

CITY OF SHERWOOD

2019 PAVEMENT OVERLAY PROJECTS

JULY 2019

(BID DOCUMENT VOLUME 2 OF 2 - CONTRACT DRAWINGS)

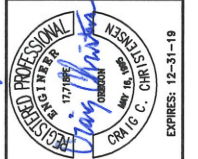
COVER SHEET

2019 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28BC, 32A AND 32C, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE ST
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
E-MAIL: engineering@sherwoodoregon.gov



7-18-19

DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS
FULL SIZE SCALE:	N/A
DATE:	JULY, 2019
2019 OVERLAYS_SHEETS.DWG	

		REVISIONS

JOB NO.	
SHEET NO.	1
	10

PROJECT LOCATION:

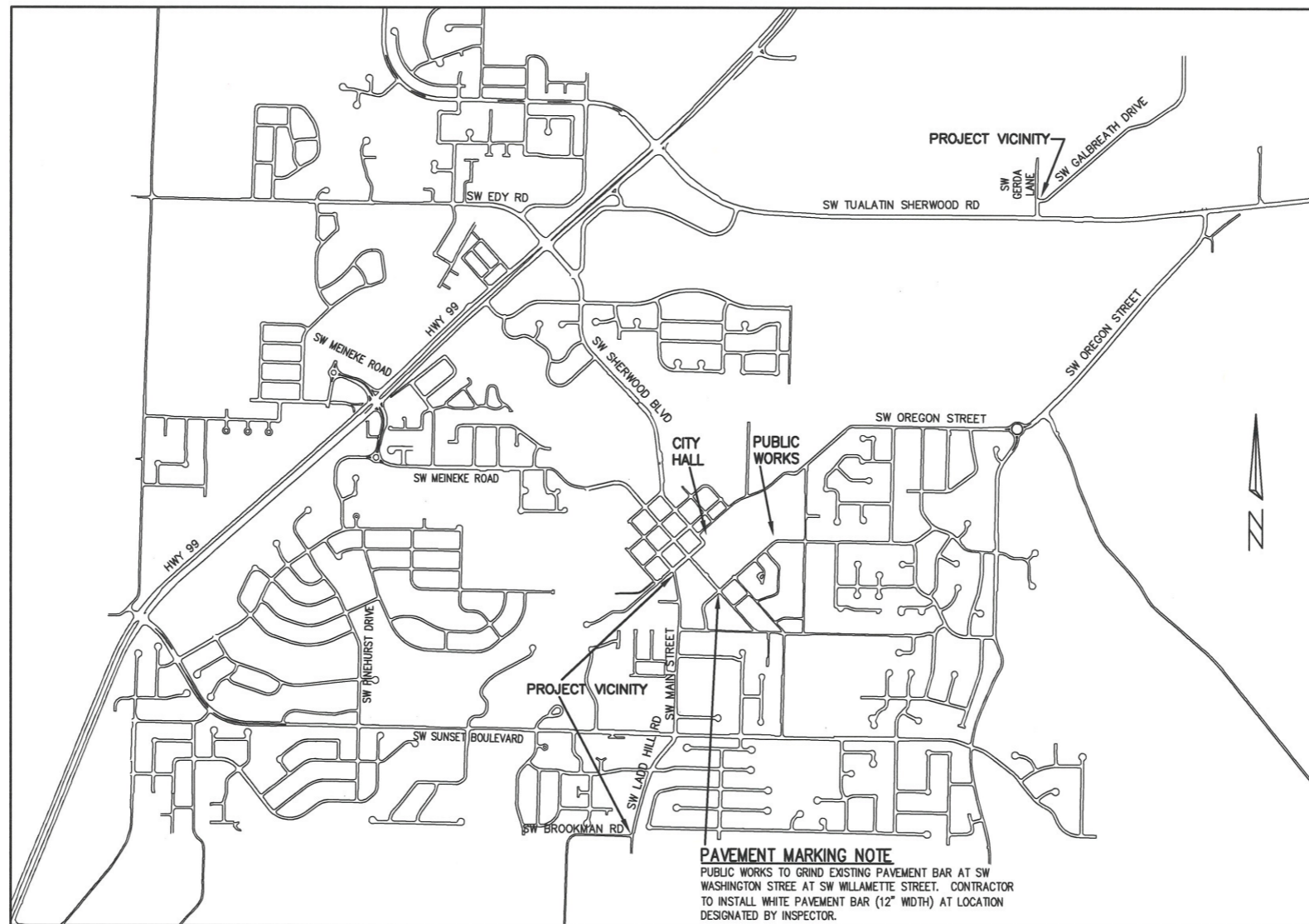
SW MAIN STREET (RAILROAD TRACKS TO SW SUNSET BLVD.)
SW LADD HILL ROAD (SW SUNSET BLVD. TO CITY LIMITS)
AND SW GALBREATH DRIVE (SW GERDA LANE TO 200' EAST)

DEVELOPER/OWNER:

CITY OF SHERWOOD
22560 SW PINE ST
SHERWOOD, OREGON 97140
CONTACT: RICH SATTLER
PH. 503-925-2319
SATTLER@SHERWOODOREGON.GOV

SHEET INDEX:

1. COVER SHEET
2. GENERAL NOTES
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6. SW MAIN STREET - PLAN VIEW
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PAVEMENT MARKING NOTE
PUBLIC WORKS TO GRIND EXISTING PAVEMENT BAR AT SW WASHINGTON STREE AT SW WILLAMETTE STREET. CONTRACTOR TO INSTALL WHITE PAVEMENT BAR (12" WIDTH) AT LOCATION DESIGNATED BY INSPECTOR.

VICINITY MAP
NOT TO SCALE

INSPECTOR INFORMATION:

CITY OF SHERWOOD: ANDY STIRLING (503) 925-2307
CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED AREA.

SURVEY INFORMATION:

HORIZONTAL
AERIAL TOPOGRAPHIC MAP (2004) AND VISUAL OBSERVATION.

VERTICAL
AERIAL TOPOGRAPHIC MAP (2004) WITH ADDITIONAL LEVEL INFORMATION PERFORMED AT CRITICAL LOCATIONS.

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL (503) 246-6699.

PRE-CONSTRUCTION, CLEARING, AND DEMOLITION NOTES:

- ALL BASE ESC MEASURES (INLET PROTECTION, PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
- SEDIMENT BARRIERS APPROVED FOR USE INCLUDE SEDIMENT FENCE, BERMS CONSTRUCTED OUT OF MULCH, CHIPPINGS, OR OTHER SUITABLE MATERIAL, STRAW WATTLES, OR OTHER APPROVED MATERIALS.
- SENSITIVE RESOURCES INCLUDING, BUT NOT LIMITED TO, TREES, WETLANDS, AND RIPARIAN PROTECTION AREAS SHALL BE CLEARLY DELINEATED WITH ORANGE CONSTRUCTION FENCING OR CHAIN LINK FENCING IN A MANNER THAT IS CLEARLY VISIBLE TO ANYONE IN THE AREA. NO ACTIVITIES ARE PERMITTED TO OCCUR BEYOND THE CONSTRUCTION BARRIER.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, STREET SWEEPING, AND VACUUMING, MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- RUN-ON AND RUN-OFF CONTROLS SHALL BE IN PLACE AND FUNCTIONING PRIOR TO BEGINNING SUBSTANTIAL CONSTRUCTION ACTIVITIES. RUN-ON AND RUN-OFF CONTROL MEASURES INCLUDE: SLOPE DRAINS (WITH OUTLET PROTECTION), CHECK DAMS, AND SURFACE ROUGHENING.

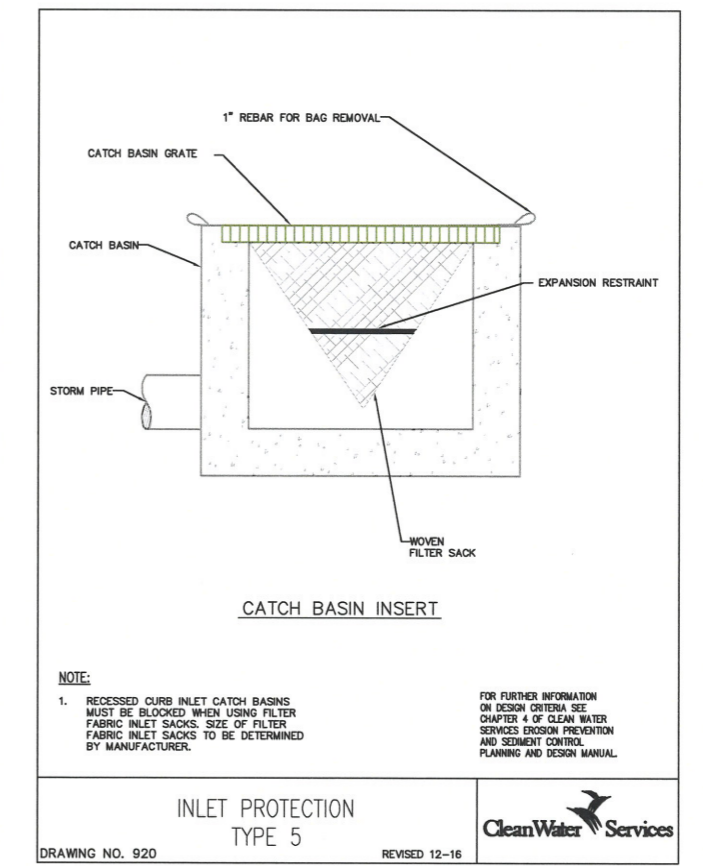
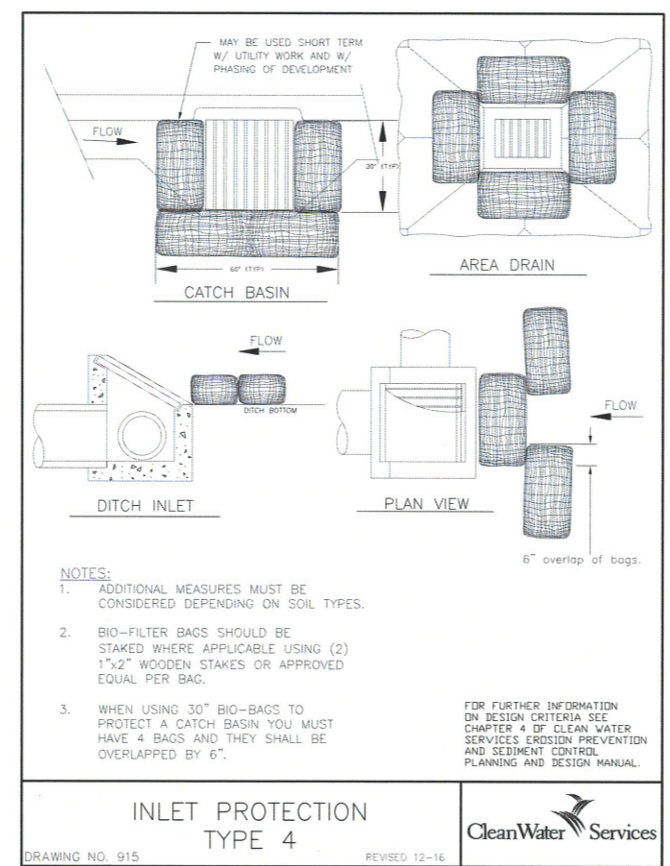
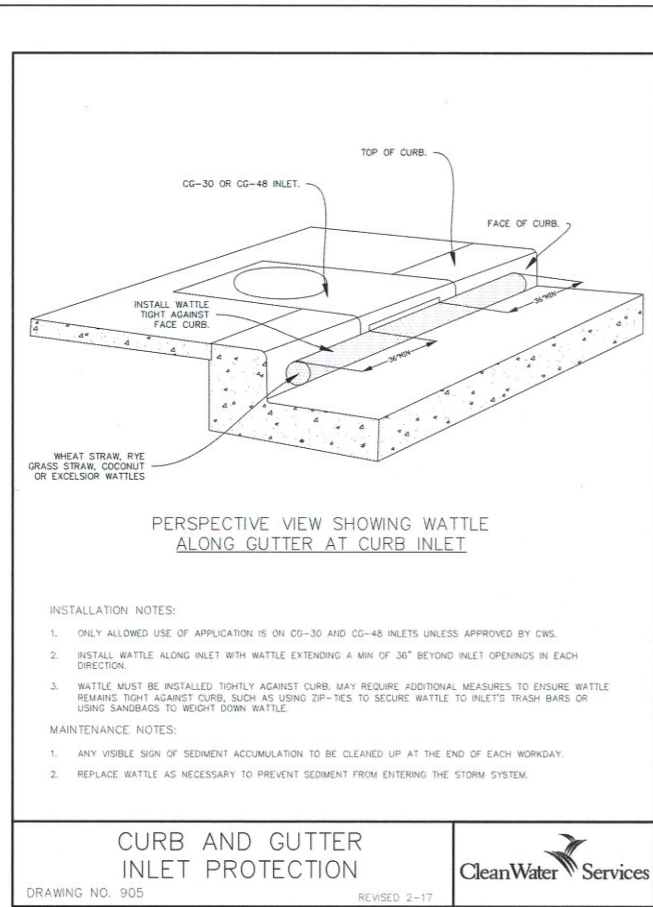
THESE EROSION AND SEDIMENT CONTROL PLANS ASSUME "DRY WEATHER" CONSTRUCTION. "WET WEATHER" CONSTRUCTION MEASURES NEED TO BE APPLIED BETWEEN OCTOBER 1ST AND MAY 31ST.

GRADING, STREET AND UTILITY EROSION AND SEDIMENT CONSTRUCTION NOTES:

- SEED USED FOR TEMPORARY OR PERMANENT SEEDING SHALL BE COMPOSED OF ONE OF THE FOLLOWING MIXTURES, UNLESS OTHERWISE AUTHORIZED:
 - VEGETATED CORRIDOR AREAS REQUIRE NATIVE SEED MIXES. SEE RESTORATION PLAN FOR APPROPRIATE SEED MIX.
 - DWARF GRASS MIX (MIN. 100 LB./AC.)
 - DWARF PERENNIAL RYEGRASS (80% BY WEIGHT)
 - CREeping RED FESCUE (20% BY WEIGHT)
 - STANDARD HEIGHT GRASS MIX (MIN. 100LB./AC.)
 - ANNUAL RYEGRASS (40% BY WEIGHT)
 - TURF-TYPE FESCUE (60% BY WEIGHT)
- SLOPE TO RECEIVE TEMPORARY OR PERMANENT SEEDING SHALL HAVE THE SURFACE ROUGHENED BY MEANS OF TRACK-WALKING OR THE USE OF OTHER APPROVED IMPLEMENTS. SURFACE ROUGHENING IMPROVES SEED BEDDING AND REDUCES RUN-OFF VELOCITY.

- LONG TERM SLOPE STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA SEEDING WITH APPROVED MIX AND APPLICATION RATE.
- TEMPORARY SLOPE STABILIZATION MEASURES SHALL INCLUDE: COVERING EXPOSED SOIL WITH PLASTIC SHEETING, STRAW MULCHING, WOOD CHIPS, OR OTHER APPROVED MEASURES.
- STOCKPILED SOIL OR STRIPPINGS SHALL BE PLACED IN A STABLE LOCATION AND CONFIGURATION. DURING "WET WEATHER" PERIODS, STOCKPILES SHALL BE COVERED WITH PLASTIC SHEETING OR STRAW MULCH. SEDIMENT FENCE IS REQUIRED AROUND THE PERIMETER OF THE STOCKPILE.
- EXPOSED CUT OR FILL AREAS SHALL BE STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS OR MATS, MID-SLOPE SEDIMENT FENCES OR WATTLES, OR OTHER APPROPRIATE MEASURES. SLOPES EXCEEDING 25% MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES.
- AREAS SUBJECT TO WIND EROSION SHALL USE APPROPRIATE DUST CONTROL MEASURES INCLUDING THE APPLICATION OF A FINE SPRAY OF WATER, PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED MEASURES.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, TIRE WASHES, STREET SWEEPING, AND VACUUMING MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

- ACTIVE INLETS TO STORM WATER SYSTEMS SHALL BE PROTECTED THROUGH THE USE OF APPROVED INLET PROTECTION MEASURES. ALL INLET PROTECTION MEASURES ARE TO BE REGULARLY INSPECTED AND MAINTAINED AS NEEDED.
- SATURATED MATERIALS THAT ARE HAULED OFF-SITE MUST BE TRANSPORTED IN WATER-TIGHT TRUCKS TO ELIMINATE SPILLAGE OF SEDIMENT AND SEDIMENT-LADEN WATER.
- AN AREA SHALL BE PROVIDED FOR THE WASHING OUT OF CONCRETE TRUCKS IN A LOCATION THAT DOES NOT PROVIDE RUN-OFF THAT CAN ENTER THE STORM WATER SYSTEM OR SURFACE WATERS. IF THE CONCRETE WASH-OUT AREA CAN NOT BE CONSTRUCTED GREATER THAN 50' FROM ANY DISCHARGE POINT, SECONDARY MEASURES SUCH AS BERMS OR TEMPORARY SETTLING PITS MAY BE REQUIRED. THE WASH-OUT SHALL BE LOCATED WITHIN SIX FEET OF TRUCK ACCESS AND BE CLEANED WHEN IT REACHES 50% OF THE CAPACITY.
- SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE SHALL NOT BE TRANSFERRED TO THE STORM WATER SYSTEM. SWEEPINGS SHALL BE PICKED UP AND DISPOSED IN THE TRASH.
- AVOID PAVING IN WET WEATHER WHEN PAVING CHEMICALS CAN RUN-OFF INTO THE STORM WATER SYSTEM.
- USE BMPs SUCH AS CHECK-DAMS, BERMS, AND INLET PROTECTION TO PREVENT RUN-OFF FROM REACHING DISCHARGE POINTS.
- COVER CATCH BASINS, MANHOLES, AND OTHER DISCHARGE POINTS WHEN APPLYING SEAL COAT, TACK COAT, ETC. TO PREVENT INTRODUCING THESE MATERIALS TO THE STORM WATER SYSTEM.



EROSION CONTROL NOTES AND DETAILS

2019 PAVEMENT OVERLAY PROJECTS

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CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
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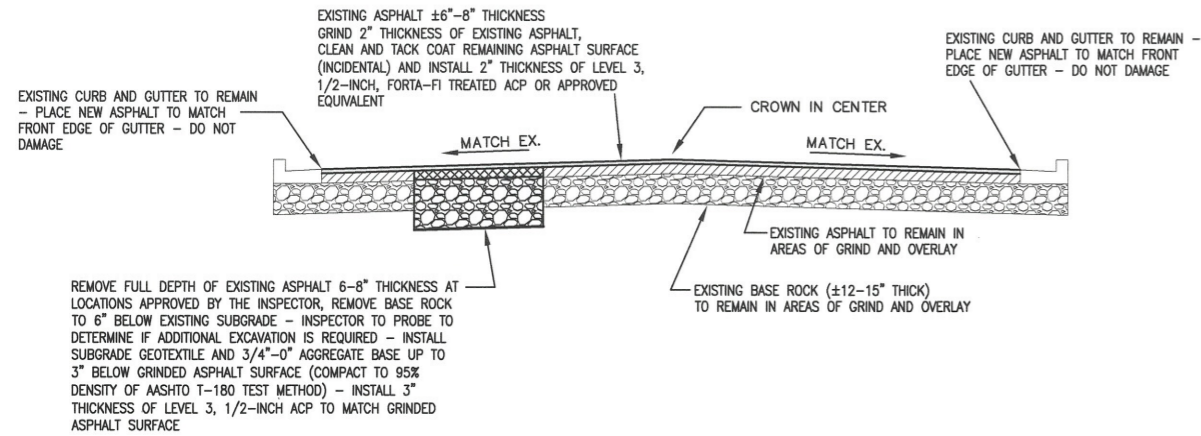
7-18-19

REGISTERED PROFESSIONAL ENGINEER
17744
1998
CRAIG C. CHRISTENSEN
EXPIRES: 12-31-19

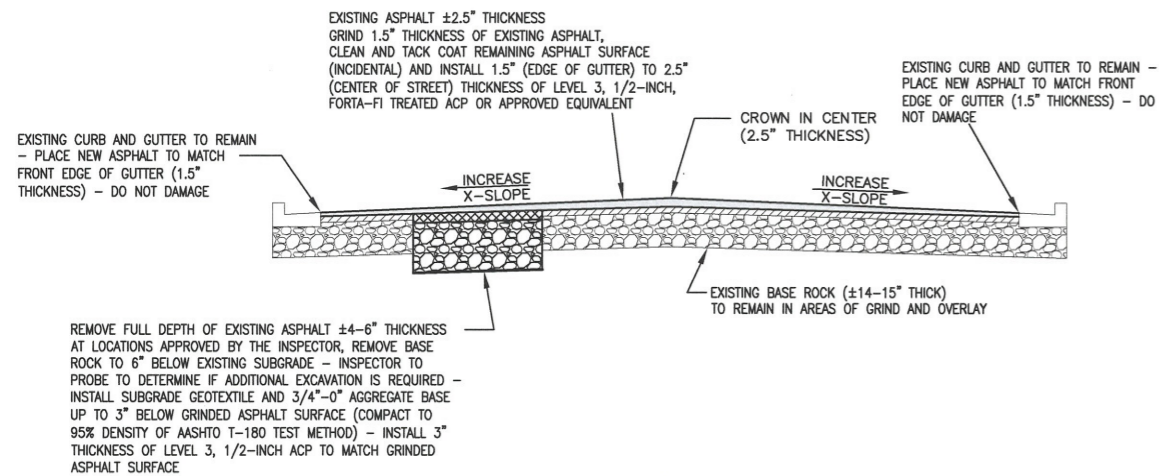
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2019 OVERLAYS...SHEETS	

REVISIONS

JOB NO.	
SHEET NO.	4
	10



TYPICAL STREET OVERLAY MATCH SECTION
(MAIN STREET - NOT TO SCALE)



TYPICAL STREET OVERLAY INCREASING CROWN SECTION
(LADD HILL ROAD - NOT TO SCALE)

STREET SECTION NOTES

EXISTING CURB TO REMAIN - DO NOT DAMAGE.

COMPACT SUBGRADE TO 95% DENSITY OF AASHTO T-99 TEST METHOD.

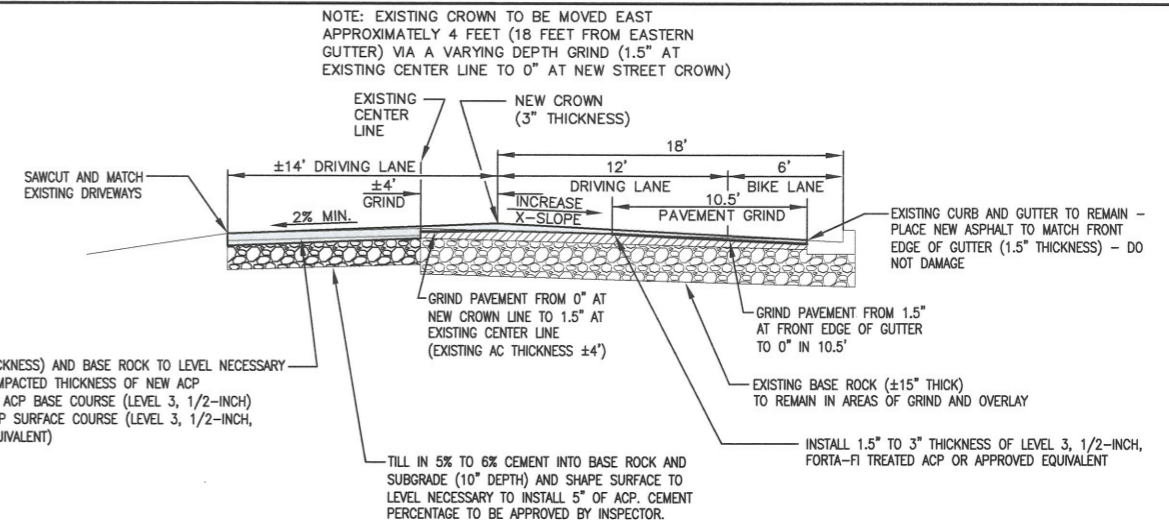
COMPACT ALL BASE ROCK TO 95% DENSITY OF AASHTO T-180 TEST METHOD (INSPECTOR WITNESSED PROOF ROLL REQUIRED).

COMPACT ALL ASPHALT TO 92% OF MMAD PER AASHTO T209.

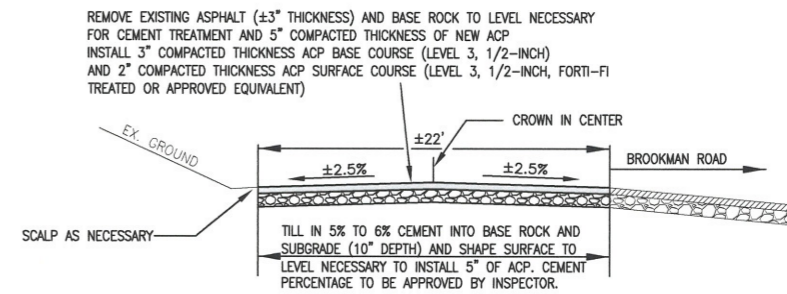
BASE ROCK WILL BE PROOF ROLLED AFTER CEMENT TREATMENT.

CEMENT TREATMENT TO BE KEPT MOIST UNTIL 1ST LIFT OF ASPHALT IS PLACED. FIRST LIFT OF ASPHALT SHALL BE INSTALLED EITHER THE DAY OF OR THE DAY AFTER CEMENT TREATMENT OF THE BASE. FIRST LIFT OF ASPHALT WILL NEED TO BE INSTALLED THE DAY OF CEMENT TREATMENT FOR SW GALBREATH DRIVE DUE TO HIGH VOLUMES OF TRUCK TRAFFIC.

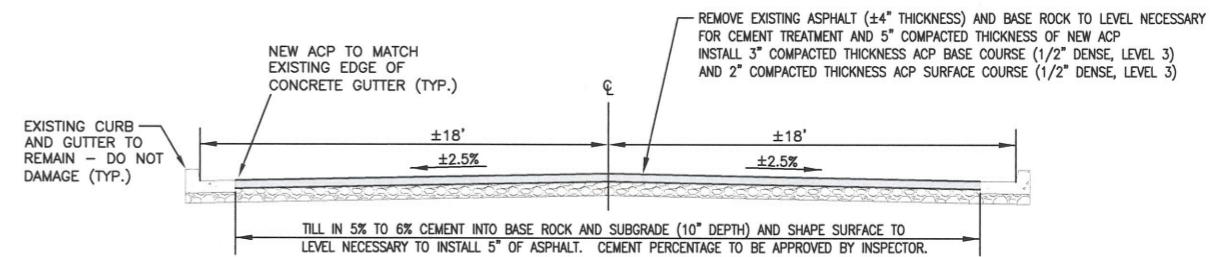
CURB EXPOSURE TO BE ±6" IN STANDARD HEIGHT CURB LOCATIONS. MAY FLUCTUATE AS NECESSARY TO AVOID PONDING AT GUTTER LINE. DO NOT COVER WEEP HOLES. CURB EXPOSURES IN DRIVEWAYS SHALL BE ±3/4". ASPHALT AT THROAT OF SIDEWALK RAMPS SHALL BE FLUSH WITH CURB.



TYPICAL STREET OVERLAY INCREASING CROWN AND CTB SECTION
(LADD HILL ROAD LOOKING NORTH - NOT TO SCALE)



TYPICAL FULL WIDTH CEMENT TREATMENT SECTION
(LADD HILL ROAD - NOT TO SCALE)



TYPICAL CEMENT TREATMENT SECTION
(GALBREATH DRIVE - NOT TO SCALE)

NOTE: EXISTING CROWN TO BE MOVED EAST APPROXIMATELY 4 FEET (18 FEET FROM EASTERN GUTTER) VIA A VARYING DEPTH GRIND (1.5" AT EXISTING CENTER LINE TO 0" AT NEW STREET CROWN)

STREET SECTIONS

2019 PAVEMENT OVERLAY PROJECTS

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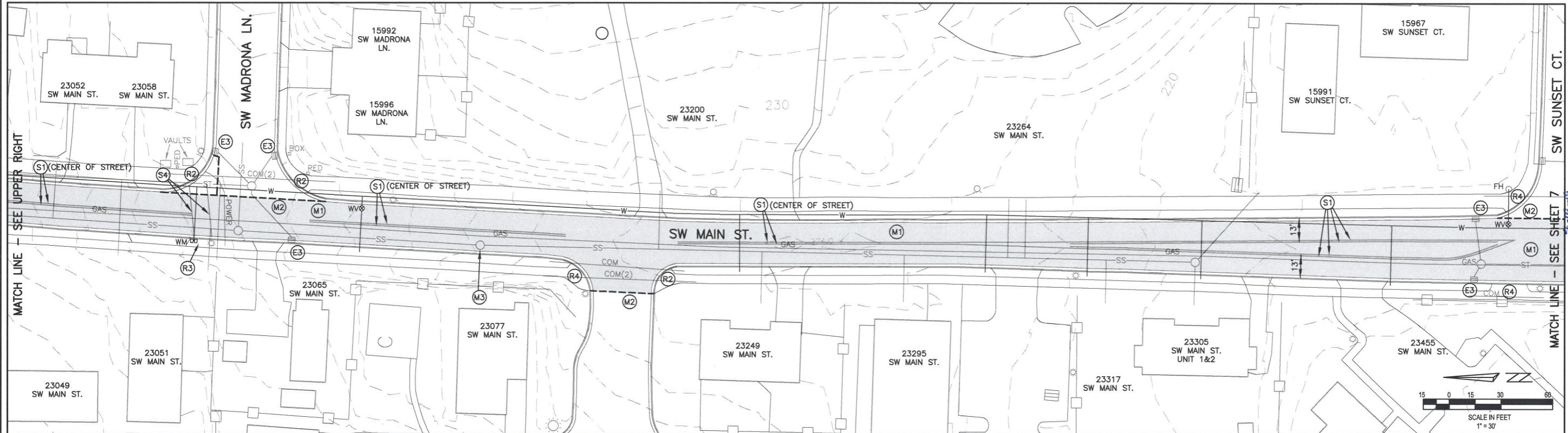
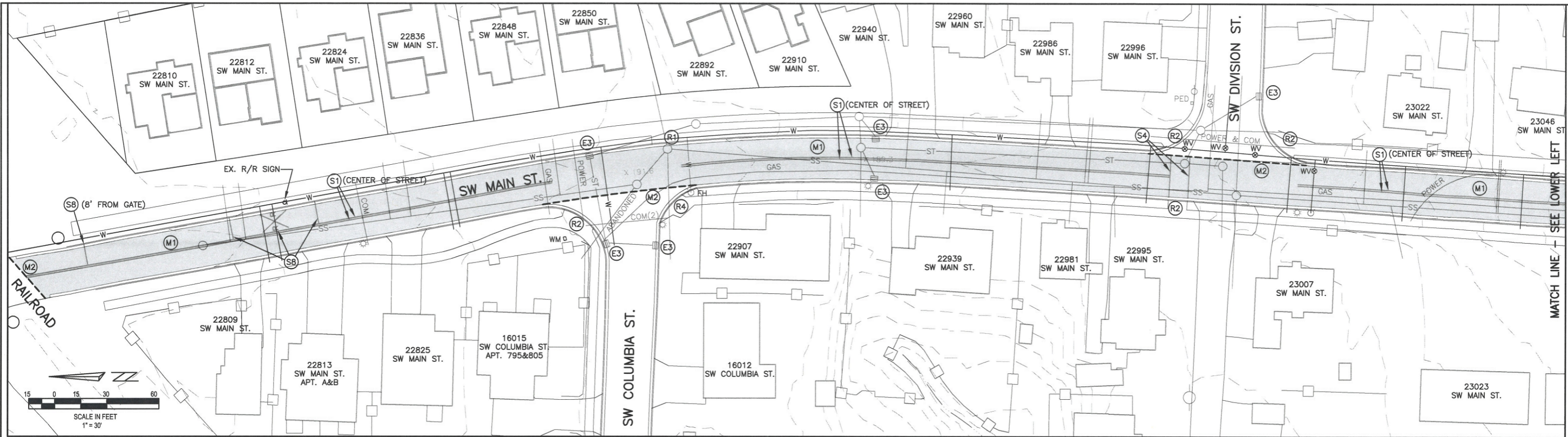
CITY OF SHERWOOD
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L. J. JONES
CRA 16 C. CHRY
EXPIRES: 12-31-19

DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS
FULL SIZE SCALE:	N/A
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2019 OVERLAYS_SHEETS	

REVISIONS	

JOB NO.	
SHEET NO.	5
OF	10



SIDEWALK RAMP KEY NOTES

- (SIDEWALK RAMP TO BE CONSTRUCTED ONE RAMP AT A TIME PER INTERSECTION UNLESS OTHERWISE APPROVED)
- (R1) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE.
 - (R2) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R3) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP AT LOCATION WHERE RAMP DOES NOT CURRENTLY EXIST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R4) REMOVE AND REPLACE EXISTING SIDEWALK AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R5) REMOVE AND REPLACE EXISTING CURB AND GUTTER AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. INSTALL 5-FOOT WIDE SIDEWALK TO MATCH SIDEWALK TO THE EAST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.

STREET KEY NOTES

- (M1) AREA OF PAVEMENT GRINDING AND NEW ASPHALT (SHADED AREA - SEE SECTION ON SHEET 5). ONE LANE TO REMAIN OPEN DURING CONSTRUCTION VIA THE USE OF FLAGGERS. REOPEN ALL LANES AT THE END OF EACH WORK DAY.
- (M2) SAW CUT TO DEPTH OF OVERLAY REQUIRED. COORDINATE WITH CITY INSPECTOR FOR EXACT LOCATION OF SAW CUT.
- (M3) SAWCUT AND REPLACE CONCRETE GRADE RINGS. ADJUST TO MATCH FINISH GRADE.

STRIPING KEY NOTES

- (CONFIRM EXACT LOCATION WITH INSPECTOR/ENGINEER)
- (S1) INSTALL YELLOW PAINT LINE (4" WIDTH)
 - (S2) INSTALL WHITE PAINT LINE (4" WIDTH)
 - (S3) INSTALL WHITE PAINT LINE (8" WIDTH)
 - (S4) INSTALL WHITE THERMOPLASTIC BAR (12" WIDTH)
 - (S5) INSTALL WHITE THERMOPLASTIC SYMBOL (SEE PLAN VIEW)
 - (S6) INSTALL WHITE THERMOPLASTIC WORD (SEE PLAN VIEW)
 - (S7) INSTALL WHITE THERMOPLASTIC BAR (8" WIDTH)
 - (S8) INSTALL WHITE THERMOPLASTIC RAILROAD CROSSING SYMBOL WITH PAVEMENT BARS PER MUTCD

NOTIFICATION NOTE

PRIOR TO ANY LANE CLOSURE, THE CONTRACTOR SHALL NOTIFY ALL OF THE RESIDENTS WITH DIRECT DRIVEWAY ACCESS TO SW MAIN STREET AND SW LADD HILL ROAD AND ALL RESIDENTS ON SW COLUMBIA STREET AND SW DIVISION STREET WEST OF SW MAIN STREET ONE WEEK IN ADVANCE OF BEGINNING THE WORK.

TRAFFIC CONTROL PLAN

CONTRACTOR SHALL PREPARE TRAFFIC CONTROL PLANS SPECIFIC TO THE STAGING BEING PERFORMED. CONTACT THE CITY INSPECTOR FOR IN PLACE INSPECTION OF TRAFFIC CONTROL MEASURES PRIOR TO COMMENCEMENT OF CONSTRUCTION WORK.

EROSION CONTROL KEY NOTES

- (E1) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING.
- (E2) INSTALL SILT SACK IN CATCH BASIN.
- (E3) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING AND SILT SACK IN CATCH BASIN.
- (E4) INSTALL BIOBAG IN GUTTER.

REQUIRED RAILROAD PERMIT

CONTRACTOR SUPPLY ALL INFORMATION, INSURANCE, ETC. AS REQUIRED BY THE GENESEE AND WYOMING TO OBTAIN PERMIT PRIOR TO WORKING IN RAILROAD RIGHT-OF-WAY. CONTRACTOR TO COORDINATE WITH GENESEE AND WYOMING TO DETERMINE IF RAILROAD FLAGGER(S) WILL BE REQUIRED IN ORDER TO PERFORM CONSTRUCTION WORK.

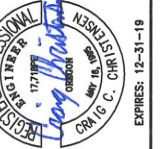
SW MAIN STREET (PLAN VIEW)

2019 PAVEMENT OVERLAY PROJECTS

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CITY OF SHERWOOD
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177 W. SW MAIN STREET
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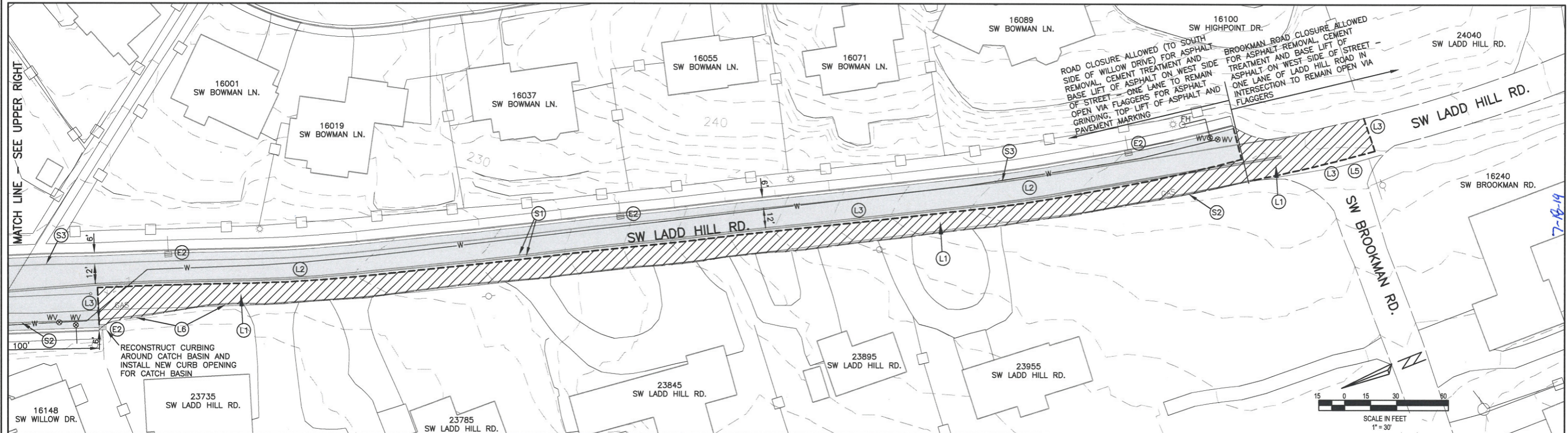
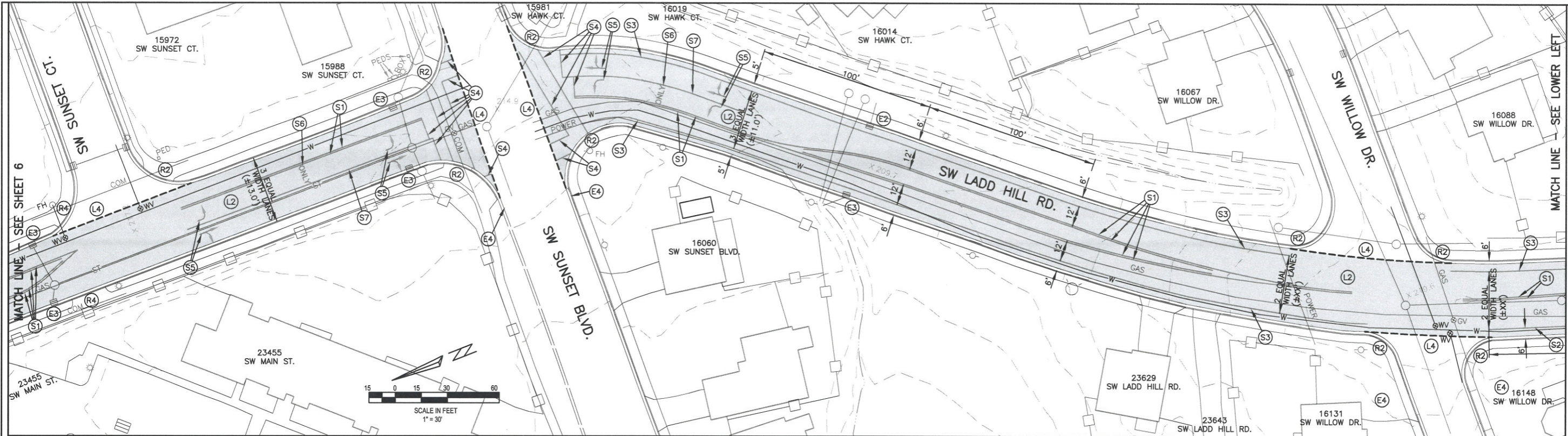
PHONE: (503) 925-2309
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DESIGNED BY:	CCC
DRAWN BY:	CCC
CHECKED BY:	RS
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2019 OVERLAYS_SHEETS.DWG	

JOB NO.	
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SHEET	6
OF	10

REVISIONS



**SW LADD HILL ROAD
(PLAN VIEW)**

2019 PAVEMENT OVERLAY PROJECTS

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IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
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CITY OF SHERWOOD
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300 SW PINE STREET
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PROFESSIONAL ENGINEER
No. 17796
Ernie Christensen
P.E.
CRE 16 C. CHR
EXPIRES: 12-31-19

DESIGNED BY:	CCC
DRAWN BY:	CCC
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FULL SIZE SCALE:	1"=30'
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2019 OVERLAYS_SHEETS.DWG

REVISIONS	
JOB NO.	
SHEET NO.	7
	10

SIDEWALK RAMP KEY NOTES

- (SIDEWALK RAMPS TO BE CONSTRUCTED ONE RAMP AT A TIME PER INTERSECTION UNLESS OTHERWISE APPROVED)
- (R1) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE.
 - (R2) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R3) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP AT LOCATION WHERE RAMP DOES NOT CURRENTLY EXIST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R4) REMOVE AND REPLACE EXISTING SIDEWALK AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
 - (R5) REMOVE AND REPLACE EXISTING CURB AND GUTTER AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. INSTALL 5-FOOT WIDE SIDEWALK TO MATCH SIDEWALK TO THE EAST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.

STREET KEY NOTES

- (L1) AREA OF CEMENT TREATMENT AND NEW ASPHALT (HATCHED AREA - SEE SECTION ON SHEET 5). ONE LANE TO REMAIN OPEN DURING CONSTRUCTION VIA THE USE OF FLAGGERS. REOPEN ALL LANES AT THE END OF EACH WORK DAY.
- (L2) AREA OF PAVEMENT GRINDING AND NEW ASPHALT (SHADED AREA - SEE SECTION ON SHEET 5). ONE LANE TO REMAIN OPEN DURING CONSTRUCTION VIA THE USE OF FLAGGERS. REOPEN ALL LANES AT THE END OF EACH WORK DAY.
- (L3) FULL DEPTH SAW CUT REQUIRED. COORDINATE WITH CITY INSPECTOR FOR EXACT LOCATION OF SAW CUT.
- (L4) SAW CUT TO DEPTH OF OVERLAY REQUIRED. COORDINATE WITH CITY INSPECTOR FOR EXACT LOCATION OF SAW CUT.
- (L5) CONTRACTOR SHALL PROVIDE FLAGGING TO KEEP THE SW LADD HILL ROAD/SW BROOKMAN ROAD INTERSECTION FUNCTIONAL FOR VEHICULAR TRAFFIC DURING CONSTRUCTION.
- (L6) EXISTING EXTRUDED CURB TO BE REMOVED. INSTALL NEW EXTRUDED CURB TO MATCH EXISTING.

STRIPING KEY NOTES

- (CONFIRM EXACT LOCATION WITH INSPECTOR/ENGINEER)
- (S1) INSTALL YELLOW PAINT LINE (4" WIDTH)
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TRAFFIC CONTROL PLAN

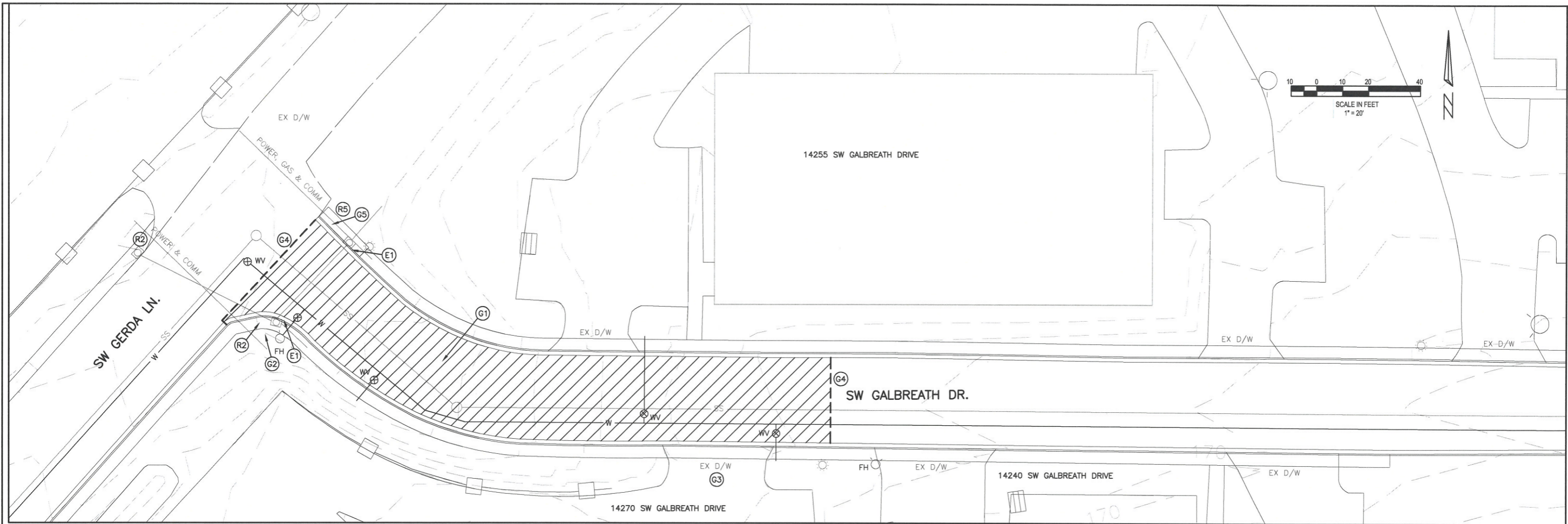
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EROSION CONTROL KEY NOTES

- (E1) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING.
- (E2) INSTALL SILT SACK IN CATCH BASIN.
- (E3) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING AND SILT SACK IN CATCH BASIN.
- (E4) INSTALL BIOBAG IN GUTTER.

REQUIRED COUNTY PERMITS

FINAL PERMITS FROM WASHINGTON COUNTY AND CLACKAMAS COUNTY ARE REQUIRED. CONTRACTOR TO SUPPLY TRAFFIC CONTROL PLANS, INSURANCE (NAMING BOTH COUNTIES AS ADDITIONALLY INSURED) AND AMEND PERMITS AS REQUIRED BY COUNTY.



SIDEWALK RAMP KEY NOTES

- (R1) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE.
- (R2) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- (R3) REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP AT LOCATION WHERE RAMP DOES NOT CURRENTLY EXIST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- (R4) REMOVE AND REPLACE EXISTING SIDEWALK AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- (R5) REMOVE AND REPLACE EXISTING CURB AND GUTTER AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. INSTALL 5-FOOT WIDE SIDEWALK TO MATCH SIDEWALK TO THE EAST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.

STREET KEY NOTES

- (G1) AREA OF CEMENT TREATMENT AND NEW ASPHALT (HATCHED AREA - SEE SECTION ON SHEET 5). ONE LANE TO REMAIN OPEN DURING CONSTRUCTION VIA THE USE OF FLAGGERS. REOPEN ALL LANES AT THE END OF EACH WORK DAY.
- (G2) IRRIGATION EXISTS IN THIS AREA - DO NOT DAMAGE. ANY DAMAGE TO IRRIGATION SYSTEM SHALL BE REPAIRED INCLUDING ANY RESULTING DAMAGE CAUSED TO THE REST OF THE SYSTEM DUE TO ANY CONTRACTOR CAUSED BREAK. RESTORE PROPERTY TO EXISTING CONDITION OR BETTER.
- (G3) CONTRACTOR SHALL CEMENT TREAT IN A MANNER TO KEEP ONE HALF OF THE DRIVEWAY OPEN AT ALL TIMES (ACTIVE COMMERCIAL BUSINESS). THE DRIVEWAY SHALL BE COMPLETELY OPEN AT THE END OF EACH WORK DAY. CONTACT BUSINESS AT LEAST ONE WEEK IN ADVANCE SO BUSINESS HAS OPPORTUNITY TO REARRANGE DELIVERY SCHEDULE.
- (G4) FULL DEPTH SAW CUT REQUIRED. COORDINATE WITH CITY INSPECTOR FOR EXACT LOCATION OF SAW CUT.
- (G5) CONTRACTOR TO RELOCATE STOP SIGN. STOP SIGN TO REMAIN UP AT ALL TIMES DURING CONSTRUCTION.

NOTIFICATION NOTE

PRIOR TO ANY LANE CLOSURE, THE CONTRACTOR SHALL NOTIFY ALL OF THE BUSINESSES ALONG SW GALBREATH DRIVE AND SW GERDA LANE ONE WEEK IN ADVANCE OF BEGINNING THE WORK.

TRAFFIC CONTROL PLAN

CONTRACTOR SHALL PREPARE TRAFFIC CONTROL PLANS SPECIFIC TO THE STAGING BEING PERFORMED. CONTACT THE CITY INSPECTOR FOR IN PLACE INSPECTION OF TRAFFIC CONTROL MEASURES PRIOR TO COMMENCEMENT OF CONSTRUCTION WORK. FLAGGING WILL LIKELY BE REQUIRED FOR CONSTRUCTION OF ADA RAMP AT THE SOUTHEAST CORNER OF THE SW GALBREATH DRIVE/SW GERDA LANE INTERSECTION DUE TO TRUCK TRAFFIC.

EROSION CONTROL KEY NOTES

- (E1) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING.

**SW GALBREATH DRIVE
(PLAN VIEW)**

2019 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28BC, 32B AND 32C, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

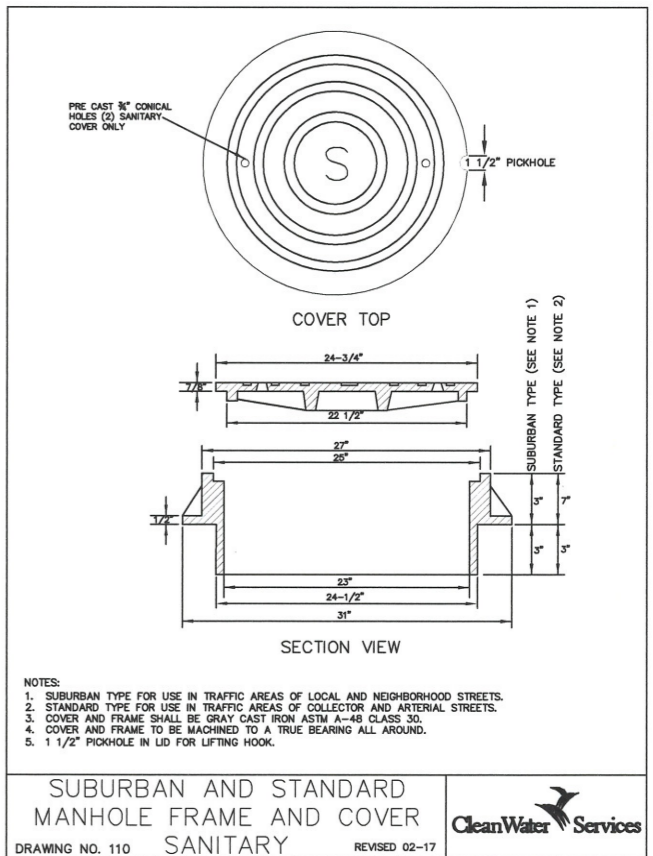
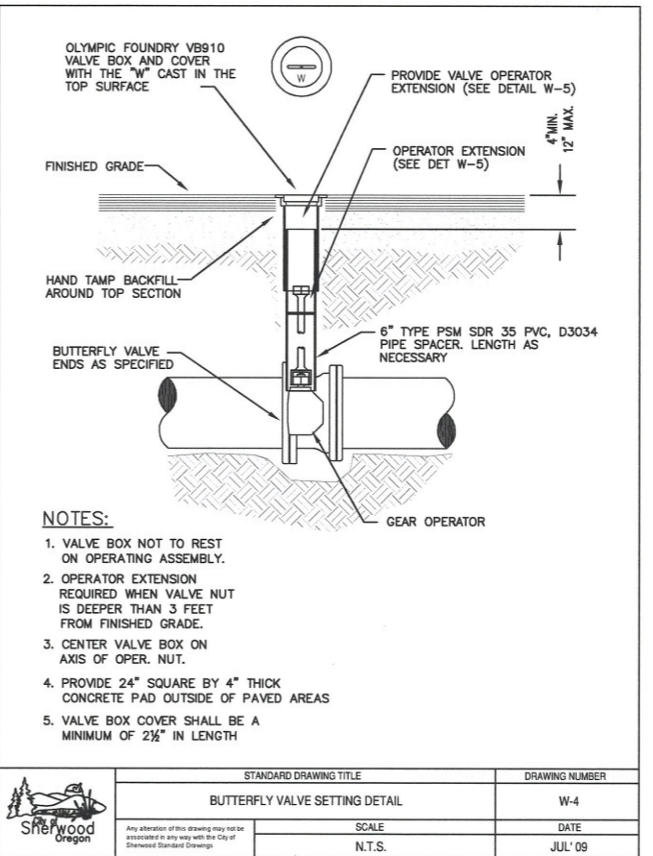
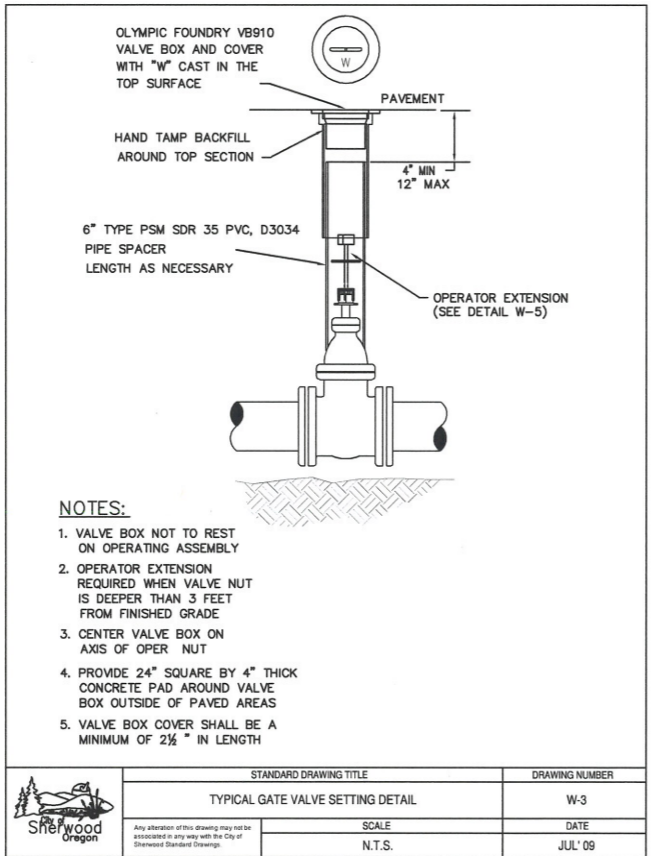
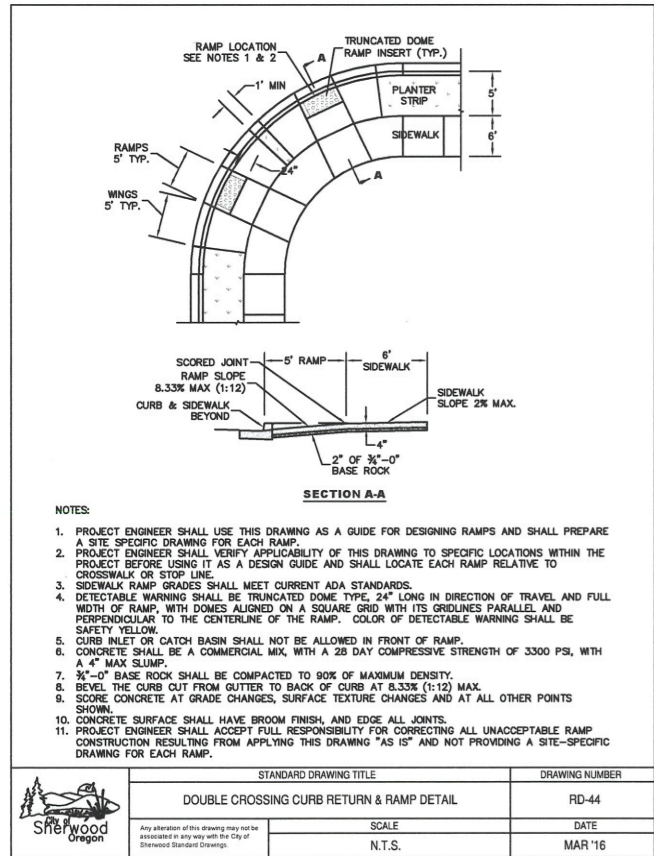
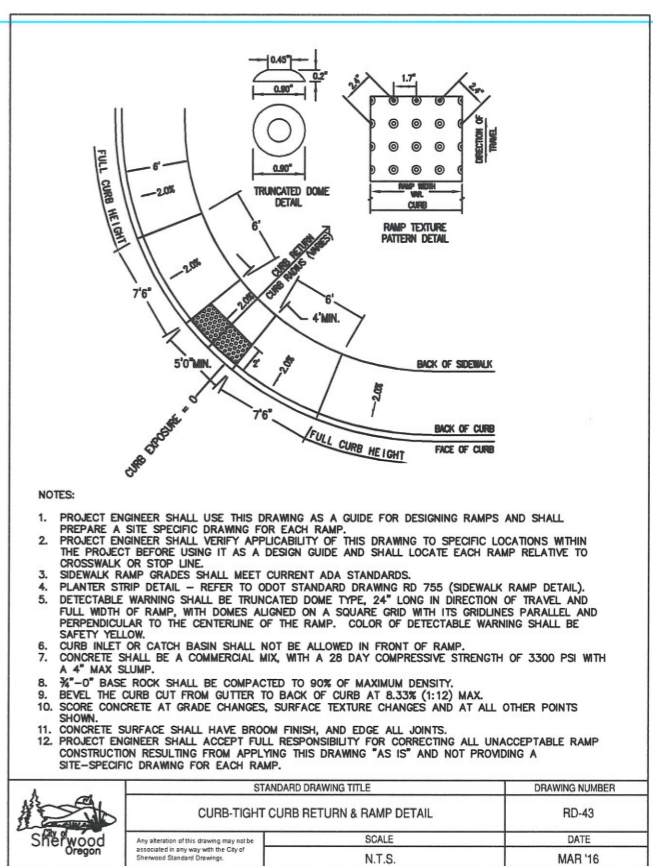
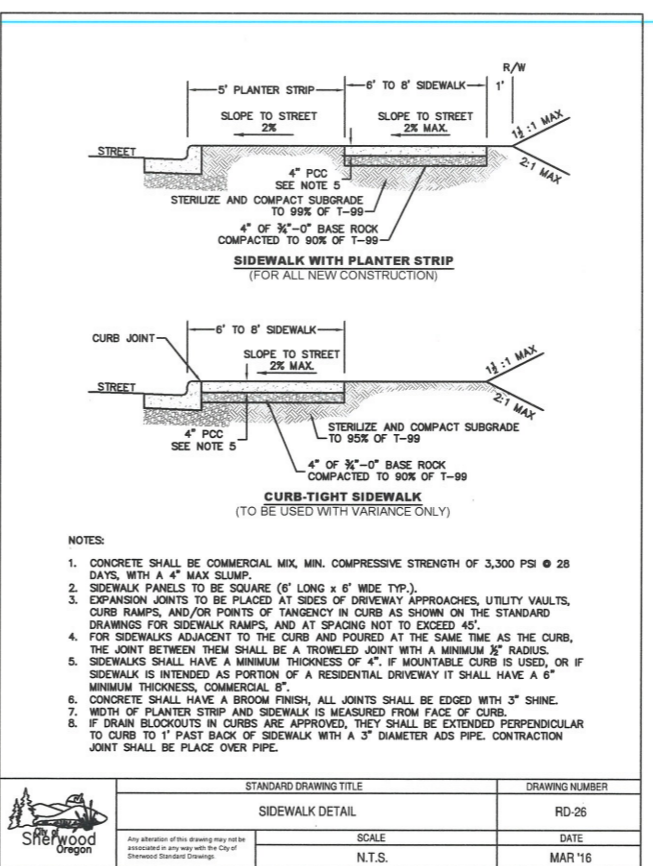
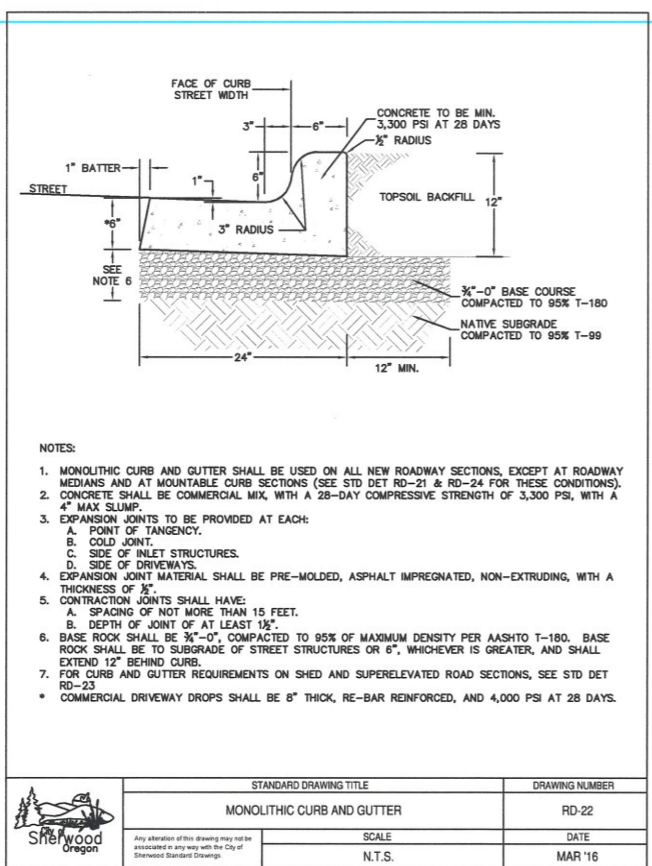
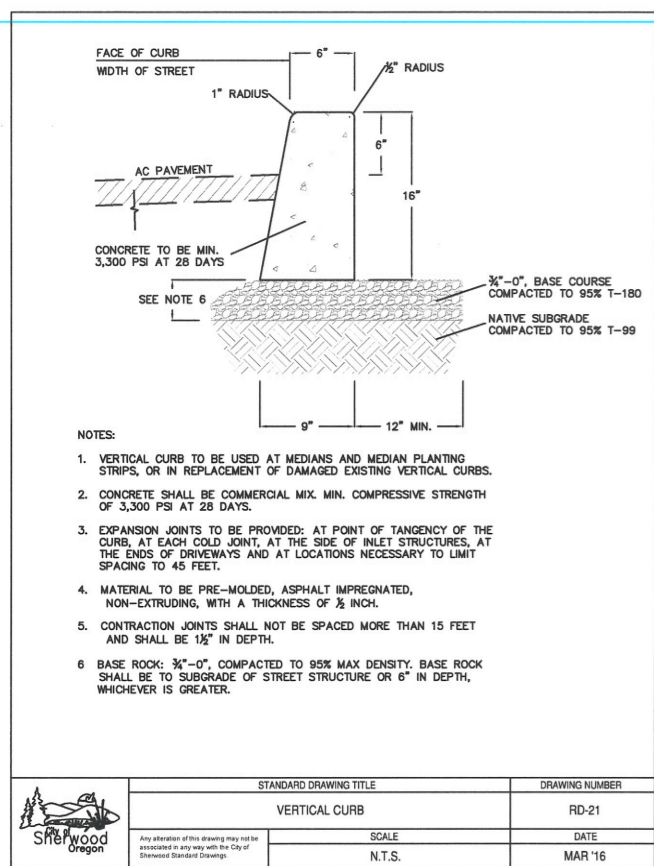
7-18-19

DESIGNED BY:	CCC	DRAWN BY:	CCC
CHECKED BY:	RS	FULL SIZE SCALE:	1"=20'
DATE:	JULY, 2019	2019 OVERLAYS_SHEETS.DWG	

REVISIONS

NO.	DESCRIPTION

SHEET NO. **8**
OF **10**



CITY OF SHERWOOD DETAILS

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28BC, 32B AND 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, OREGON

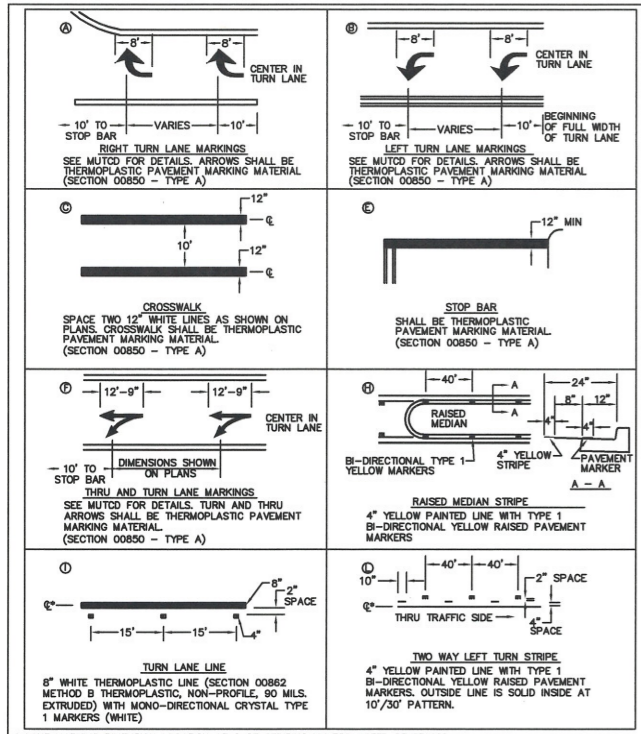
CITY OF SHERWOOD ENGINEERING DEPARTMENT 22560 SW PINE STREET SHERWOOD, OREGON 97140
PHONE: (503) 925-2309 E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL ENGINEER
CRAIG C. CHRISTENSEN
EXPIRES: 12-31-18

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	AS NOTED
DATE:	AUGUST, 2018
SUNSET SHEETS	

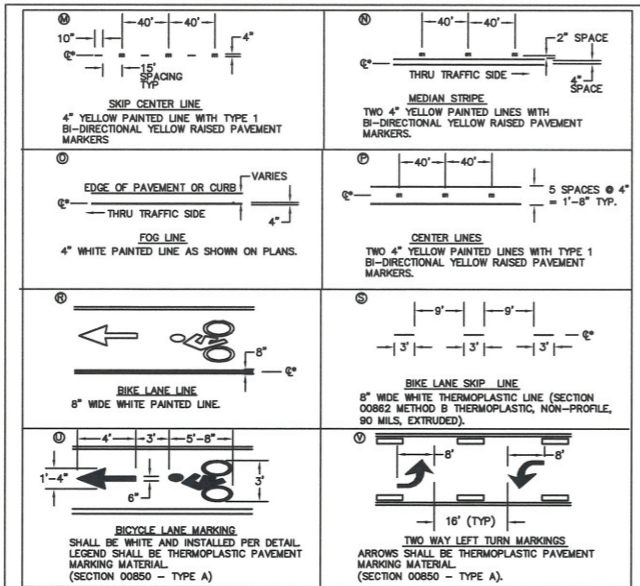
REVISIONS

JOB NO.	
SHEET NO.	9
	10



* LANE MARKING DIMENSION LOCATION AT & OF STRIPING UNLESS NOTED OTHERWISE.
SEE DRAWING NUMBER S-4 FOR STRIPING NOTES.

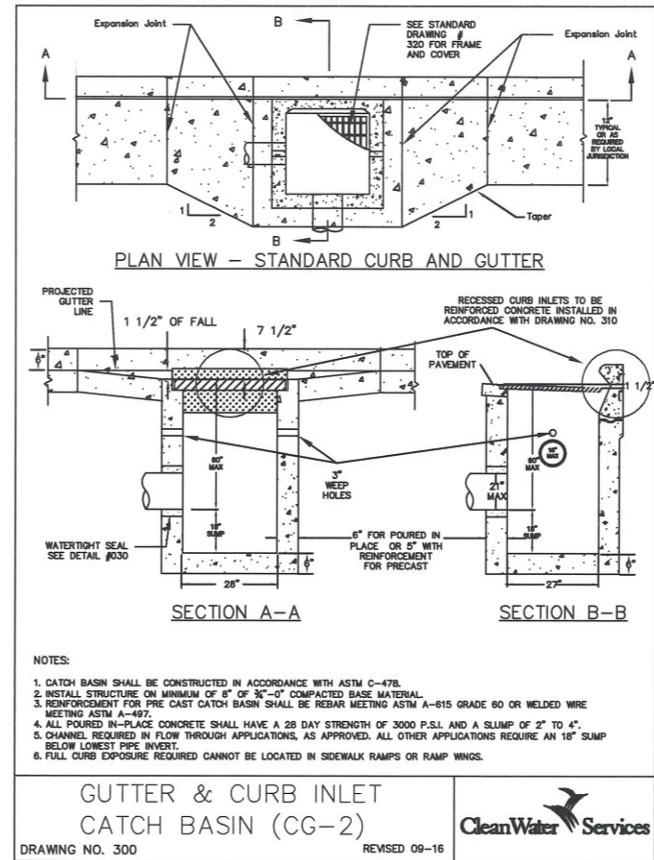
STANDARD DRAWING TITLE	DRAWING NUMBER
STRIPING DETAILS	S-3
SCALE	DATE
N.T.S.	MAR '16



NOTES:
1. LOCATE STOP BARS 10' BACK OF THE EXTENDED FOG LINE, EDGE OF PAVEMENT, OR CURB FACE. VERIFY SIGHT DISTANCE.
2. LOCATE CROSSWALKS AS PER WHEELCHAIR RAMP LOCATIONS OR 5' BACK OF EXTENDED FOG LINE, EDGE OF PAVEMENT OR CURB FACE.
3. REMOVAL OF EXISTING STRIPING IS TO BE DETERMINED IN THE FIELD AND IS CONSIDERED INCIDENTAL WORK. STRIPING SHALL BE GROUND OFF AS DIRECTED BY THE ENGINEER.
4. ALL THERMOPLASTIC PAVEMENT MARKING MATERIAL SHALL BE INSTALLED AS PER OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION. (LATEST EDITION).
5. ALL PAINTED LINES SHALL CONFORM TO THE MOST CURRENT ODOT SPECIFICATIONS FOR BEAD BINDER PAINT AS LISTED IN ODOT'S OPL.

* LANE MARKING DIMENSION LOCATION AT & OF STRIPING UNLESS NOTED OTHERWISE.

STANDARD DRAWING TITLE	DRAWING NUMBER
STRIPING DETAILS II	S-4
SCALE	DATE
N.T.S.	MAR '16



CITY OF SHERWOOD DETAILS

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28BC, 32B AND 32C, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
222560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 895-2309
E-MAIL: engineering@ci.sherwood.oregon.gov

7-18-19

REGISTERED PROFESSIONAL ENGINEER
19196
C. CHRISTENSEN
MAY 16, 2016

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	AS NOTED
DATE:	APRIL 7, 2015
SUNSET SHEETS	

REVISIONS

JOB NO.	SHEET NO.	SHEET
	10	10