERMINE IF THERE ARE AREAS THAT NEED FULL DEPTH RECONSTRUCTION. ALLOW FOR TIME FOR RECONSTRUCTION PRIOR TO ASPHALT OVERLAY.

TYPICAL STREET SECTION WITH RECONSTRUCTION AREA
NOT TO SCALE

STREET SIGNS

- (S1) PUBLIC WORKS TO INSTALL "BIKE LANE ENDS" SIGN
- (S2) PUBLIC WORKS TO INSTALL PEDESTRIAN CROSSING SIGN WITH DIAGONAL ARROW.

PLAN VIEW

LANGER DRIVE GRIND AND INLAY

LOCATED IN SECTION 29C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, IN THE CITY OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
SHERWOOD, OREGON 97140
PHONE: (503) 825-2309
E-MAIL: engineering@sherwoodoregon.gov

3-31-22

DESIGNED BY: CCC
DRAWN BY: CCC
CHECKED BY: RS/AS
FULL SIZE SCALE: 1"=30'
DATE: MARCH, 2022
LANGER DRIVE OVERLAY_SHEETS.DWG

REVISIONS

NO.	DESCRIPTION	DATE

JOB NO. _____
SHEET NO. **5**
OF **9**