or 10

# SURVEY INFORMATION:

HORIZONTAL AERIAL TOPOGRAPHIC MAP (2004) AND VISUAL OBSERVATION.

AERIAL TOPOGRAPHIC MAP (2004) WITH ADDITIONAL LEVEL INFORMATION PERFORMED AT

# CITY OF SHERWOOD 2019 PAVEMENT OVERLAY PROJECTS

**JULY 2019** 

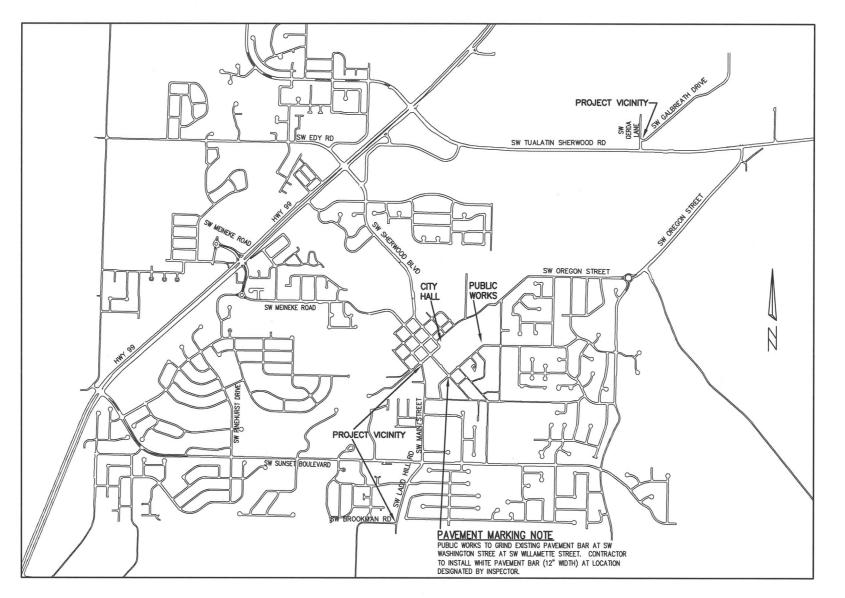
(BID DOCUMENT VOLUME 2 OF 2 - CONTRACT DRAWINGS)

SW MAIN STREET (RAILROAD TRACKS TO SW SUNSET BLVD.) SW LADD HILL ROAD (SW SUNSET BLVD. TO CITY LIMITS) AND SW GALBREATH DRIVE (SW GERDA LANE TO 200' EAST)

DEVELOPER/OWNER: CITY OF SHERWOOD 22560 SW PINE ST SHERWOOD, OREGON 97140 PH. 503-925-2319

#### SHEET INDEX:

- COVER SHEET
- GENERAL NOTES
- 1200-CN COVER
- EROSION CONTROL NOTES AND DETAILS
- STREET SECTIONS
- SW MAIN STREET PLAN VIEW
- SW LADD HILL ROAD PLAN VIEW
- SW GALBREATH DRIVE PLAN VIEW
- CITY OF SHERWOOD DETAILS
- 10. CITY OF SHERWOOD DETAILS



THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY PROHIBITED AREA.

## VICINITY MAP

NOT TO SCALE

#### INPSECTOR INFORMATION:

CITY OF SHERWOOD: ANDY STIRLING (503) 925-2307 CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL (503) 246-6699.

- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROCURE ALL APPLICABLE PERMITS, LICENSES, AND
  CERTIFICATES RELATIVE TO THE TRADES TO COMPLETE THE PROJECT AND FOR THE USE OF SUCH WORK WHEN COMPLETED.
  COMPLIANCE SHALL BE AT ALL LEVELS, FEDERAL, STATE, COUNTY, AND LOCAL, RELATING TO THE PERFORMANCE OF THIS WORK, A CITY OF SHERWOOD OR A METRO BUSINESS LICENSE IS REQUIRED FOR THE CONTRACTOR AND ALL
- 3. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION AND ARRANGE FOR PROTECTION OF UTILITIES IN CONFLICT WITH PROPOSED WORK. THE LOCATIONS, DEPTH AND DESCRIPTION OF EXISTING UTILITIES SHOWN WERE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. ADDITIONAL UTILITIES MAY EXIST WITHIN THE WORK AREA. POTHOLING PRIOR TO CONSTRUCTION MAY BE NECESSARY. ALL WORK TO VERIFY UTILITY LOCATIONS AND DEPTHS IS INCIDENTAL.
- 4. OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON LITHLITY NOTIFICATION CENTER THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT WWW.STATE.OR.US/GOVERNMENT.HTM. THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION, CALL (503) 246-6699.
- 5. THE CONTRACTOR SHALL MAKE PROVISIONS TO KEEP ALL EXISTING UTILITIES IN SERVICE AND PROTECT THEM DURING CONSTRUCTION. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGED UTILITIES USING MATERIAL AND METHODS APPROVED BY THE UTILITY OWNER. NO SERVICE INTERRUPTIONS SHALL BE PERMITTED WITHOUT PRIOR WRITTEN AGREEMENT WITH THE UTILITY PROVIDER AND NOTFICATION TO SERVICE CUSTOMER. PLEASE NOTE THE STORM SEWER IN THIS AREA IS SHALLOW — DO NOT DAMAGE. REPAIR TO UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE AT THE
- 6. THE CONTRACTOR SHALL PRUNE ALL VEGETATION, AS NECESSARY FOR EQUIPMENT CLEARANCES AWAY AND UP FROM THE STREET AND SIDEWALK. AS WELL AS ANY ROOT PRUNING AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING THAT IS TO REMAIN. DAMAGE TO TREES AND SHRUBS IS THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR. COST OF TREE PRUNING IS INCIDENTAL.
- CONTRACTOR SHALL NOTIFY THE CITY OF SHERWOOD 48 HOURS IN ADVANCE OF STARTING CONSTRUCTION AND 24 HOURS BEFORE RESUMING WORK AFTER SHUTDOWNS, EXCEPT FOR NORMAL RESUMPTION OF WORK FOLLOWING SATURDAYS,
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, EXCESS TOPSOIL, AND OTHER EXCESS
  MATERIAL OFF SITE. MATERIALS SHALL BE DISPOSED OF IN SUCH A MANNER AS TO MEET ALL APPLICABLE REGULATIONS.
  DISPOSAL LOCATION INFORMATION TO BE PROVIDED TO CITY PRIOR TO BEGINNING ANY WORK.
- CONSTRUCTION VEHICLES SHALL PARK AT A LOCATION APPROVED BY THE INSPECTOR OR AT A LOCATION(S) INDICATED ON AN APPROVED PLAN. HOURS OF CONSTRUCTION SHALL BE AS SPECIFIED IN THE PLANS — UNLESS OTHERWISE APPROVED BY THE INSPECTOR. CONSTRUCTION IS PROHIBITED ON SATURDAY AND SUNDAY UNLESS WRITTEN APPROVAL IS OBTAINED FROM THE PUBLIC WORKS DIRECTOR AND CITY MANAGER. CONSTRUCTION ACTIVITIES INCLUDE ALL FIELD MAINTENANCE OF EQUIPMENT, REFUELING, AND PICK UP AND DELIVERY OF EQUIPMENT AS WELL AS THE ACTUAL CONSTRUCTION ACTIVITY.
- 10. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN UP THE PROJECT AREA AND LEAVE IT IN A NEAT AND SECURED MANNER. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED
- 11. THE CONTRACTOR SHALL KEEP AN APPROVED SET OF PLANS ON THE PROJECT SITE AT ALL TIMES WITH ALL KNOWN CHANGES REDLINED ON THE WORKING PLAN SET FOR THE SOLE PURPOSE OF RECORDING AS BUILT INFORMATION OF IMPROVEMENTS AND LOCATION AND DEPTH OF ANY EXISTING UTILITIES ENCOUNTERED.
- 12. ANY ALTERATIONS OR VARIATIONS FROM THESE PLANS EXCEPT MINOR FIELD ADJUSTMENTS NEEDED TO MEET EXISTING FIELD CONDITIONS, SHALL BE APPROVED BY THE ENGINEER AND APPLICABLE REGULATORY AGENCY REPRESENTATIVE. CONTRACTOR SHALL KEEP RECORD OF ALL CHANGES AND NOTE ON AS BUILT PLANS, CONTRACTOR SHALL SUBMIT ACCURATE AND COMPLETE "AS-BUILT" INFORMATION TO THE ENGINEER UPON COMPLETION OF CONSTRUCTION.
- 13. THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THIS PROJECT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET THE INTENT OF THE PROJECT CONTRACT DOCUMENTS, APPLICABLE AGENCY REQUIREMENTS AND OTHER WORK AS NECESSARY TO PROVIDE A COMPLETE
- 14. CONTRACTOR SHALL PROVIDE EFFECTIVE EROSION PROTECTION TO INCLUDE, BUT NOT BE LIMITED TO, GRADING, DITCHING, STRAW WAITLES, TRIANGULAR SILT DAMS, SILT FENCING, AND SEDIMENT BARRIERS TO MINIMIZE EROSION AND IMPACT TO ADJACENT PROPERTY. SEE EROSION AND SEDIMENT CONTROL NOTES AND PLANS.
- 15. THE CONTRACTOR SHALL MAINTAIN AND COORDINATE ACCESS TO ALL AFFECTED PROPERTIES. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS A MINIMUM OF 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURES AND ASSIST PROPERTY OWNER WITH ALTERNATIVE PARKING AND ACCESS. TO ENDESTRIAN ACCESS TO ENTRANCES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL PROVIDE RAMPS OR OTHER APPROVED METHODS FOR MAINTAINING ACCESS. TO ENTRANCES. RAMPS SHALL BE ADA COMPLIANT, APPROVED BY THE ENGINEER, AND INCIDENTAL TO THE CONTRACT. RESIDENTIAL DRIVEWAYS MAY ONLY BE CLOSED FOR A MAXIMUM OF ONE DAY (7:00 AM TO 6:00 PM).
- 16. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND VERIFY ALL EXISTING CONDITIONS BEFORE THE START OF WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY FIELD MEASUREMENTS AND OTHERWISE VERIFY ALL DIMENSIONS AND EXISTING CONSTRUCTION CONDITIONS INDICATED AND/OR SHOWN ON THE PLANS. SHOULD ANY ERROR OR INCONSISTENCY EXIST. THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK AFFECTED UNTIL REPORTED TO THE PROJECT ENGINEER
- 17. ANY INSPECTION BY THE CITY, COUNTY, STATE, FEDERAL AGENCY OR PROJECT ENGINEER SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN COMPLIANCE WITH THE APPLICABLE CODES, REGULATIONS, CITY STANDARDS AND PROJECT CONTRACT DOCUMENTS.
- 18. CONTRACTOR TO VERIFY CENTERLINE AND GUTTER SLOPES PRIOR TO CONSTRUCTION TO ENSURE COMPLIANCE WITH THE CONSTRUCTION DRAWINGS AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- 19. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN CONFORMING TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION AND ACCORDING TO PROJECT SPECIFICATIONS. TRAFFIC CONTROL PLAN SHALL MATCH APPROVED PHASING/SEQUENCING PLAN AND SHALL BE SUBJECT TO THE APPROVAL BY THE CITY, WASHINGTON COUNTY AND/OR ODOT AS APPLICABLE.
- 20. THE CONTRACTOR SHALL PROVIDE TO THE CITY PROJECT MANAGER AND INSPECTOR A 24 HOUR CONTACT PERSON AND
- 21. NO TRENCHES OR PITS WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT, ALL TRENCHES AND PITS SHALL BE COVERED WITH STEEL PLATES OR FILLED IN AT NIGHT.
- 22. PROPERTY LINES SHOWN ON ALL PLAN SHEETS ARE FOR GENERAL DELINEATION ONLY AND ARE, BY NO MEANS, MEANT TO

- 23. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL CONSTRUCTION SAFETY, HEALTH AND OTHER RULES AND REGULATIONS FROM OSHA, DEQ. STATE, AND LOCAL REGULATING AGENCIES FOR SAFETY AND INSTALLATION THE WORK INCLUDING BUT NOT LIMITED TO SHORING, BRACING, ERECTION/INSTALLATION, FALL PROTECTION,
- 24. IT IS THE CONTRACTOR'S RESPONSIBILITY TO REMOVE EXISTING AGGREGATE BASE AS NECESSARY TO CEMENT TREAT BASE AND ACHIEVE SURFACE BASE LEVEL FOR PLACEMENT OF NEW HMAC WHILE MEETING THE CURB AND DRIVEWAY
- 25. WHEN MATCHING EXISTING PAVEMENT, SAWCUT TO FIRM PAVEMENT. MINIMUM SAWCUT IS 1' FROM EXISTING EDGE OF PAVEMENT OR AS SHOWN ON PLANS. SAWCUT LOCATION TO BE APPROVED BY INSPECTOR.
- 26. CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING STRUCTURES AND UTILITIES NOT SHOWN TO BE REMOVED. CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING STRUCTURES (SIDEWALKS, DRIVEWAYS, CURB, FENCE, STREET TREES, ETC.) DAMAGED DURING CONSTRUCTION, IN ACCORDANCE WITH CITY STANDARDS.
- THEREFORE NOT GUARANTEED FOR ACCURACY AND SHOULD NOT BE USED FOR OTHER PURPOSES.

#### **EROSION CONTROL NOTES**

- THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION/LANDSCAPING IS ESTABLISHED.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED AFTER PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS RE-ESTABLISHED
- IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH MAY 31ST, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL (CHAPTER 4) OF CLEAN WATER SERVICES FOR REQUIREMENTS.
- 9. THE CONTRACTOR SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
- 10. ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (FILTER BAG, DIRT BAG, ETC.)
- 11. SITE EROSION CONTROL PLAN AND BMP'S MEETING CWS STANDARDS TO BE IN PLACE AND APPROVED PRIOR TO
- 12. TRACKING OF MATERIALS OUTSIDE OF THE WORK ZONE, ONTO ADJACENT STREETS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE AND CLEAN TO THE SATISFACTION OF THE INSPECTOR. THIS INCLUDES BUT IS NOT LIMITED TO SOIL, AGGREGATE, ASPHALT DEBRIS, ETC.
- 13. CONTRACTOR TO PROVIDE FOR CONCRETE WASHOUT. SUBMIT PLAN FOR CONCRETE WASHOUT TO INSPECTOR FOR

## WORK HOURS/TRAFFIC CONTROL NOTES

- 1. WORK HOURS ARE FROM 8:00AM TO 6:00PM MONDAY THROUGH FRIDAY.
- 2. SW MAIN STREET, SW LADD HILL ROAD AND SW GALBREATH DRIVE SHALL HAVE TWO LANES OPEN TO TRAFFIC AT ALL TIMES UNLESS FLAGGERS ARE PROVIDED FOR A SINGLE LANE CLOSURE. LANE CLOSURE MAY ONLY OCCUR BETWEEN THE HOURS OF 8:30AM TO 4:30PM WITH THE EXCEPTION OF SW LADD HILL ROAD CLOSURE FOR THE CEMENT TREATMENT OF THE WEST LANE OF SW LADD HILL ROAD — SEE SHEET 7
- 3. THE DRIVEWAY AT 14270 SW GALBREATH ROAD SHALL HAVE HALF OF THE DRIVEWAY OPEN AT ALL TIMES (COMMERCIAL
- THE INTERSECTION OF SW LADD HILL ROAD AND SW BROOKMAN ROAD SHALL REMAIN OPERATIONAL DURING CONSTRUCTION (USE FLAGGERS AS NECESSARY).
- TRENCHES WITHIN EXISTING ASPHALT PAVING SHALL HAVE EITHER ASPHALT COLD PATCH OR ASPHALT HOT PATCH (1\* MIN. THICKNESS) AT THE END OF EACH WORK DAY PRIOR TO REOPENING THE STREET, PLATING IS ACCEPTABLE WITH THE APPROVAL OF THE INSPECTOR, COST IS INCIDENTAL
- 6. GAPS BETWEEN ADA SIDEWALK RAMPS AND EXISTING ASPHALT SHALL BE TEMPORARY COLD PATCHED (INCIDENTAL).
- 7. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EMERGENCY VEHICLE ACCESS TO ALL PROPERTIES AT ALL TIMES.
- 8. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS FOR MAIL AND TRASH (FRIDAY) AT ALL TIMES.
- 9. TRAFFIC CONTROL SHALL INCLUDE ALL PAVEMENT MARKING REMOVAL TEMPORARY PAVEMENT MARKING RARRICADES. DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAYEMENT MARKINGS AND ALL INCIDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".
- CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION OF A TRAFFIC CONTROL PLAN AND ITS CONTINUED FUNCTIONING FOR THE PROTECTION OF CONSTRUCTION WORKERS, VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIANS. ALL TRAFFIC CONTROL DEVICES/SIGNAGE SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. APPROVAL OF THE TRAFFIC CONTROL PLAN BY THE CITY OF SHERWOOD DOES NOT NEGATE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN A SAFE WORK ZONE. THE CITY OF SHERWOOD BEARS NO LIABILITY FOR THE CONTRACTOR'S IMPLEMENTATION OF THE TRAFFIC

#### STREET NOTES

- 1. IN AREAS WHERE CURB AND GUTTER EXISTS, NEW ASPHALT WILL MATCH FRONT FDGE OF CONCRETE GUTTER.
- TACK COAT SHALL BE APPLIED AGAINST CONCRETE GUTTER EDGE, CURB FACE, CONCRETE STRUCTURES, EXISTING ASPHALT EDGE, MANHOLE FRAMES, BLOW-OFF FRAMES, CLEANOUT FRAMES AND OTHER SURFACES OR STRUCTURES THAT
- 3. HOT RUBBERIZED ASPHALT SEALER REQUIRED AT ASPHALT JOINTS (INCIDENTAL). DO NOT OVERCUT ASPHALT. CITY HAS THE RIGHT TO HAVE OVERCUT ASPHALT REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
- 4. PAVEMENT CROSS SLOPES ON SW GALBREATH SHALL BE AT LEAST 2.5%.
- NEW ASPHALT SHALL MATCH EDGE OF CONCRETE CATCH BASINS. THERE WILL BE NO ADJUSTING OF CATCH BASIN GRATES ALLOWED.
- NEW ASPHALT SHALL NOT COVER ANY WEEP HOLES (AT I.E. OR LOWER). CITY WILL PHOTO DOCUMENT PROJECT AREA INCLUDING EXISTING CURBS. COVERED WEEPHOLES SHALL BE CLEARED AT CONTRACTORS EXPENSE AND STREET
- 7. TYPICAL FULL CURB EXPOSURE IS 6" AT TOP FACE OF CURB UNLESS OTHERWISE NOTED.
- 8. TYPICAL DRIVEWAY CURB EXPOSURE IS 3/4" AT TOP FACE OF CURB.
- 9. ADJUST ALL EXISTING VALVES, BLOW-OFFS AND OTHER MISCELLANEOUS BOXES AS NECESSARY TO MATCH NEW ASPHALT SURFACE GRADE - TO BE PAID FOR UNDER THE BID ITEM OF "ADJUSTING BOXES".
- 10. MANHOLES THAT REQUIRE REMOVAL OF LOWER LEVEL OF ASPHALT TO ADD CONCRETE RINGS OR OTHERWISE ADJUST THE FRAME WILL BE PAID FOR UNDER THE BID ITEM "MINOR ADJUSTMENT OF MANHOLES". THERE WILL BE NO PAYMENT FOR MANHOLE FRAMES THAT ONLY NEED TO HAVE THE ASPHALT GRINDED AROUND AND NEW ASPHALT OVERLAY MATCHING THE FRAME.
- 11. CONTRACTOR TO TEST ASPHALT SURFACE WITH 12-FOOT STRAIGHT EDGE IN TRAVEL LANES PARALLEL TO AND PERPENDICULAR TO THE CENTER LINE, AS DIRECTED BY THE INSPECTOR. THE ASPHALT SURFACE SHALL NOT VARY BY MORE THAN 1/4 INCH.
- 12. EQUIPMENT PARKED ON SITE SHALL BE PARKED AWAY FROM AN INTERSECTION AT A LOCATION APPROVED BY THE INSPECTOR.
- 13. WAITER FOR CONSTRUCTION MAY BE OBTAINED FROM CITY FIRE HYDRANTS. INSTALL FIRE HYDRANT METER PRIOR TO DRAWING WAITER. FIRE HYDRANT METER TO BE OBTAINED FROM CITY OF SHERWOOD WAITER DEPARTMENT. COORDINATE WITH INSPECTOR. WAITER FOR CONSTRUCTION IS PAID FOR BY THE CONTRACTOR AND CONSTRUCTIOR DAY.
- 14. REPAIR ALL DISTURBED NEIGHBORING PROPERTY BACK TO ORIGINAL OR BETTER CONDITION
- 15. ANY SEGMENT OF STREET OPEN TO TRAFFIC THAT HAS A VERTICAL DROP (INCLUDING DRIVEWAYS) SHALL HAVE A
- 16. ANY EXISTING SURFACE TO REMAIN (CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, ETC.) THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED AND THE CONTRACTOR'S EXPENSE
- 17. TRAFFIC CONTROL SHALL INCLUDE BUT IS NOT LIMITED TO PAVEMENT MARKING REMOVAL, TEMPORARY PAVEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAVEMENT MARKINGS AND ALL INCEDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE". IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN A SAFE WORKING AREA FOR WORKERS. VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIAN TRAFFIC
- CONTRACTOR TO CLEAN UP JAGGED CONCRETE EDGES MEETING THE APPROVAL OF THE INSPECTOR. SOME SAWCUTTING MAY BE REQUIRED. COST IS INCIDENTAL.
- ANY HOLES IN THE EXISTING ASPHALT BELOW THE SURFACE LEVEL OF THE GRIND SURFACE SHALL BE FILLED WITH ASPHALT AND COMPACTED PRIOR TO OVERLAY (INCIDENTAL).
- 20. CONTRACTOR TO CLEAN AND TACK COAT GRINDED ASPHALT SURFACE PRIOR TO OVERLAY (INCIDENTAL).
- 21. CONTRACTOR TO SHAPE EXISTING GRAVEL SHOULDERS TO MATCH NEW ASPHALT EDGE (INCIDENTAL).
- 22. SCALPING OF VEGETATION/SOILS FROM SURFACE OF ASPHALT IS INCIDENTAL

#### RAILROAD NOTES

- CONTRACTOR TO OBTAIN PERMIT FROM RAILROAD PRIOR TO WORKING WITHIN RAILROAD RIGHT-OF-WAY. CITY HAS MADE INITIAL CONTACT AND PERMIT SUBMITTAL AND HAS PAID PERMIT FEE.
- 2. CONTRACTOR TO PERFORM WORK IN COMPLIANCE WITH THE RAILROAD PERMIT AND IS RESPONSIBLE TO MEET RAILROAD'S INSURANCE REQUIREMENTS (INCIDENTAL).
- 3. CONTRACTOR SHALL PROVIDE FOR RAILROAD INSPECTOR/FLAGGER IF REQUIRED BY THE RAILROAD.

#### LEGEND

= EXISTING CURB AND GUTTER

= FXISTING/FINAL PAVEMENT MARKING

= EXISTING WATER VALVE

--- W--- = EXISTING WATER LINE

--- ST-- = EXISTING STORM LINE

-SS- = EXISTING STORM LINE - G - EXISTING GAS LINE

-T- = EXISTING TELECOMMUNICATION LINE

= EXISTING MANHOLE

= FXISTING CATCH BASIN

S/W = SIDEWALK

9

**PROJECT** 

OVERLAY

**PAVEMENT** 

2018

SHERWOOD G DEPARTMENT I PINE STREET OREGON 97140 P CTZ

HEET NO

or 10

нет 3

or 10

ESC PLAN FOR SITES 1 TO 5 ACRES

DEVELOPER

DEVELOPER/COMPANY: CITY OF SHERWOOD CONTACT: CRAIG CHRISTENSEN P ADDRESS SHERWOOD, OR 97140 FAX: 503-625-0629

#### PLANNING / ENGINEERING / SURVEYING FIRM

DEVELOPER/COMPANY: CITY OF SHERWOOD CONTACT: CRAIG CHRISTENSEN, PE ADDRESS 22560 SW PINE STREET ADDRESS SHERWOOD, OR 97140 PHONE: 503-925-2301 FAX: 503-625-0629

#### NARRATIVE DESCRIPTIONS

EXISTING SITE CONDITIONS

\* PAVED STREETS

**DEVELOPED CONDITIONS** 

\* PAVED STREETS

NATURE OF CONSTRUCTION ACTIVITY AND ESTIMATED

- \* CLEARING (N/A)
- \* MASS GRADING (N/A)
- \* UTILITY INSTALLATION (N/A)
- \* STREET CONSTRUCTION (AUGUST SEPTEMBER, 2018)
- \* FINAL STABILIZATION (SEPTEMBER 2018)

TOTAL SITE AREA =  $\pm 112,518$  SF =  $\pm 2.58$  ACRES

TOTAL DISTURBED AREA =  $\pm 112,518$  SF =  $\pm 2.58$ 

SITE SOIL CLASSIFICATION:

GENERALLY HILLSBORO AND QUATAMA

ON-SITE SOILS HAVE A MODERATE TO HIGH EROSION POTENTIAL.

RECEIVING WATER BODIES:

NEAREST WATER BODY: MAIN ST & LADD HILL RD - CEDAR CREEK

#### STANDARD EROSION AND SEDIMENT **CONTROL PLAN DRAWING NOTES:**

- All permit registrants must implement the ESCP. Failure to implement any of the control measures or practices described in the ESCP is a violation of the permit.

  The ESCP measures shown on this plan are minimum requirements for anticipated site conditions. During the construction
- period, upgrade these measures as needed to comply with all applicable local, state, and federal erosion and sediment
- control regulations. Submission of all ESCP revisions is not required. Submittal of the ESCP revisions is only under specific conditions. Submit all necessary revision to DEQ or Agent.

  4. Phase clearing and grading to the maximum extent practical to prevent exposed inactive areas from becoming a source of
- 5. Identify, mark, and protect (by fencing off or other means) critical riparian greas and vegetation including important trees ided rooting zones, and vegetation areas to be preserved. Identify vegetative buffer zones between the site and
- sensitive areas (e.g., wellands), and other areas to be preserved, especially in perimeter areas.

  Preserve existing vegetation when practical and re-vegetate open areas. Re-vegetate open areas when practicable before and after grading or construction. Identify the type of vegetative seed mix used.
- Trosion and sediment control measures including perimeter sediment control must be in place before vegetation is disturbed and must remain in place and be mointained, repaired, and promptly implemented following procedures established for the duration of construction, including protection for active storm drain inlets and catch basins and appropriate non-stormwater pollution controls.
- position controls concrete truck and other concrete equipment washout areas before beginning concrete work. Direct all wash water into a pit or leak-proof container. Handle wash water as waste, concrete discharge to waters of the state is prohibited.
- Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses and for all roadways including gravel roadways.
- Establish material and waste storage areas, and other non-stormwater controls.
- 11. Prevent tracking of sediment onto public or private roads using BMPs such as: graveled (or paved) exits and parking areas, gravel all unpaved roads located onsite, or use an exit tire wash. These BMPs must be in place prior to land-disturbing
- decivities.

  When trucking saturated soils from the site, either use water-light trucks or drain loads on site.

  Use BMPs to prevent or minimize stormwater exposure to pollutants from spills; vehicle and equipment fueling, maintenance, and storage; other cleaning and maintenance activities; and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, leftover paints, solvents, and glues from cons
- Implement the following BMPs when applicable: written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures, spill kits in all vehicles, regular maintenance schedule for vehicles and mac material delivery and storage controls, training and signage, and covered storage areas for waste and supplies.

  Use water, soil—binding agent or other dust control technique as needed to avoid wind—blown soil.
- 16. The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient releases to surface waters. Exercise coution when using time-release fertilizers within any waterway riparian zone.
- If a storrmwater treatment system (for example, electro-coagulation, floculation, filtration, etc.) for sediment or other
  pollutant removal is employed, submit an operation and maintenance plan (including system schematic, location of system, location of inlet, location of discharge, discharge dispersion device design, and a sampling plan and frequency) before operating the treatment system. Obtain plan approval before operating the treatment system. Operate and maintain the
- reatment system according to manufacturer's specifications. It the end of each workday soil stockpiles must be stabilized or covered, or other BMPs must be implemented to prevent discharges to surface waters or conveyance systems leading to surface waters.
- 19. Construction activities must avoid or minimize excavation and creation of bare ground during wet weather October 01 May
- Sediment fence: remove trapped sediment before it reaches one third of the above ground fence height and before fence
- 21. Other sediment barriers (such as biobags): remove sediment before it reaches two inches depth above ground height, and
- Catch basins: clean before retention capacity has been reduced by fifty percent. Sediment basins and sediment traps: remove trapped sediments before design capacity has been reduced by fifty percent and at completion of project.
   Within 24 hours, significant sediment that has left the construction site, must be remediated. Investigate the cause of the sediment release and implement steps to prevent a recurrence of the discharge within the same 24 hours. Any in-stream clean up of sediment shall be performed according to the Oregon Division of State Lands required timeframe.
- ciean up of sediment shall be performed according to the Oregon Division of State Lands required timetrame.

  The intentional washing of sediment into storm sewers or drainage ways must not occur. Vacuuming or dry sweeping and material pickup must be used to cleanup released sediments.

  Provide permanent erosion control measures on all exposed areas. Do not remove temporary sediment control practices until permanent vegetation or other cover of exposed areas is established. However, do remove all temporary erosion control measures as exposed areas become stabilized, unless doing so conflicts with local requirements. Properly dispose of construction materials and waste, including sediment retained by temporary BMPs.

  If vegetative seed mixes are specified, seeding must take place no later that September 1; the type and percentages of seed in the mix must be identified on the nations.
- in the mix must be identified on the plans.
- 27. All pumping of sediment laden water shall be discharged over an undisturbed, preferably vegetated area, and through a
- All pumping of sediment loden water shall be discharged over an undisturbed, preferably vegetated area, and through a sediment control BMP i.e. (filter bag).
   All exposed soils must be covered during the wet weather period, October 01 May 31.
   If water of the state is within the project site or within 50 feet of the project boundary, maintain the existing natural buffer within the 50-foot zone for the duration of the permit coverage, or maintain less than the entire existing natural buffer and provide additional erosion and sediment control BMPs.

#### INSPECTION FREQUENCY:

mier de l'est i mage di le li	
SITE CONDITION	MINIMUM FREQUENCY
ACTIVE PERIOD	WEEKLY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOW MELT, IS OCCURRING.  AT LEAST ONCE EVERY MONTH, REGARDLESS OF WHETHER STORMWATER RUNOFF IS OCCURRING.
PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY.	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MANTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
INACTIVE PERIODS GREATER THAN FOURTEEN     (14) CONSECUTIVE CALENDAR DAYS.	ONCE EVERY MONTH.
PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.
5. PERIODS DURING WHICH DISCHARGE IS UNLIKELY DUE TO FROZEN CONDITIONS.	MONTHLY. RESUME MONITORING IMMEDIATELY UPON MELT, OR WHEN WEATHER CONDITIONS MAKE DISCHARGES LIKELY.

HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO

DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS.

ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200—CN PERMIT REQUIREMENTS.

INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEG'S 1200-CN PERMIT REQUIREMENTS.
RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. DURING INACTIVE PERIODS OF GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR

THE PERMITTEE IS REQUIRED TO MEET ALL THE CONDITIONS OF THE 1200-CN PERMIT. THIS ESCP AND GENERAL CONDITIONS HAVE BEEN DEVELOPED TO FACILITATE COMPLIANCE WITH THE 1200-CN PERMIT REQUIREMENTS. IN CASES OF DISCREPANCIES OR OMISSIONS, THE 1200-CN PERMIT REQUIREMENTS SUPERCEDE REQUIREMENTS OF THIS PLAN.

#### BMP MATRIX FOR CONSTRUCTION PHASES

#### REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'8.

		MASS	UILLIT	SIREEI	FINAL	WEI WEATHER
	CLEARING	GRADING	INSTALLATION	CONSTRUCTION	STABILIZATION	(OCT. 1 - MAY 31ST)
EROSION PREVENTION						
PRESERVE NATURAL VEGETATION	T					
GROUND COVER					X	
HYDRAULIC APPLICATIONS						
PLASTIC SHEETING						
MATTING						
DUST CONTROL	T			X		
TEMPORARY/ PERMANENT SEEDING					Х Х	
BUFFER ZONE						
OTHER:						
SEDIMENT CONTROL						
SEDIMENT FENCE (PERIMETER)	T					
SEDIMENT FENCE (INTERIOR)	_					
STRAW WATTLES						
FILTER BERM						
INLET PROTECTION **				X	X	
DEWATERING						
SEDIMENT TRAP						
NATURAL BUFFER ENCROACHMENT						
OTHER:						
RUN OFF CONTROL						
CONSTRUCTION ENTRANCE	T					
PIPE SLOPE DRAIN						
OUTLET PROTECTION						
SURFACE ROUGHENING						
CHECK DAMS **						
OTHER:	T					
POLLUTION PREVENTION						
PROPER SIGNAGE	T			X	Х	
HAZ WASTE MGMT				X	X	
SPILL KIT ON-SITE				X	X	
CONCRETE WASHOUT AREA				X	X	
THER:						

SIGNIFIES ADDITIONAL BMP'S REQUIRED FOR WORK WITHIN 50' OF WATER OF THE STATE

## RATIONALE STATEMENT

A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S GUIDANCE MANUAL HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMP's WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOIL CONDITIONS TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS, AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESC PLAN, AN ACTION PLAN WILL BE SUBMITTED.

PERMITTEE'S SITE INSPECTOR: ANDY STIRLING

COMPANY/AGENCY: CITY OF SHERWOOD PHONE: (503) 925-2307 FAX: (503) 625-0629 E-MAIL: STIRLINGA@SHERWOODOREGON.GOV DESCRIPTION OF EXPERIENCE: CITY EROSION CONTROL INSPECTOR

#### SHEET INDEX

#### **EROSION AND SEDIMENT CONTROL PLANS**

EROSION AND SEDIMENT CONTROL COVER SHEET

EROSION AND SEDIMENT CONTROL NOTES AND DETAILS

SEE PLAN VIEW SHEET FOR EROSION AND SEDIMENT CONTROL PLAN

# VICINITY MAP NOT TO SCALE

SEE SHEETS 6-8

NOT TO SCALE

SITE MAP

#### PROJECT LOCATION:

SW SUNSET BOULEVARD BETWEEN SW RICHEN PARK TERRACE AND SW EUCALYPTUS TERRACE AND SW GALBREATH DRIVE BETWEEN SW GERDA LANE AND BONNEVILLE POWER LINES, WASHINGTON COUNTY, OREGON MAIN ST. LATITUDE = 45.352244. LONGITUDE = -122.8414450 |ADD|HIII|RD, |ATITUDE| = 45.347911 | |ONGITUDE| = -122.842796GALBREATH DR. LATITUDE = 45.368547, LONGITUDE = -122.924156

## PROPERTY DESCRIPTION:

RANGE 1 WEST, WILLAMETTE MERIDIAN, WASHINGTON

#### ATTENTION EXCAVATORS:

OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING 503-232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER, YOU MUST NOTIFY THE CENTER AT LEAST TWO BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL 503-246-6699

DAYS, RETAIN THE ESCP AT THE CONSTRUCTION SITE OR AT ANOTHER LOCATION

# EROSION CONTROL I AND DETAILS

# PRE-CONSTRUCTION, CLEARING, AND DEMOLITION NOTES:

- 1. ALL BASE ESC MEASURES (INLET PROTECTION, PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
- 2. SEDIMENT BARRIERS APPROVED FOR USE INCLUDE SEDIMENT FENCE, BERMS CONSTRUCTED OUT OF MULCH, CHIPPINGS, OR OTHER SUITABLE MATERIAL, STRAW WATTLES, OR OTHER APPROVED MATERIALS.
- 3. SENSITIVE RESOURCES INCLUDING, BUT NOT LIMITED TO, TREES, WETLANDS, AND RIPARIAN PROTECTION AREAS SHALL BE CLEARLY DELINEATED WITH ORANGE CONSTRUCTION FENCING OR CHAIN LINK FENCING IN A MANNER THAT IS CLEARLY VISIBLE TO ANYONE IN THE AREA. NO ACTIVITIES ARE PERMITTED TO OCCUR BEYOND THE CONSTRUCTION BARRIER.
- 4. CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, STREET SWEEPING, AND VACUUMING, MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- 5. RUN-ON AND RUN-OFF CONTROLS SHALL BE IN PLACE AND FUNCTIONING PRIOR TO BEGINNING SUBSTANTIAL CONSTRUCTION ACTIVITIES. RUN-ON AND RUN-OFF CONTROL MEASURES INCLUDE: SLOPE DRAINS (WITH OUTLET PROTECTION), CHECK DAMS, AND SURFACE ROUGHENING.

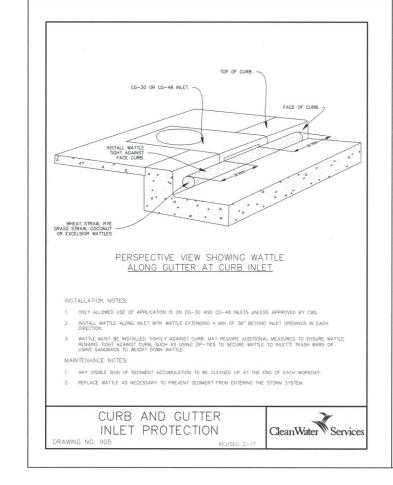
THESE EROSION AND SEDIMENT CONTROL PLANS ASSUME "DRY WEATHER" CONSTRUCTION. "WET WEATHER" CONSTRUCTION MEASURES NEED TO BE APPLIED BETWEEN OCTOBER 1ST AND MAY 31ST.

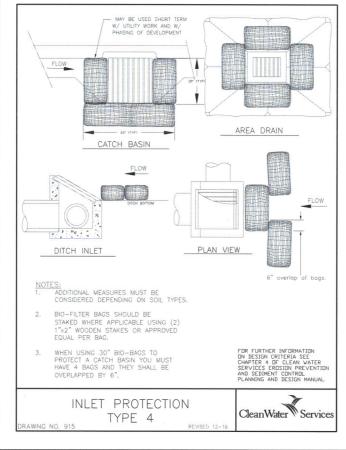
#### GRADING, STREET AND UTILITY EROSION AND SEDIMENT CONSTRUCTION NOTES:

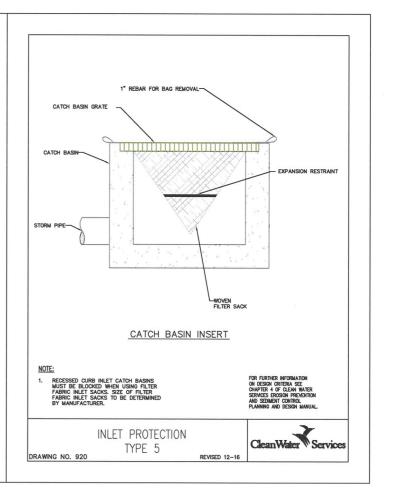
- 1. SEED USED FOR TEMPORARY OR PERMANENT SEEDING SHALL BE COMPOSED OF ONE OF THE FOLLOWING MIXTURES, UNLESS OTHERWISE AUTHORIZED:
  - A. VEGETATED CORRIDOR AREAS REQUIRE NATIVE SEED MIXES. SEE RESTORATION PLAN FOR APPROPRIATE SEED MIX.
  - B. DWARF GRASS MIX (MIN. 100 LB./AC.)
    - 1. DWARF PERENNIAL RYEGRASS (80% BY WEIGHT)
    - 2. CREEPING RED FESCUE (20% BY WEIGHT)
  - C. STANDARD HEIGHT GRASS MIX (MIN. 100LB./AC.)
    - 1. ANNUAL RYEGRASS (40% BY WEIGHT)
    - 2. TURF-TYPE FESCUE (60% BY WEIGHT)
- 2. SLOPE TO RECEIVE TEMPORARY OR PERMANENT SEEDING SHALL HAVE THE SURFACE ROUGHENED BY MEANS OF TRACK-WALKING OR THE USE OF OTHER APPROVED IMPLEMENTS. SURFACE ROUGHENING IMPROVES SEED BEDDING AND REDUCES RUN-OFF VELOCITY.

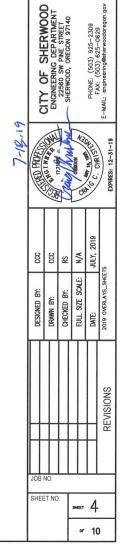
- 3. LONG TERM SLOPE STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA SEEDING WITH APPROVED MIX AND APPLICATION RATE.
- 4. TEMPORARY SLOPE STABILIZATION MEASURES SHALL INCLUDE: COVERING EXPOSED SOIL WITH PLASTIC SHEETING, STRAW MULCHING, WOOD CHIPS, OR OTHER APPROVED MEASURES
- 5. STOCKPILED SOIL OR STRIPPINGS SHALL BE PLACED IN A STABLE LOCATION AND CONFIGURATION. DURING "WET WEATHER" PERIODS. STOCKPILES SHALL BE COVERED WITH PLASTIC SHEETING OR STRAW MUICH. SEDIMENT FENCE IS REQUIRED AROUND THE PERIMETER OF THE STOCKPILE.
- 6. EXPOSED CUT OR FILL AREAS SHALL BE STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS OR MATS, MID-SLOPE SEDIMENT FENCES OR WATTLES, OR OTHER APPROPRIATE MEASURES. SLOPES EXCEEDING 25% MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES.
- 7. AREAS SUBJECT TO WIND EROSION SHALL USE APPROPRIATE DUST CONTROL MEASURES INCLUDING THE APPLICATION OF A FINE SPRAY OF WATER, PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED
- 8. CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, TIRE WASHES. STREET SWEEPING, AND VACUUMING MAY BE BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

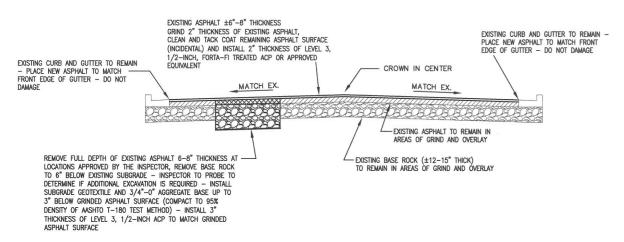
- 9. ACTIVE INLETS TO STORM WATER SYSTEMS SHALL BE PROTECTED THROUGH THE USE OF APPROVED INLET PROTECTION MEASURES. ALL INLET PROTECTION MEASURES ARE TO BE REGULARLY INSPECTED AND MAINTAINED AS NEEDED.
- 10. SATURATED MATERIALS THAT ARE HAULED OFF-SITE MUST BE TRANSPORTED IN WATER-TIGHT TRUCKS TO ELIMINATE SPILLAGE OF SEDIMENT AND SEDIMENT-LADEN WATER.
- 11. AN AREA SHALL BE PROVIDED FOR THE WASHING OUT OF CONCRETE TRUCKS IN A LOCATION THAT DOES NOT PROVIDE RUN-OFF THAT CAN ENTER THE STORM WATER SYSTEM OR SURFACE WATERS, IF THE CONCRETE WASH-OUT AREA CAN NOT BE CONSTRUCTED GREATER THAN 50' FROM ANY DISCHARGE POINT, SECONDARY MEASURES SUCH AS BERMS OR TEMPORARY SETTLING PITS MAY BE REQUIRED. THE WASH-OUT SHALL BE LOCATED WITHIN SIX FEET OF TRUCK ACCESS AND BE CLEANED WHEN IT REACHES 50% OF THE CAPACITY.
- 12. SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE SHALL NOT BE TRANSFERRED TO THE STORM WATER SYSTEM. SWEEPINGS SHALL BE PICKED UP AND DISPOSED IN THE TRASH.
- 13. AVOID PAVING IN WET WEATHER WHEN PAVING CHEMICALS CAN RUN-OFF INTO THE STORM WATER SYSTEM.
- 14. USE BMPs SUCH AS CHECK-DAMS, BERMS, AND INLET PROTECTION TO PREVENT RUN-OFF FROM REACHING DISCHARGE POINTS.
- 15. COVER CATCH BASINS. MANHOLES. AND OTHER DISCHARGE POINTS WHEN APPLYING SEAL COAT, TACK COAT, ETC. TO PREVENT INTRODUCING THESE MATERIALS TO THE STORM WATER SYSTEM.





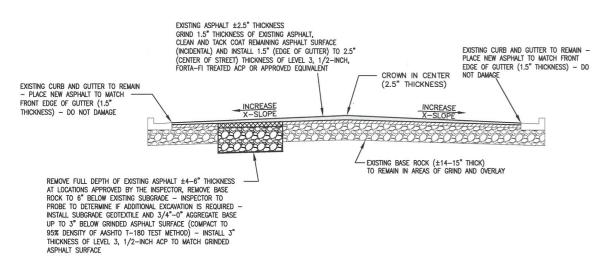






#### TYPICAL STREET OVERLAY MATCH SECTION

(MAIN STREET - NOT TO SCALE)



#### TYPICAL STREET OVERLAY INCREASING CROWN SECTION

(LADD HILL ROAD - NOT TO SCALE)

STREET SECTION NOTES
EXISTING CURB TO REMAIN - DO NOT DAMAGE.

COMPACT SUBGRADE TO 95% DENSITY OF AASHTO T-99 TEST METHOD.

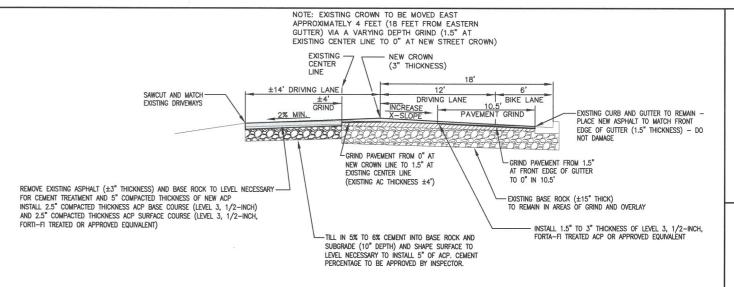
COMPACT ALL BASE ROCK TO 95% DENSITY OF AASHTO T-180 TEST METHOD (INSPECTOR WITNESSED PROOF ROLL REQUIRED).

COMPACT ALL ASPHALT TO 92% OF MMAD PER AASHTO T209.

BASE ROCK WILL BE PROOF ROLLED AFTER CEMENT TREATMENT.

CEMENT TREATMENT TO BE KEPT MOIST UNTIL 1ST LIFT OF ASPHALT IS PLACED. FIRST LIFT OF ASPHALT SHALL BE INSTALLED EITHER THE DAY OF OR THE DAY AFTER CEMENT TREATMENT OF THE BASE. FIRST LIFT OF ASPHALT WILL NEED TO BE INSTALLED THE DAY OF CEMENT TREATMENT FOR SW GALBREATH DRIVE DUE TO HIGH VOLUMES OF TRUCK TRAFFIC.

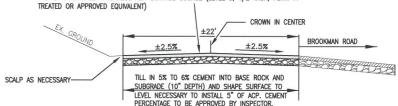
CURB EXPOSURE TO BE ±6" IN STANDARD HEIGHT CURB LOCATIONS. MAY FLUCTUATE AS NECESSARY TO AVOID PONDING AT GUTTER LINE. DO NOT COVER WEEP HOLES. CURB EXPOSURES IN DRIVEWAYS SHALL BE ±3.4" ASPHALT AT THIRROAT OF SIDEWALK RAMPS SHALL BE F. LISH WITH CURB.



#### TYPICAL STREET OVERLAY INCREASING CROWN AND CTB SECTION

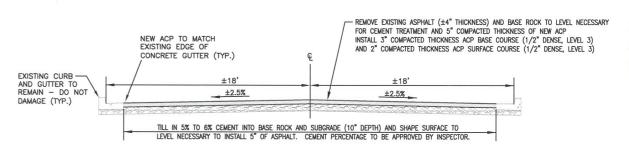
(LADD HILL ROAD LOOKING NORTH - NOT TO SCALE)

REMOVE EXISTING ASPHALT  $(\pm 3^\circ)$  THICKNESS) AND BASE ROCK TO LEVEL NECESSARY FOR CEMENT TREATMENT AND 5 $^\circ$  COMPACTED THICKNESS OF NEW ACP INSTALL 3 $^\circ$  COMPACTED THICKNESS ACP BASE COURSE (LEVEL 3, 1/2-INCH, FORTI-FI AND 2 $^\circ$  COMPACTED THICKNESS ACP SURFACE COURSE (LEVEL 3, 1/2-INCH, FORTI-FI



#### TYPICAL FULL WIDTH CEMENT TREATMENT SECTION

(LADD HILL ROAD - NOT TO SCALE)



#### TYPICAL CEMENT TREATMENT SECTION

(GALBREATH DRIVE - NOT TO SCALE)

| CITY OF STATE | PROPERTY | CCC | COC | C

SECTIONS

**PROJECTS** 

OVERLAY

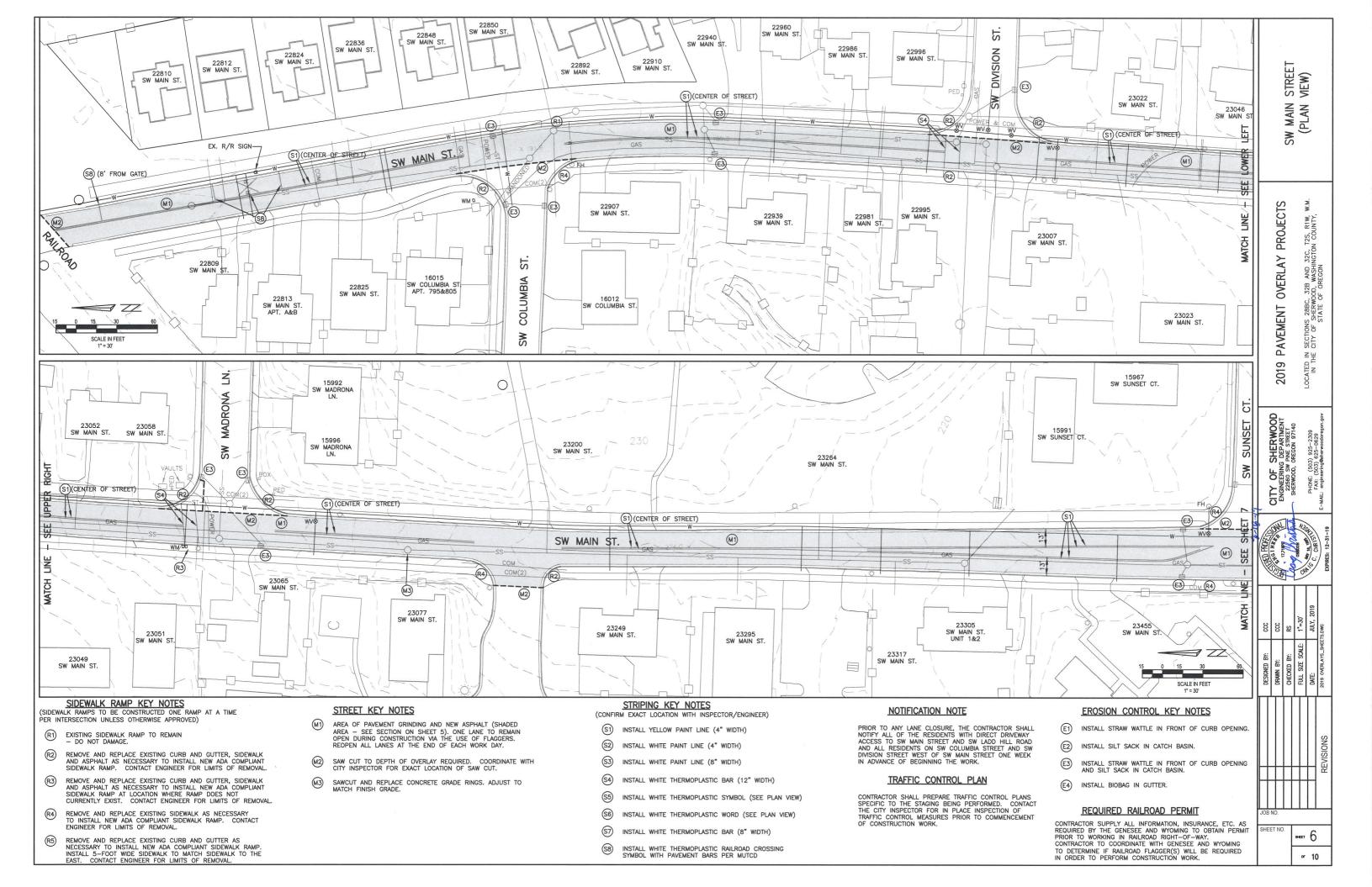
**PAVEMENT** 

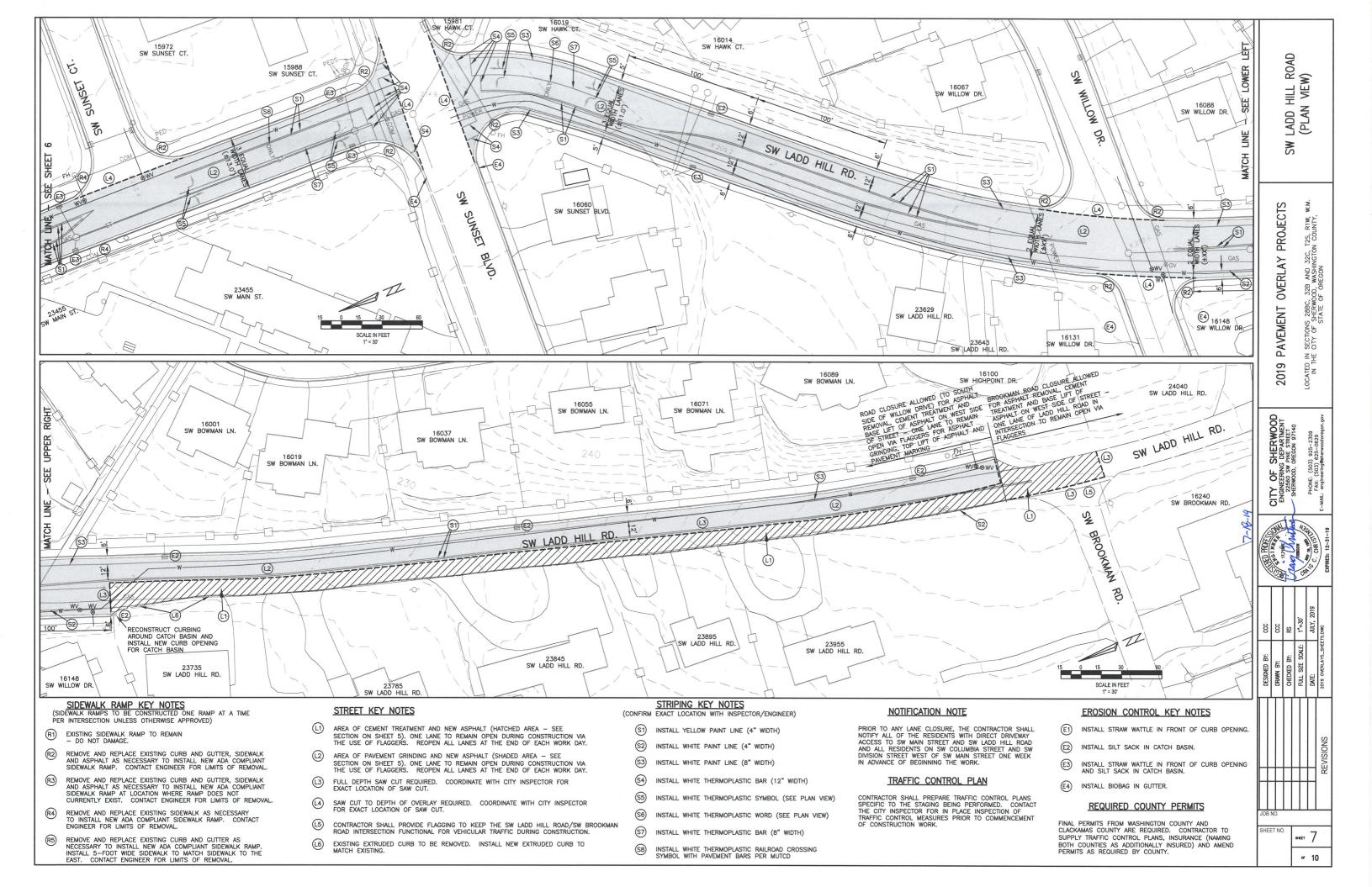
2019

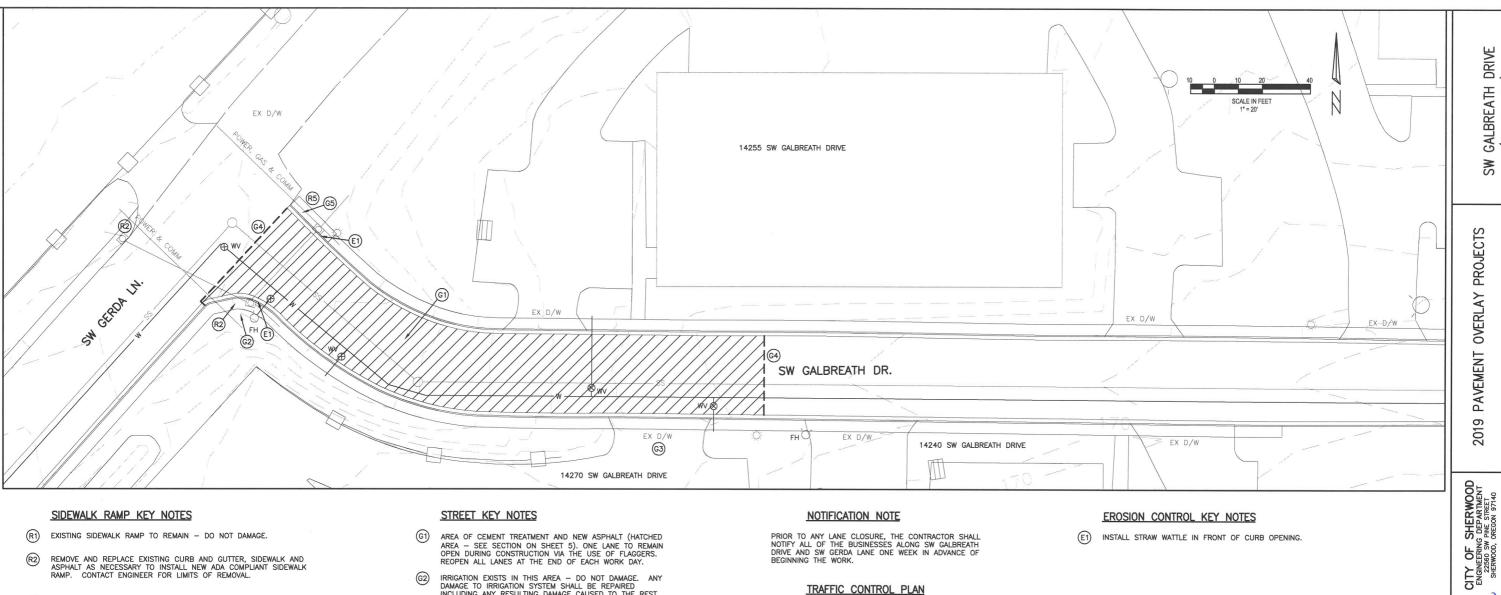
SHERWOOD
IG DEPARTMENT
W PINE STREET
OREGON 97140

SECT

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#### SIDEWALK RAMP KEY NOTES

- R1) EXISTING SIDEWALK RAMP TO REMAIN DO NOT DAMAGE.
- $\mathbb{R}^2$ REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- REMOVE AND REPLACE EXISTING CURB AND GUTTER, SIDEWALK AND ASPHALT AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP AT LOCATION WHERE RAMP DOES NOT CURRENTLY EXIST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- REMOVE AND REPLACE EXISTING SIDEWALK AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. CONTACT ENGINEER FOR LIMITS OF REMOVAL.
- REMOVE AND REPLACE EXISTING CURB AND GUTTER AS NECESSARY TO INSTALL NEW ADA COMPLIANT SIDEWALK RAMP. INSTALL 5-FOOT WIDE SIDEWALK TO MATCH SIDEWALK TO THE EAST. CONTACT ENGINEER FOR LIMITS OF REMOVAL.

#### STREET KEY NOTES

- G1) AREA OF CEMENT TREATMENT AND NEW ASPHALT (HATCHED AREA - SEE SECTION ON SHEET 5). ONE LANE TO REMAIN OPEN DURING CONSTRUCTION VIA THE USE OF FLAGGERS. REOPEN ALL LANES AT THE END OF EACH WORK DAY.
- IRRIGATION EXISTS IN THIS AREA DO NOT DAMAGE. ANY DAMAGE TO IRRIGATION SYSTEM SHALL BE REPAIRED INCLUDING ANY RESULTING DAMAGE CAUSED TO THE REST OF THE SYSTEM DUE TO ANY CONTRACTOR CAUSED BREAK. RESTORE PROPERTY TO EXISTING CONDITION OR BETTER.
- CONTRACTOR SHALL CEMENT TREAT IN A MANNER TO KEEP ONE HALF OF THE DRIVEWAY OPEN AT ALL TIMES (ACTIVE COMMERCIAL BUSINESS). THE DRIVEWAY SHALL BE COMPLETELY OPEN AT THE END OF EACH WORK DAY. CONTACT BUSINESS AT LEAST ONE WEEK IN ADVANCE SO BUSINESS HAS OPPORTUNITY TO REARRANGE DELIVERY
- FULL DEPTH SAW CUT REQUIRED. COORDINATE WITH CITY INSPECTOR FOR EXACT LOCATION OF SAW CUT.
- CONTRACTOR TO RELOCATE STOP SIGN. STOP SIGN TO REMAIN UP AT ALL TIMES DURING CONSTRUCTION.

#### NOTIFICATION NOTE

PRIOR TO ANY LANE CLOSURE, THE CONTRACTOR SHALL NOTIFY ALL OF THE BUSINESSES ALONG SW GALBREATH DRIVE AND SW GERDA LANE ONE WEEK IN ADVANCE OF BEGINNING THE WORK.

#### TRAFFIC CONTROL PLAN

CONTRACTOR SHALL PREPARE TRAFFIC CONTROL PLANS SPECIFIC TO THE STAGING BEING PERFORMED. CONTACT THE CITY INSPECTOR FOR IN PLACE INSPECTION OF TRAFFIC CONTROL MEASURES PRIOR TO COMMENCEMENT OF CONSTRUCTION WORK. FLAGGING WILL LIKELY BE REQUIRED FOR CONSTRUCTION OF ADA RAMP AT THE SOUTHEAST CORNER OF THE SW GALBREATH DRIVE/SW GERDA LANE INTERSECTION DUE TO TRUCK TRAFFIC.

#### **EROSION CONTROL KEY NOTES**

(E1) INSTALL STRAW WATTLE IN FRONT OF CURB OPENING.

GALBREATH DRIVE (PLAN VIEW)

SW

SECT

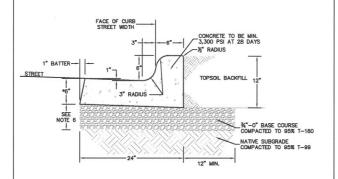
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PHONE: FAX: engine

8 T3H8

- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
- CONCRETE SHALL BE COMMERCIAL MIX. MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
- EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
- CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1½" IN DEPTH.
- 6 BASE ROCK: %"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

4	STA	NDARD DRAWING TITLE	DRAWING NUMBER
	VERTICAL CURB		RD-21
Sne wood Any alteration of this drawing may not be associated in any way with the City of Sherwood Standard Drawings.		SCALE	DATE
	N.T.S.	MAR '16	



- NOTES:

  1. MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RO—21 & RO—24 FOR THESE CONDITIONS).

  2. CONCRETE SHALL BE COMMERCALL MIX, WITH A 28—DAY COMPRESSIVE STRENGTH OF 3,300 PS, WITH A 4 MAX SULMPL. STO DE PROWINDED AT EACH:

  2. EXPANSION, MIX TANCENCY.

  3. EXPANSION FOR THE STRUCTURES.

  9. SIDE OF INLET STRUCTURES.

  10. SIDE OF DRIVEWAYS.

  4. EXPANSION JOINT MATERIAL SHALL BE PRE—MOLDED, ASPHALT IMPREGNATED, NON—EXTRUDING, WITH A THICKNESS OF \$7.

  5. CONTRACTION JOINTS SHALL HAVE:

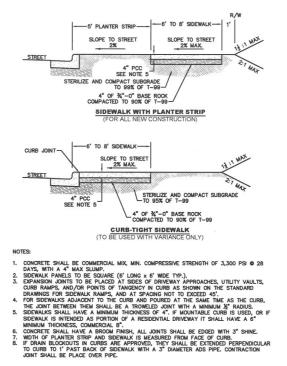
  A. SPACING OF NOT MORE THAN 15 FEET.

  B. DEPTH OF JOINT OF AT LEAST 157.

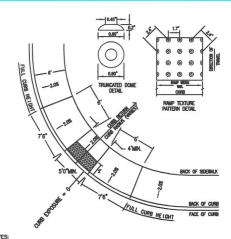
  6. BASE ROCK SHALL BE \$7.0°, COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T—180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 6°, WHICHEVER IS GREATER, AND SHALL EXTEND 12° BEHIND CURB.

- EXTEND 12" BEHIND CURB.
  FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23
- J ERCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.

Sherwood	STANDARD DRAWING TITLE		DRAWING NUMBER
	MONOLITHIC CURB AND GUTTER		RD-22
	Any alteration of this drawing may not be	SCALE	DATE
	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	MAR '16



A	STANDARD I	DRAWING TITLE	DRAWING NUMBER
	SIDEWALK DETAIL		RD-26
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
Oregon associated in any way Sherwood Standard Dr	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	MAR '16



**DETAIL** 

SHERWOOD

P

CITY

R1W, UNTY,

25,

SECT N THE IN

(503) 925–2309 (503) 625–0629 reering@sherwoodnrand

FAX: (Fax: )

**PROJECTS** 

OVERLAY

**PAVEMENT** 

2018

SHERWOOD
IG DEPARTMENT
W PINE STREET
OREGON 97140

CITY OF S
ENGINEERING
22560 SW P
SHERWOOD, 0

#### NOTES:

- IN PROJECT ENGINEER SHALL USE THIS DRAWING AS A QUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.

  PROJECT ENGINEER SHALL VERIEY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT. AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSMALK OR STOP LINE IT. AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSMALK OR STOP LINE IT. AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSMALK OR STOP LINE IT. AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO SECURITY TO A STANDARDS.

  5. DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24° LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH TIS GRIDURES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE
- PERPENDICILAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELDOW. 
  CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP. 
  CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI WITH A 4\* MAX SLUMP.

- A 4" MAX SLUMP.

  8. %"—O" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.

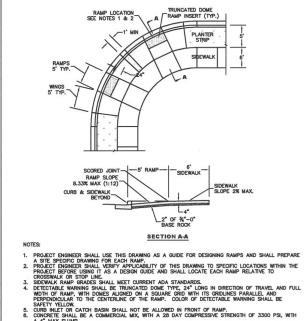
  9. BEVEL THE CURB CUT FROM GUITER TO BACK OF CURB AT 8.33% (1:12) MAX.

  10. SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.

  11. CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.

  12. PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

Α .	STANDARD DRAWING TITLE		DRAWING NUMBER
	CURB-TIGHT	CURB RETURN & RAMP DETAIL	RD-43
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	MAR '16



- PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.

  5. CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.

  6. CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH

  7. %"-O" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXMAILIN DENISTY.

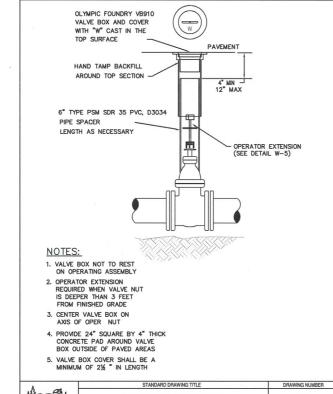
  8. BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT B.33% (1-12) MAX.

  9. SOORE CONCRETE AT GRADE CHAMGES, SURFACE TEXTURE CHAMGES AND AT ALL OTHER POINTS SHOWN.

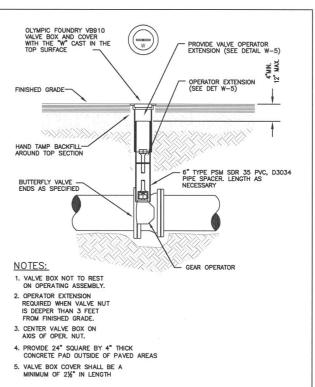
  10. CONCRETE SUFFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.

  11. PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION ESTAIL TO STAND THE STREAMS OF EACH RAMP CONSTRUCTION FROM APPLITING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

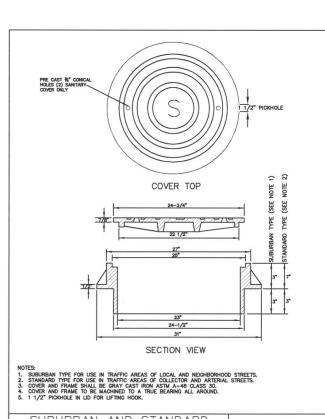
Α	STANDARD DRAWING TITLE		DRAWING NUMBER
	DOUBLE CROSS	RD-44	
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	MAR '16



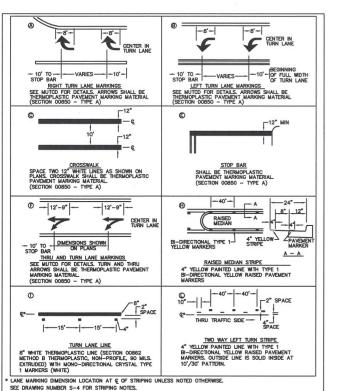
A	STANDARD I	RAWING TITLE	DRAWING NUMBER
TYPICAL GATE VALVE SETTING DETAIL		W-3	
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	JUL' 09



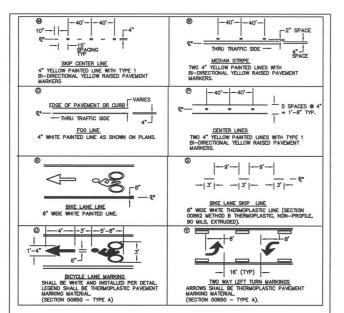
A	STANDARD	DRAWING TITLE	DRAWING NUMBER
	BUTTERFLY VALVE SETTING DETAIL		W-4
Sherwood	Any alteration of this drawing may not be	SCALE	DATE
Oregon associated in any way with the City of Sherwood Standard Drawings		N.T.S.	JUL' 09



SUBURBAN AND STANDARD MANHOLE FRAME AND COVER Clean Water Services SHEET NO неет 9 DRAWING NO. 110 SANITARY REVISED 02-17 or 10







LOCATE STOP BARS 10' BACK OF THE EXTENDED FOG LINE, EDGE OF PAVEMENT, OR CURB FACE. VERIFY SIGHT DISTANCE.

- SIGHT DISTANCE.

  LOCATE CROSSWALKS AS PER WHEELCHAIR RAMP LOCATIONS OR 5' BACK OF EXTENDED FOG LINE, EDBE OF PAREMENT OR CURB FACE.

  REMOVAL OF EXISTING STRIPPING IS TO BE DETERMINED IN THE FEELD AND IS CONSIDERED INCIDENTAL WORK.

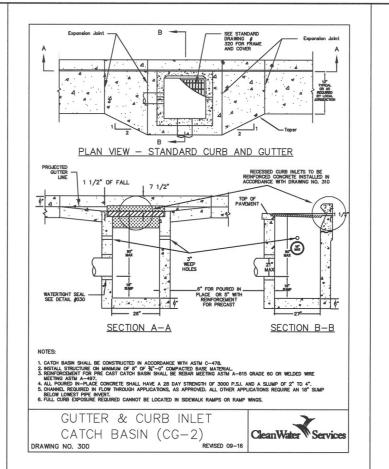
  REMOVAL OF EXISTING STRIPPING IS TO BE DETERMINED IN THE FEELD AND IS CONSIDERED INCIDENTAL WORK.

  ALL THERMOPLASTIC PAREMENT MARKING MATERIAL SHALL BE INSTALLED AS PER OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION, (LITEST EDITION).

  ALL PAINTED LINES SHALL CONFORM TO THE MOST CURRENT ODOT SPECIFICATIONS FOR BEAD BINDER PAINT AS LISTED IN 1000TS GPL.

Lane marking dimension location at  $\ensuremath{\varepsilon}$  of striping unless noted otherwise.

A .	STANDARD DRAWING TITLE		DRAWING NUMBER
		STRIPING DETAILS II	S-4
Oregon 35500	Any alteration of this drawing may not be	SCALE	DATE
	associated in any way with the City of Sherwood Standard Drawings.	N.T.S.	MAR '16



PAVEMENT OVERLAY PROJECTS 2018

DETAILS

SHERWOOD

P

CITY

B AND 32C, T2S, R1W, W WASHINGTON COUNTY, DREGON

CITY OF SHERWOOD ENGINEERING DEPARTMENT 22560 SW PINE STREET SHERWOOD, OREGON 97140

SHEET 10 or 10