

GENERAL NOTES

- ALL CONSTRUCTION WORK, MATERIALS AND TESTING SHALL CONFORM TO THE APPLICABLE SECTION OF THE CITY OF SHERWOOD ENGINEERING DESIGN AND STANDARD DETAILS MANUAL. CONTRACTOR AND SUBCONTRACTOR(S) SHALL HAVE A MINIMUM OF ONE SET OF APPROVED PLANS AND CITY OF SHERWOOD CONSTRUCTION SPECIFICATIONS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROCURE ALL APPLICABLE PERMITS, LICENSES, AND CERTIFICATES RELATIVE TO THE TRADES TO COMPLETE THE PROJECT AND FOR THE USE OF SUCH WORK WHEN COMPLETED. COMPLIANCE SHALL BE AT ALL LEVELS, FEDERAL, STATE, COUNTY, AND LOCAL, RELATING TO THE PERFORMANCE OF THIS WORK. A CITY OF SHERWOOD OR A METRO BUSINESS LICENSE IS REQUIRED FOR THE CONTRACTOR AND ALL SUBCONTRACTORS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION AND ARRANGE FOR PROTECTION OF UTILITIES IN CONFLICT WITH PROPOSED WORK. THE LOCATIONS, DEPTH AND DESCRIPTION OF EXISTING UTILITIES SHOWN WERE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. ADDITIONAL UTILITIES MAY EXIST WITHIN THE WORK AREA. POTHOLES PRIOR TO CONSTRUCTION MAY BE NECESSARY. ALL WORK TO VERIFY UTILITY LOCATIONS AND DEPTHS IS INCIDENTAL.
- OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT WWW.STATE.OR.US/GOVERNMENT.HTM. THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL (503) 246-6699.
- THE CONTRACTOR SHALL MAKE PROVISIONS TO KEEP ALL EXISTING UTILITIES IN SERVICE AND PROTECT THEM DURING CONSTRUCTION. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGED UTILITIES USING MATERIAL AND METHODS APPROVED BY THE UTILITY OWNER. NO SERVICE INTERRUPTIONS SHALL BE PERMITTED WITHOUT PRIOR WRITTEN AGREEMENT WITH THE UTILITY PROVIDER AND NOTIFICATION TO SERVICE CUSTOMER. PLEASE NOTE THE STORM SEWER IN THIS AREA IS SHALLOW - DO NOT DAMAGE. REPAIR TO UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PRUNE ALL VEGETATION, AS NECESSARY FOR EQUIPMENT CLEARANCES AWAY AND UP FROM THE STREET AND SIDEWALK. AS WELL AS ANY ROOT PRUNING AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING THAT IS TO REMAIN. DAMAGE TO TREES AND SHRUBS IS THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR. COST OF TREE PRUNING IS INCIDENTAL.
- CONTRACTOR SHALL NOTIFY THE CITY OF SHERWOOD 48 HOURS IN ADVANCE OF STARTING CONSTRUCTION AND 24 HOURS BEFORE RESUMING WORK AFTER SHUTDOWNS, EXCEPT FOR NORMAL RESUMPTION OF WORK FOLLOWING SATURDAYS, SUNDAYS, OR HOLIDAYS.
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, EXCESS TOPSOIL, AND OTHER EXCESS MATERIAL OFF SITE. MATERIALS SHALL BE DISPOSED OF IN SUCH A MANNER AS TO MEET ALL APPLICABLE REGULATIONS. DISPOSAL LOCATION INFORMATION TO BE PROVIDED TO CITY PRIOR TO BEGINNING ANY WORK.
- CONSTRUCTION VEHICLES SHALL PARK AT A LOCATION APPROVED BY THE INSPECTOR OR AT A LOCATION(S) INDICATED ON AN APPROVED PLAN. HOURS OF CONSTRUCTION SHALL BE AS SPECIFIED IN THE PLANS - UNLESS OTHERWISE OBTAINED BY THE INSPECTOR. CONSTRUCTION IS PROHIBITED ON SATURDAY AND SUNDAY UNLESS WRITTEN APPROVAL IS OBTAINED FROM THE PUBLIC WORKS DIRECTOR AND CITY MANAGER. CONSTRUCTION ACTIVITIES INCLUDE ALL FIELD MAINTENANCE OF EQUIPMENT, REFUELING, AND PICK UP AND DELIVERY OF EQUIPMENT AS WELL AS THE ACTUAL CONSTRUCTION ACTIVITY.
- AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN UP THE PROJECT AREA AND LEAVE IT IN A NEAT AND SECURED MANNER. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL.
- THE CONTRACTOR SHALL KEEP AN APPROVED SET OF PLANS ON THE PROJECT SITE AT ALL TIMES WITH ALL KNOWN CHANGES REDLINED ON THE WORKING PLAN SET FOR THE SOLE PURPOSE OF RECORDING AS BUILT INFORMATION OF IMPROVEMENTS AND LOCATION AND DEPTH OF ANY EXISTING UTILITIES ENCOUNTERED.
- ANY ALTERATIONS OR VARIATIONS FROM THESE PLANS EXCEPT MINOR FIELD ADJUSTMENTS NEEDED TO MEET EXISTING FIELD CONDITIONS, SHALL BE APPROVED BY THE ENGINEER AND APPLICABLE REGULATORY AGENCY REPRESENTATIVE. CONTRACTOR SHALL KEEP RECORD OF ALL CHANGES AND NOTE ON AS BUILT PLANS. CONTRACTOR SHALL SUBMIT ACCURATE AND COMPLETE "AS-BUILT" INFORMATION TO THE ENGINEER UPON COMPLETION OF CONSTRUCTION.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THIS PROJECT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET THE INTENT OF THE PROJECT CONTRACT DOCUMENTS, APPLICABLE AGENCY REQUIREMENTS AND OTHER WORK AS NECESSARY TO PROVIDE A COMPLETE PROJECT.
- CONTRACTOR SHALL PROVIDE EFFECTIVE EROSION PROTECTION TO INCLUDE, BUT NOT BE LIMITED TO, GRADING, DITCHING, STRAW WATTLES, TRIANGULAR SILT DAMS, SILT FENCING, AND SEDIMENT BARRIERS TO MINIMIZE EROSION AND IMPACT TO ADJACENT PROPERTY. SEE EROSION AND SEDIMENT CONTROL NOTES AND PLANS.
- THE CONTRACTOR SHALL MAINTAIN AND COORDINATE ACCESS TO ALL AFFECTED PROPERTIES. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS A MINIMUM OF 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURES AND ASSIST PROPERTY OWNER WITH ALTERNATIVE PARKING AND ACCESS. PEDESTRIAN ACCESS TO ENTRANCES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL PROVIDE RAMPS OR OTHER APPROVED METHODS FOR MAINTAINING ACCESS TO ENTRANCES. RAMPS SHALL BE ADA COMPLIANT, APPROVED BY THE ENGINEER, AND INCIDENTAL TO THE CONTRACT. RESIDENTIAL DRIVEWAYS MAY ONLY BE CLOSED FOR A MAXIMUM OF ONE DAY (7:00 AM TO 6:00 PM).
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND VERIFY ALL EXISTING CONDITIONS BEFORE THE START OF WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY FIELD MEASUREMENTS AND OTHERWISE VERIFY ALL DIMENSIONS AND EXISTING CONSTRUCTION CONDITIONS INDICATED AND/OR SHOWN ON THE PLANS. SHOULD ANY ERROR OR INCONSISTENCY EXIST, THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK AFFECTED UNTIL REPORTED TO THE PROJECT ENGINEER FOR CLARIFICATION OR CORRECTION.
- ANY INSPECTION BY THE CITY, COUNTY, STATE, FEDERAL AGENCY OR PROJECT ENGINEER SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN COMPLIANCE WITH THE APPLICABLE CODES, REGULATIONS, CITY STANDARDS AND PROJECT CONTRACT DOCUMENTS.
- CONTRACTOR TO VERIFY CENTERLINE AND GUTTER SLOPES PRIOR TO CONSTRUCTION TO ENSURE COMPLIANCE WITH THE CONSTRUCTION DRAWINGS AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN CONFORMING TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION AND ACCORDING TO PROJECT SPECIFICATIONS. TRAFFIC CONTROL PLAN SHALL MATCH APPROVED PHASING/SEQUENCING PLAN AND SHALL BE SUBJECT TO THE APPROVAL BY THE CITY, WASHINGTON COUNTY AND/OR ODOT AS APPLICABLE.
- THE CONTRACTOR SHALL PROVIDE TO THE CITY PROJECT MANAGER AND INSPECTOR A 24 HOUR CONTACT PERSON AND CELL PHONE NUMBER.
- NO TRENCHES OR PITS WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT. ALL TRENCHES AND PITS SHALL BE COVERED WITH STEEL PLATES OR FILLED IN AT NIGHT.
- PROPERTY LINES SHOWN ON ALL PLAN SHEETS ARE FOR GENERAL DELINEATION ONLY AND ARE, BY NO MEANS, MEANT TO REPRESENT THE ACTUAL BOUNDARIES.

- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL CONSTRUCTION SAFETY, HEALTH AND OTHER RULES AND REGULATIONS FROM OSHA, DEQ, STATE, AND LOCAL REGULATING AGENCIES FOR SAFETY AND INSTALLATION OF THE WORK INCLUDING BUT NOT LIMITED TO SHORING, BRACING, ERECTION/INSTALLATION, FALL PROTECTION, GUARDRAILS, ETC.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO REMOVE EXISTING AGGREGATE BASE AS NECESSARY TO CEMENT TREAT BASE AND ACHIEVE SURFACE BASE LEVEL FOR PLACEMENT OF NEW HMAC WHILE MEETING THE CURB AND DRIVEWAY EXPOSURE REQUIREMENTS IN THE PLANS.
- WHEN MATCHING EXISTING PAVEMENT, SAWCUT TO FIRM PAVEMENT. MINIMUM SAWCUT IS 1' FROM EXISTING EDGE OF PAVEMENT OR AS SHOWN ON PLANS. SAWCUT LOCATION TO BE APPROVED BY INSPECTOR.
- CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING STRUCTURES AND UTILITIES NOT SHOWN TO BE REMOVED. CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING STRUCTURES (SIDEWALKS, DRIVEWAYS, CURB, FENCE, STREET TREES, ETC.) DAMAGED DURING CONSTRUCTION, IN ACCORDANCE WITH CITY STANDARDS.
- ALL LINEWORK SHOWN ON THE PLAN IS FROM A 2004 AERIAL MAP AND FROM VISUAL OBSERVATION AND IS THEREFORE NOT GUARANTEED FOR ACCURACY AND SHOULD NOT BE USED FOR OTHER PURPOSES.

EROSION CONTROL NOTES

- THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION/LANDSCAPING IS ESTABLISHED.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED AFTER PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS RE-ESTABLISHED.
- IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH MAY 31ST, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL (CHAPTER 4) OF CLEAN WATER SERVICES FOR REQUIREMENTS.
- THE CONTRACTOR SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
- ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (FILTER BAG, DIRT BAG, ETC.)
- SITE EROSION CONTROL PLAN AND BMP'S MEETING CWS STANDARDS TO BE IN PLACE AND APPROVED PRIOR TO CONSTRUCTION.
- TRACKING OF MATERIALS OUTSIDE OF THE WORK ZONE, ONTO ADJACENT STREETS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE AND CLEAN TO THE SATISFACTION OF THE INSPECTOR. THIS INCLUDES BUT IS NOT LIMITED TO SOIL, AGGREGATE, ASPHALT DEBRIS, ETC.
- CONTRACTOR TO PROVIDE FOR CONCRETE WASHOUT. SUBMIT PLAN FOR CONCRETE WASHOUT TO INSPECTOR FOR APPROVAL (INCIDENTAL).

WORK HOURS/TRAFFIC CONTROL NOTES

- WORK HOURS ARE FROM 8:00AM TO 6:00PM MONDAY THROUGH FRIDAY. SATURDAY WORK (FOUNDRY AVENUE) WILL BE FROM 8:00AM TO 5:00PM.
- WORK WITHIN THE SW WILLAMETTE STREET/SW 2ND STREET INTERSECTION WHEN RESTRICTING THE INTERSECTION FROM FULL USAGE, SHALL BE FLAGGED.
- SW WASHINGTON STREET MAY BE CLOSED FOR CONCRETE REMOVAL, CEMENT GRADING AND ASPHALT PLACEMENT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO GIVE ADVANCE NOTICE TO ALL OF THE RESIDENTS ON THIS SECTION OF SW WASHINGTON STREET. THE SW WASHINGTON STREET/SW 2ND STREET INTERSECTION SHALL BE FULLY FUNCTIONAL AT THE END OF EACH WORK DAY.
- SW WASHINGTON STREET BETWEEN SW WILLAMETTE STREET MAY BE "CLOSED - LOCAL TRAFFIC ONLY" AS NECESSARY. DURING CEMENT TREATMENT AND PAVING THE STREET MAY BE CLOSED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO GIVE ADVANCE NOTICE TO ALL OF THE RESIDENTS ON THIS SECTION OF SW WASHINGTON STREET, SW OGDEN GRIFFITH LANE, THE ARTS CENTER AND BREW HOUSE. CONTRACTOR TO CONTACT CITY PROJECT MANAGER TO VERIFY SCHEDULE OF CEMENT TREATMENT AND PAVING TO CONFIRM THAT IT DOES NOT CONFLICT WITH ART CENTER ACTIVITIES.
- SW FOUNDRY STREET RECONSTRUCTION WORK TO BE PERFORMED ON SATURDAYS. SEE WORK SCHEDULE NOTE ON SHEET 5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO GIVE ADVANCE NOTICE TO ALL OF THE RESIDENTS ON SW FOUNDRY STREET.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EMERGENCY VEHICLE ACCESS TO ALL PROPERTIES AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS FOR MAIL AND TRASH (FRIDAY) AT ALL TIMES.
- TRAFFIC CONTROL SHALL INCLUDE ALL PAVEMENT MARKING REMOVAL, TEMPORARY PAVEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAVEMENT MARKINGS FOR BOTH VEHICULAR AND PEDESTRIAN TRAFFIC AND ALL INCIDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".
- CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION OF THIS TRAFFIC CONTROL PLAN AND ITS CONTINUED FUNCTIONING FOR THE PROTECTION OF CONSTRUCTION WORKERS, VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIANS. ALL TRAFFIC CONTROL DEVICES/SIGNAGE SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. APPROVAL OF THE TRAFFIC CONTROL PLAN BY THE CITY OF SHERWOOD DOES NOT NEGATE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN A SAFE WORK ZONE. THE CITY OF SHERWOOD BEARS NO LIABILITY FOR THE CONTRACTOR'S IMPLEMENTATION OF THIS TRAFFIC CONTROL PLAN.

STREET NOTES

- IN AREAS WHERE CURB AND GUTTER EXISTS, NEW ASPHALT WILL MATCH FRONT EDGE OF CONCRETE GUTTER.
- TACK COAT SHALL BE APPLIED AGAINST CONCRETE GUTTER EDGE, CURB FACE, CONCRETE STRUCTURES, EXISTING ASPHALT EDGE, MANHOLE FRAMES, BLOW-OFF FRAMES, CLEANOUT FRAMES AND OTHER SURFACES OR STRUCTURES THAT WILL BE PAVED AGAINST WITH NEW ASPHALT (INCIDENTAL) PRIOR TO PAVING.
- HOT RUBBERIZED ASPHALT SEALER REQUIRED AT ASPHALT JOINTS (INCIDENTAL).
- PAVEMENT CROSS SLOPES SHALL BE AT LEAST 2.0%.
- NEW ASPHALT SHALL MATCH EDGE OF CONCRETE CATCH BASINS. THERE WILL BE NO ADJUSTING OF CATCH BASIN GRATES ALLOWED.
- NEW ASPHALT SHALL NOT COVER ANY WEEP HOLES (AT I.E. OR LOWER). CITY WILL PHOTO DOCUMENT PROJECT AREA INCLUDING EXISTING CURBS. COVERED WEEPHOLES SHALL BE CLEARED AT CONTRACTORS EXPENSE AND STREET REPAVED.
- TYPICAL FULL CURB EXPOSURE IS 6" AT TOP FACE OF CURB UNLESS OTHERWISE NOTED.
- TYPICAL DRIVEWAY CURB EXPOSURE IS 3/4" AT TOP FACE OF CURB.
- ADJUST ALL EXISTING VALVES, BLOW-OFFS AND OTHER MISCELLANEOUS BOXES AS NECESSARY TO MATCH NEW ASPHALT SURFACE GRADE - TO BE PAID FOR UNDER THE BID ITEM "ADJUSTING BOXES".
- CONTRACTOR TO TEST ASPHALT SURFACE WITH 12-FOOT STRAIGHT EDGE IN TRAVEL LANES PARALLEL TO AND PERPENDICULAR TO THE CENTER LINE, AS DIRECTED BY THE INSPECTOR. THE ASPHALT SURFACE SHALL NOT VARY BY MORE THAN 1/4 INCH.
- EQUIPMENT PARKED ON SITE SHALL BE PARKED AWAY FROM AN INTERSECTION AT A LOCATION APPROVED BY THE INSPECTOR.
- WATER FOR CONSTRUCTION MAY BE OBTAINED FROM CITY FIRE HYDRANTS. INSTALL FIRE HYDRANT METER PRIOR TO DRAINING WATER. FIRE HYDRANT METER TO BE OBTAINED FROM CITY OF SHERWOOD WATER DEPARTMENT. COORDINATE WITH INSPECTOR. WATER FOR CONSTRUCTION IS INCIDENTAL.
- REPAIR ALL DISTURBED NEIGHBORING PROPERTY BACK TO ORIGINAL OR BETTER CONDITION.
- ANY SEGMENT OF STREET OPEN TO TRAFFIC THAT HAS A VERTICAL DROP (INCLUDING DRIVEWAYS) SHALL HAVE A TEMPORARY WEDGE INSTALLED. (INCIDENTAL).
- ANY EXISTING SURFACE TO REMAIN (CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, ETC.) THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED AND THE CONTRACTOR'S EXPENSE.
- CONTRACTOR TO CLEAN UP JAGGED CONCRETE EDGES MEETING THE APPROVAL OF THE INSPECTOR. SOME SAWCUTTING MAY BE REQUIRED. COST IS INCIDENTAL.

STORM SEWER NOTES

- STORM SEWER MATERIALS AND TESTING SHALL MEET CLEAN WATER SERVICES (CWS) DESIGN AND CONSTRUCTION SPECIFICATIONS AND THE CITY OF SHERWOOD'S ENGINEERING DESIGN MANUAL.
- ALL STORM SEWER LINES SHALL BE VIDEO INSPECTED BY THE CONTRACTOR. TESTING AND INSPECTION SHALL BE IN ACCORDANCE WITH ALL APPLICABLE CODES. THIS WILL BE WITNESSED BY THE CITY. MINIMUM 48 HOUR NOTICE IS REQUIRED. CITY WITNESSED VIDEO INSPECTION SHALL OCCUR AFTER THE PLACEMENT OF ASPHALT. CITY STRONGLY ENCOURAGES VIDEO INSPECTION BY THE DEVELOPER AND/OR CONTRACTOR PRIOR TO ASPHALT PLACEMENT. SHOULD CONTRACTOR OR DEVELOPER HAVE QUESTIONS REGARDING SPECIFIC SECTIONS OF PRE-ASPHALT VIDEO, CITY INSPECTOR SHALL PROVIDE A RECOMMENDATION UPON THE ACCEPTABILITY OF THE SECTION IN QUESTION.
- ALL STORM SEWER LINES SHALL HAVE A MANDREL PASSED THROUGH TO CHECK DEFLECTION. THIS WILL BE WITNESSED BY THE CITY. MINIMUM 48 HOUR NOTICE IS REQUIRED.

LEGEND

- ===== = EXISTING CURB AND GUTTER
- = EXISTING/FINAL PAVEMENT MARKING
- ⊗_{WV} = EXISTING WATER VALVE
- W— = EXISTING WATER LINE
- ST— = EXISTING STORM LINE
- SS— = EXISTING STORM LINE
- G— = EXISTING GAS LINE
- T— = EXISTING TELECOMMUNICATION LINE
- = EXISTING MANHOLE
- ≡ = EXISTING CATCH BASIN
- S/W = SIDEWALK

GENERAL NOTES

2019 PAVEMENT REHAB PROJECTS

LOCATED IN SECTIONS 32B AND 32A, T2S, R1W, W.M., IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22860 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 825-2309
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E-MAIL: engineering@shermetroregon.gov

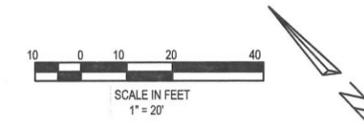
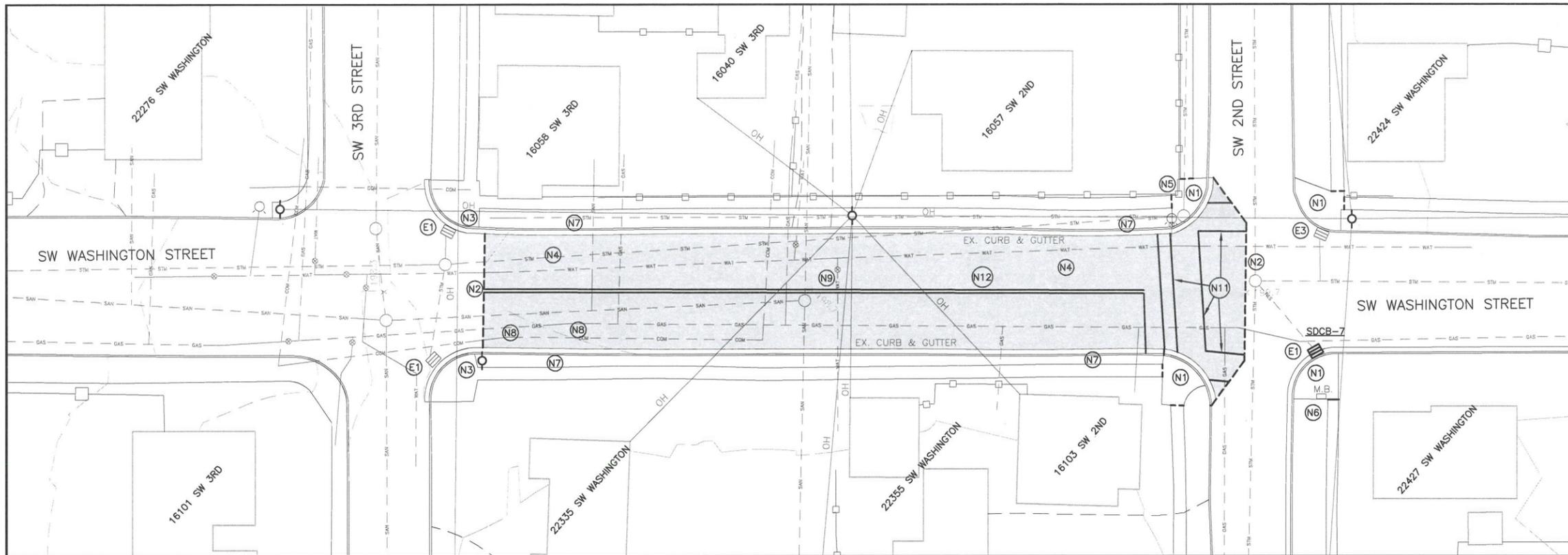


3-14-19

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	N/A
DATE:	MARCH, 2019

JOB NO.	
SHEET NO.	2
REV.	9

REVISIONS



SW WASHINGTON STREET
(NORTH - PLAN VIEW)

2019 PAVEMENT REHAB PROJECTS
LOCATED IN SECTIONS 32B AND 32A, T2S, R1W, W.M. IN
THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

STANDARD KEY NOTES

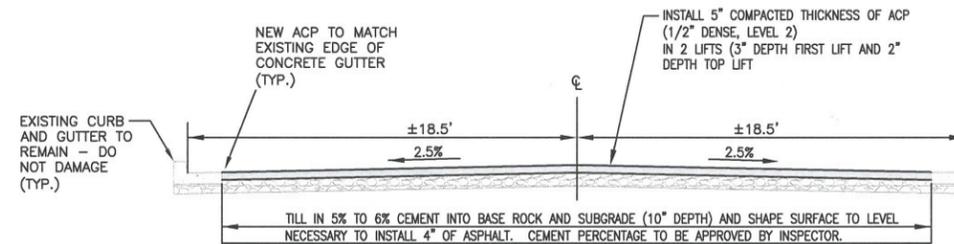
- (N1) REMOVE EXISTING SIDEWALK RAMP AND INSTALL NEW SIDEWALK RAMP TO MEET ADA STANDARDS. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT. COORDINATE LAYOUT AND LIMITS OF REMOVAL WITH ENGINEER AND INSPECTOR PRIOR TO REMOVAL. ONLY ONE SIDEWALK RAMP MAY BE REMOVED AND REPLACED AT A TIME TO MAINTAIN PEDESTRIAN ACCESS.
- (N2) SAWCUT AND MATCH EXISTING ASPHALT.
- CONFIRM LOCATION OF SAWCUT WITH INSPECTOR
- (N3) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT.
- (N4) AREA OF NEW ASPHALT - SEE TYPICAL SECTIONS TO THE RIGHT.
- (N5) EXISTING FENCE TO BE REMOVED AND REPLACED AS NECESSARY. COORDINATE WITH OWNER.
- (N6) EXISTING MAILBOX TO BE TEMPORARILY RELOCATED DURING RAMP CONSTRUCTION. NEW CONCRETE MAIL BOX PAD TO BE INSTALLED ADJACENT TO SIDEWALK LANDING PAD AREA. COORDINATE WITH ENGINEER AND INSPECTOR.
- (N7) EXISTING CURB AND GUTTER TO REMAIN. DO NOT DAMAGE.
- (N8) EXISTING GAS LINE FROM GAS POT HOLE #18 TO NORTHWEST IS UNDER 2 FEET IN DEPTH. CONTRACTOR TO POT HOLE GAS LINE AND PROTECT AS NECESSARY AGAINST DAMAGE DURING THE CEMENT TREATMENT PROCESS.
- (N9) REPLACE EXISTING WATER VALVE BOX WITH NEW VALVE BOX. SEE DETAIL ON SHEET 7.
- (N10) INSTALL 12-INCH WIDE WHITE THERMOPLASTIC CROSSWALK BAR. VERIFY LOCATION WITH INSPECTOR PRIOR TO PLACING.
- (N11) INSTALL 12-INCH WIDE WHITE THERMOPLASTIC STOP BAR. VERIFY LOCATION WITH INSPECTOR PRIOR TO PLACING.
- (N12) INSTALL TWO 4-INCH WIDE SOLID YELLOW CENTER LINES WITH 12-INCH SPACE IN BETWEEN. CENTER IN STREET.

EROSION CONTROL NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- (E3) INSTALL BIOBAGS IN FRONT OF CURB OPENING AND SILT SACK IN CATCH BASIN.

GAS NOTE

THE GAS COMPANY INTENDS TO LOWER THE GAS SERVICES AT GAS POT HOLE #14 AND #17 PRIOR TO CONSTRUCTION. IF WORK HAS NOT BEEN COMPLETED BEFORE THE START OF CONSTRUCTION, THEN COORDINATE WORK WITH NORTHWEST NATURAL BEFORE CEMENT TREATMENT.



SW WASHINGTON STREET (2ND ST. TO 3RD ST.) - TYPICAL SECTION

NOT TO SCALE

STREET SECTION NOTES

EXISTING CURB AND GUTTER TO REMAIN - DO NOT DAMAGE.

REMOVE EXISTING CONCRETE (±7" THICK) AND BASE ROCK AS NECESSARY FOR CEMENT TREATMENT AND NEW ASPHALT TO ACHIEVE FINAL GRADES SHOWN IN THE PLANS. USE CAUTION WHEN REMOVING CONCRETE PAVEMENT TO USE MEANS THAT WILL NOT CAUSE EXCESSIVE SHAKING DAMAGE TO OLDER HOMES IN THIS AREA.

COMPACT SUBGRADE TO 95% DENSITY OF AASHTO T-99 TEST METHOD.

COMPACT ALL BASE ROCK TO 95% DENSITY OF AASHTO T-180 TEST METHOD (INSPECTOR WITNESSED PROOF ROLL REQUIRED).

COMPACT ALL ASPHALT TO 92% RICE METHOD.

IF THERE IS NOT ENOUGH EXISTING AGGREGATE TO ACHIEVE THE FINISH GRADE SHOW ON THE PLANS, THEN ADDITIONAL 3/4"-0" BASE AGGREGATE SHALL BE ADDED TO ACHIEVE NECESSARY GRADES FOR CEMENT TREATMENT.

BASE ROCK WILL BE PROOF ROLLED AFTER CEMENT TREATMENT.

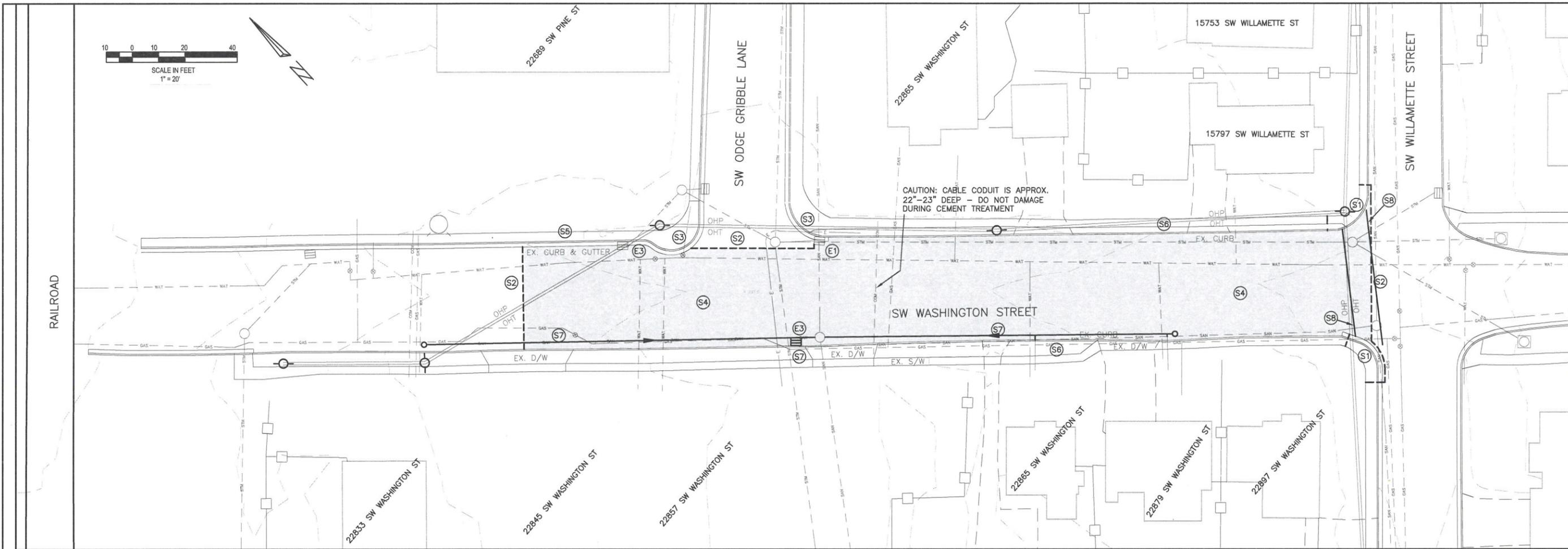
CEMENT TREATMENT TO BE KEPT MOIST UNTIL 1ST LIFT OF ASPHALT IS PLACED. FIRST LIFT OF ASPHALT SHALL BE INSTALLED EITHER THE DAY OF OR THE DAY AFTER CEMENT TREATMENT OF THE BASE.

CITY OF SHERWOOD
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DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	1"=20'
DATE:	MARCH, 2019
2019 PAVING_SHEETS.DWG	

NO.	DESCRIPTION



SW WASHINGTON STREET
(PLAN VIEW)

2019 PAVEMENT REHAB PROJECTS
LOCATED IN SECTIONS 32B AND 32A, T2S, R1W, W4M, IN
THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

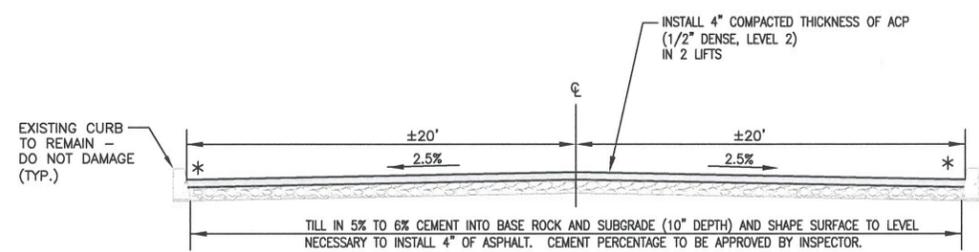
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SHERWOOD, OREGON 97140
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STANDARD KEY NOTES

- (S1) REMOVE EXISTING SIDEWALK RAMP AND INSTALL NEW SIDEWALK RAMP TO MEET ADA STANDARDS. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT. COORDINATE LAYOUT AND LIMITS OF REMOVAL WITH ENGINEER AND INSPECTOR PRIOR TO REMOVAL. ONLY ONE SIDEWALK RAMP MAY BE REMOVED AND REPLACED AT A TIME TO MAINTAIN PEDESTRIAN ACCESS.
- (S2) SAWCUT AND MATCH EXISTING ASPHALT.
- CONFIRM LOCATION OF SAWCUT WITH INSPECTOR
- (S3) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT.
- (S4) AREA OF NEW ASPHALT - SEE TYPICAL SECTIONS TO THE RIGHT.
- (S5) EXISTING CURB AND GUTTER TO REMAIN. DO NOT DAMAGE.
- (S6) EXISTING CURB TO REMAIN. DO NOT DAMAGE.
- (S7) NEW SANITARY SEWER AND STORM CATCH BASIN TO BE INSTALLED BY OTHERS PRIOR TO CONSTRUCTION.
- (S8) INSTALL 12-INCH WIDE WHITE THERMOPLASTIC CROSSWALK BAR. VERIFY LOCATION WITH INSPECTOR PRIOR TO PLACING.

EROSION CONTROL NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- (E3) INSTALL BIOBAGS IN FRONT OF CURB OPENING AND SILT SACK IN CATCH BASIN.



SW WASHINGTON STREET (EDGE GRIBBLE LANE TO WILLAMETTE ST.) - TYPICAL SECTION
NOT TO SCALE

STREET SECTION NOTES

- EXISTING CURB TO REMAIN - DO NOT DAMAGE.
- REMOVE EXISTING ASPHALT (±4" THICK) AND BASE ROCK AS NECESSARY FOR CEMENT TREATMENT AND NEW ASPHALT TO ACHIEVE FINAL GRADES SHOWN IN THE PLANS.
- COMPACT SUBGRADE TO 95% DENSITY OF AASHTO T-99 TEST METHOD.
- COMPACT ALL BASE ROCK TO 95% DENSITY OF AASHTO T-180 TEST METHOD (INSPECTOR WITNESSED PROOF ROLL REQUIRED).
- COMPACT ALL ASPHALT TO 92% RICE METHOD.
- IF THERE IS NOT ENOUGH EXISTING AGGREGATE TO ACHIEVE THE FINISH GRADE SHOW ON THE PLANS, THEN ADDITIONAL 3/4"-0" BASE AGGREGATE SHALL BE ADDED TO ACHIEVE NECESSARY GRADES FOR CEMENT TREATMENT.
- BASE ROCK WILL BE PROOF ROLLED AFTER CEMENT TREATMENT.
- CEMENT TREATMENT TO BE KEPT MOIST UNTIL 1ST LIFT OF ASPHALT IS PLACED. FIRST LIFT OF ASPHALT SHALL BE INSTALLED EITHER THE DAY OF OR THE DAY AFTER CEMENT TREATMENT OF THE BASE.
- * CURB EXPOSURE TO BE ±6" IN STANDARD HEIGHT CURB LOCATIONS. MAY FLUCTUATE AS NECESSARY TO AVOID PONDING AT GUTTER LINE. DO NOT COVER WEEP HOLES. CURB EXPOSURES IN DRIVEWAYS SHALL BE ±3/4". ASPHALT AT THROAT OF SIDEWALK RAMPS SHALL BE FLUSH WITH CURB.

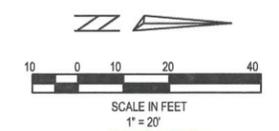
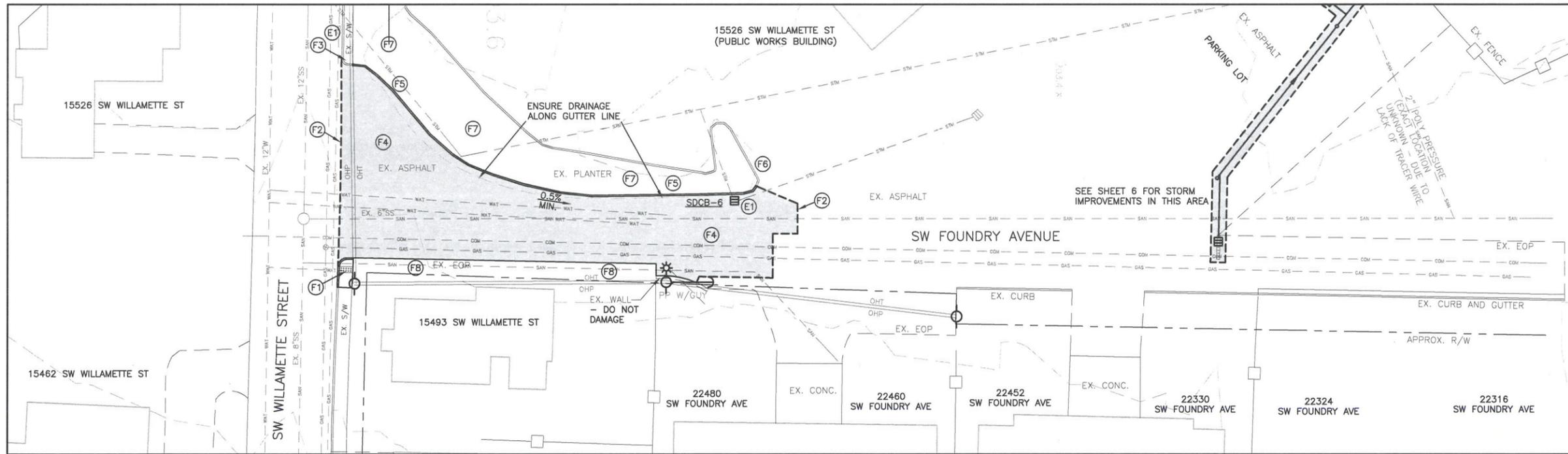
3-19-19



DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/VJS
FULL SIZE SCALE:	1"=20'
DATE:	MARCH 2019
2019 PAVING_SHEETS.DWG	

JOB NO.	
SHEET NO.	4
	9

REVISIONS

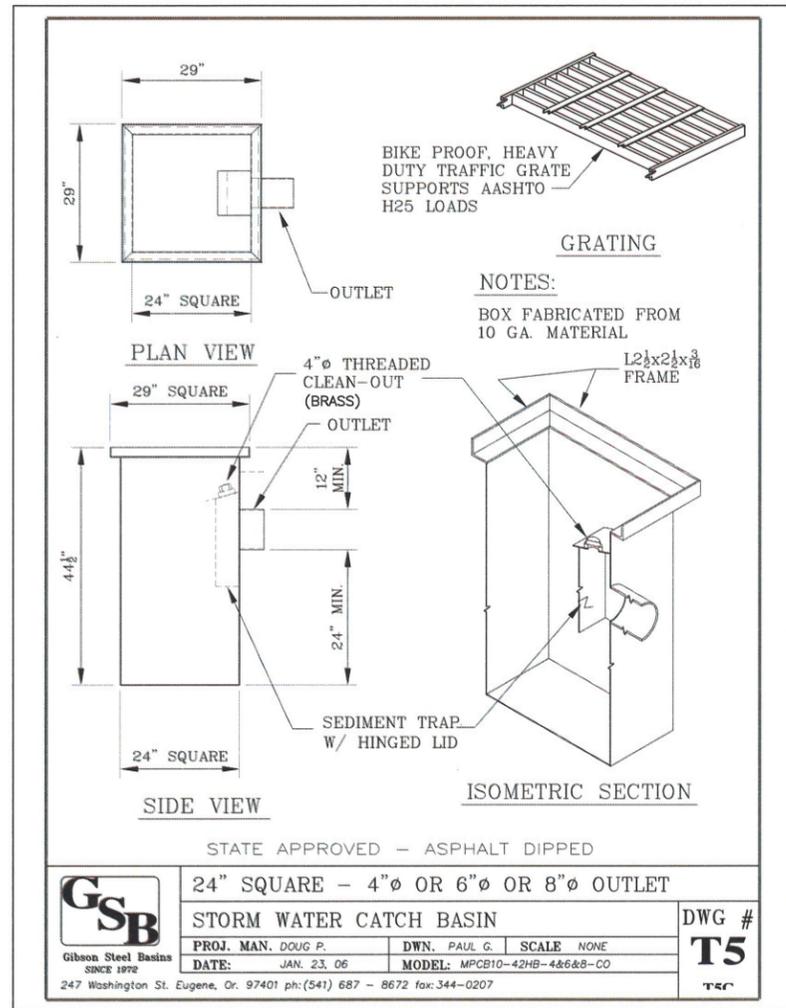


STORM KEY NOTES

SDCB-6
 REMOVE EXISTING CATCH BASIN AND
 INSTALL NEW GIBSON CATCH BASIN DETAIL
 T6 OR APPROVED EQUIVALENT
 GRATE EL. = 199.50
 4" I.E. IN = 197.33 (E)
 4" I.E. OUT = 197.28 (N)
 MATCH NEW ASPHALT TO C.B. GRATE
 CONNECT TO EXISTING STORM SEWERS
 (INCIDENTAL)

EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN GUTTER.

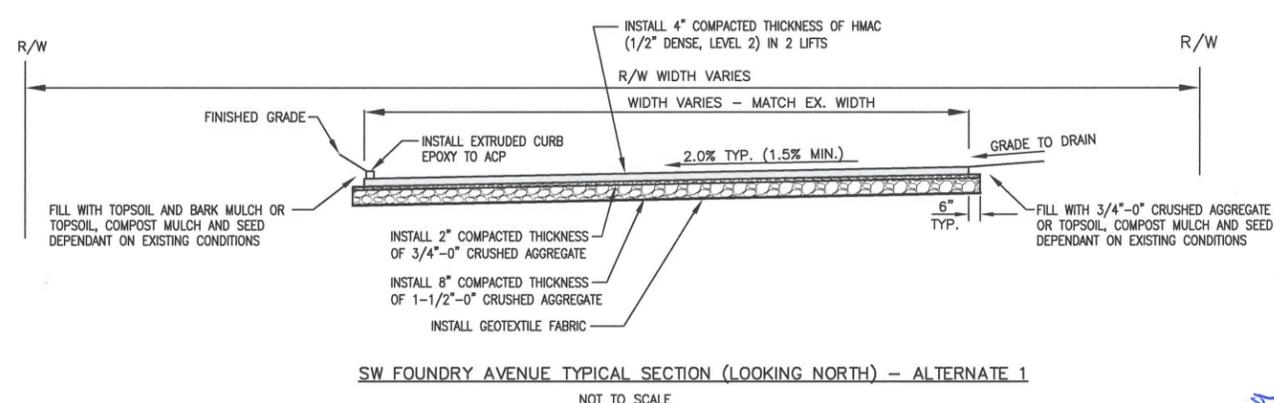
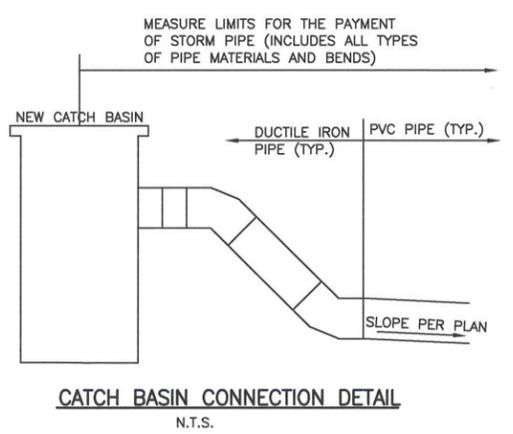


STANDARD KEY NOTES

- (F1) REMOVE EXISTING SIDEWALK RAMP AND INSTALL NEW SIDEWALK RAMP TO MEET ADA STANDARDS. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT. COORDINATE LAYOUT AND LIMITS OF REMOVAL WITH ENGINEER AND INSPECTOR PRIOR TO REMOVAL. SINCE THIS IS THE ONLY SIDEWALK IN THE AREA, SIDEWALK CLOSURE TIME SHALL BE MINIMIZED.
- (F2) SAWCUT AND MATCH EXISTING ASPHALT. - CONFIRM LOCATION OF SAWCUT WITH INSPECTOR
- (F3) EXISTING SIDEWALK RAMP TO REMAIN - DO NOT DAMAGE. ASPHALT TO BE FLUSH WITH SIDEWALK RAMP ACCESS POINT.
- (F4) AREA OF NEW ASPHALT - SEE TYPICAL SECTIONS TO THE RIGHT.
- (F5) EXISTING EXTRUDED CURB TO BE REMOVED AND REPLACED IN KIND. EXTRUDED CURB TO BE SECURED TO THE ASPHALT WITH EPOXY.
- (F6) EXISTING EXTRUDED CURB TO REMAIN - DO NOT DAMAGE.
- (F7) EXISTING SHRUBS AND TREES TO REMAIN - DO NOT DAMAGE.
- (F8) SHAPE GRAVEL/GROUND TO MATCH NEW EDGE OF ASPHALT.

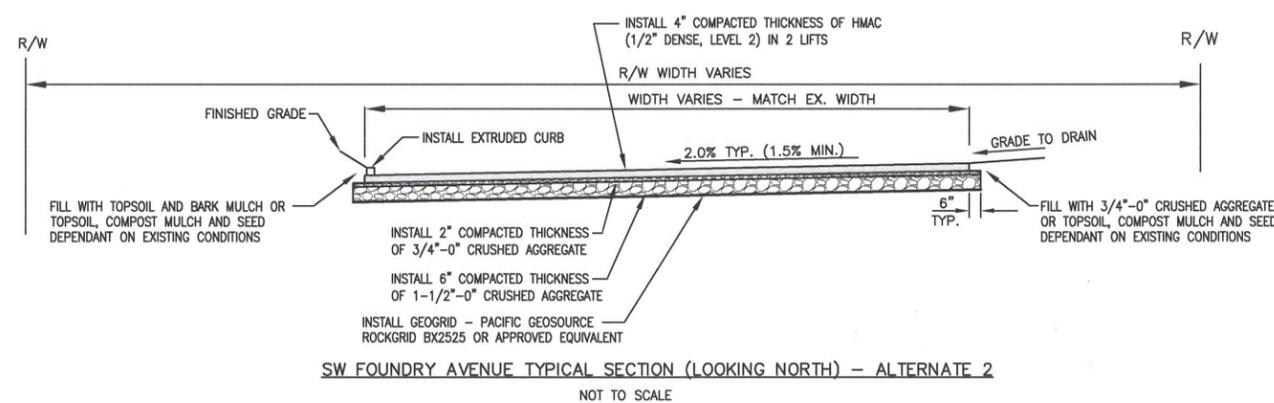
WORK SCHEDULE NOTE

DUE TO SW FOUNDRY STREET BEING THE ONLY ACCESS FOR THE PUBLIC WORKS BUILDING, STREET RECONSTRUCTION OF SW FOUNDRY STREET SHALL BE PERFORMED ON SATURDAYS. AFTER EACH SATURDAY OF WORK, SW FOUNDRY STREET SHALL BE LEFT IN A CONDITION ADEQUATE FOR VEHICULAR TRAFFIC. CONTACT RICH SATTLER (503) 925-2319 AT LEAST A WEEK AHEAD FOR SATURDAY INSPECTIONS. IT IS ANTICIPATED THAT STREET RECONSTRUCTION WORK WILL TAKE 2 SATURDAYS (ONE FOR REMOVAL, EXCAVATION AND BASE ROCK WITH A SECOND SATURDAY FOR ASPHALT PLACEMENT). IF 2 SATURDAYS ARE NECESSARY, THEN THE WORK WILL BE DONE ON CONSECUTIVE SATURDAYS.



STREET SECTION NOTES

EXISTING EXTRUDED CURB AND ASPHALT SURFACE TO BE REMOVED (±2" THICK).
 CONFIRM TYPICAL SECTION TO BE USED FOR EXCAVATION DEPTH AND PAVEMENT STRUCTURE.
 EXCAVATE TO NEEDED DEPTH AND CONTACT INSPECTOR FOR PROOF ROLL.
 COMPACT SUBGRADE TO 95% DENSITY OF AASHTO T-99 TEST METHOD.
 COMPACT ALL BASE ROCK TO 95% DENSITY OF AASHTO T-180 TEST METHOD (INSPECTOR WITNESSED PROOF ROLL REQUIRED).
 COMPACT ALL ASPHALT TO 92% RICE METHOD.



ALTERNATE SECTION NOTE

SELECTION OF EITHER ALTERNATE 1 OR ALTERNATE 2 WILL RESULT IN A REDUCTION OR ELIMINATION OF CERTAIN BID QUANTITIES DEPENDING UPON WHICH ALTERNATE IS SELECTED. CONFIRM ALTERNATE PRIOR TO BE USED PRIOR TO CONSTRUCTION

FOUNDRY STREET
(PLAN AND PROFILE)

2019 PAVEMENT REHAB PROJECTS
 LOCATED IN SECTIONS 32B AND 32A, T2S, R1W, W.M. IN
 THE CITY OF SHERWOOD, WASHINGTON COUNTY,
 STATE OF OREGON

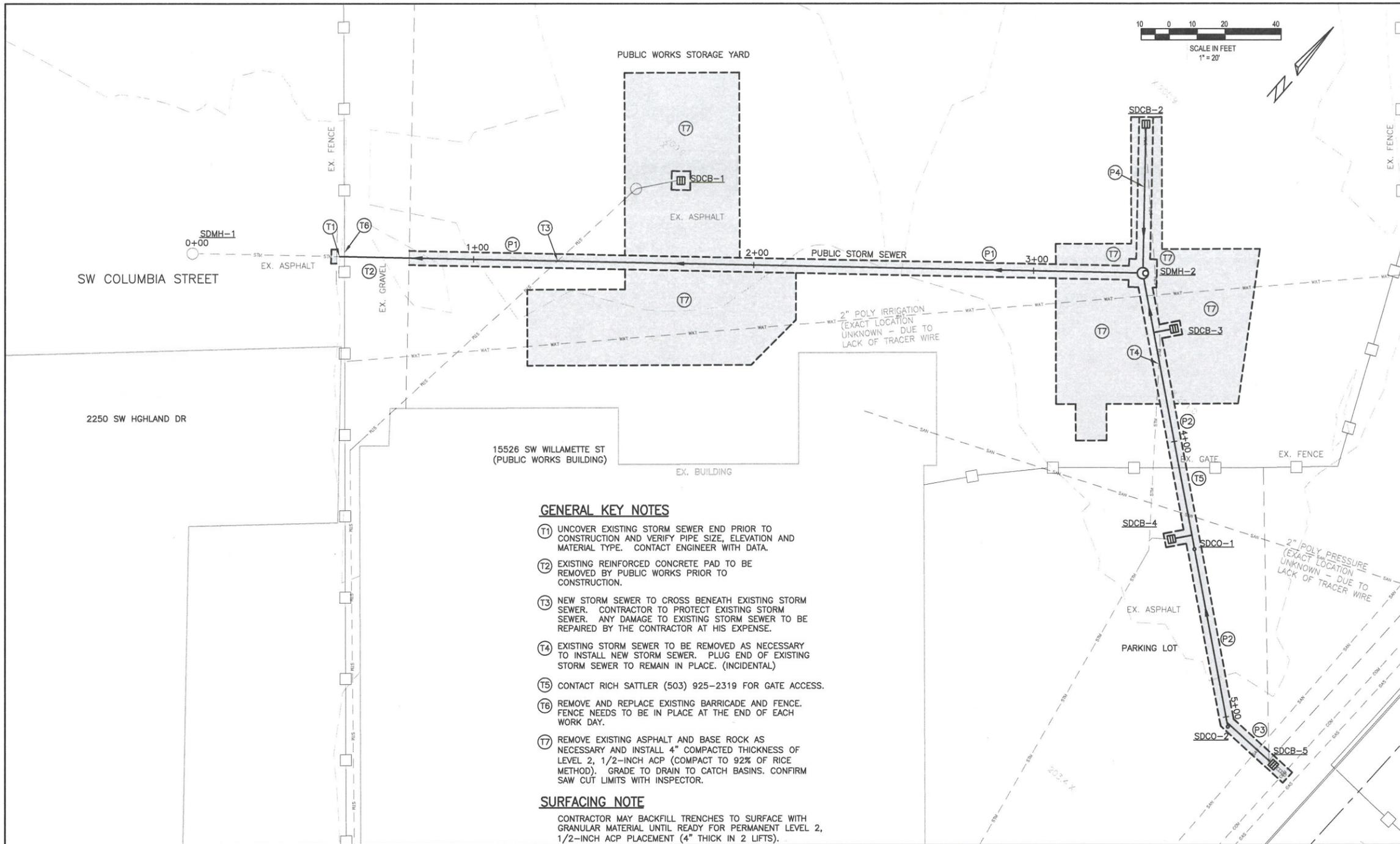
CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 SHERWOOD, OREGON 97140
 PHONE: (503) 925-2309
 E-MAIL: engineering@sherwoodoregon.gov



DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	1"=20"
DATE:	MARCH, 2019
2019 PAVING_SHEETS.DWG	

NO.	DESCRIPTION

JOB NO. _____
 SHEET NO. **5**
 OF **9**



STORM SEWER KEY NOTES

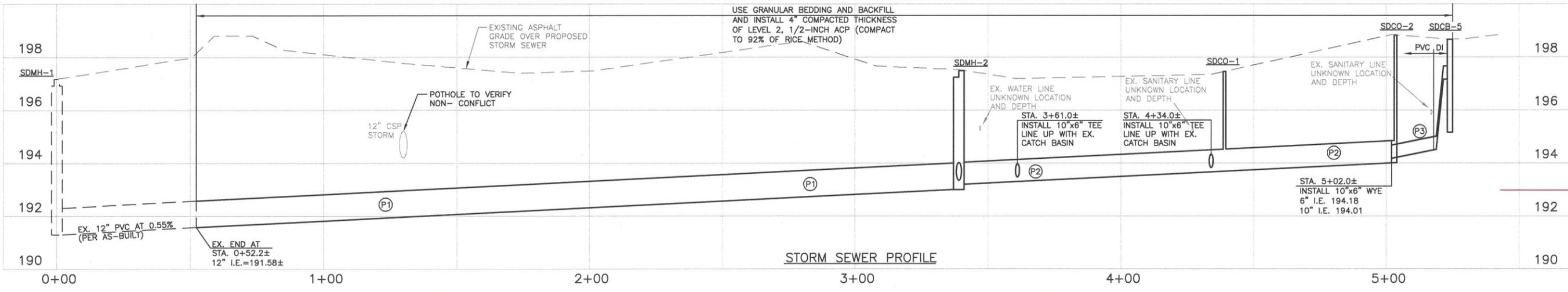
- SDMH-1**
STA. 0+00.00
EX. STORM MANHOLE
RIM EL.=197.17 (ASSUMED ELEVATION)
12" I.E. IN=191.30 (NE)
- SDMH-2**
STA. 3+39.0±
INSTALL SHALLOW STORM MANHOLE WITH FRAME AND COVER
RIM EL.=197.52± (MATCH A.C.)
8" I.E. IN=193.35(NW)
10" I.E. IN=193.20(SE)
12" I.E. IN=193.00(SW)
- SDCB-1**
REMOVE EXISTING CATCH BASIN AND INSTALL GIBSON CATCH BASIN DETAIL T6 OR APPROVED EQUIVALENT
RIM EL.=196.66
10" I.E. OUT=194.99 (SW)
CONNECT TO EXISTING STORM SEWER (INCIDENTAL)
- SDCB-2**
REMOVE EXISTING CATCH BASIN AND INSTALL GIBSON CATCH BASIN DETAIL T5 OR APPROVED EQUIVALENT
RIM EL.=196.83
8" I.E. OUT=195.16 (SE)
- SDCB-3**
REMOVE EXISTING CATCH BASIN AND INSTALL GIBSON CATCH BASIN DETAIL T5 OR APPROVED EQUIVALENT
RIM EL.=197.03
6" I.E. OUT=195.53 (SW)
- SDCB-4**
REMOVE EXISTING CATCH BASIN AND INSTALL GIBSON CATCH BASIN DETAIL T5 OR APPROVED EQUIVALENT
RIM EL.=197.03
6" I.E. OUT=195.53 (NE)
PLUG END OF EXISTING SEWER (INCIDENTAL)
- SDCB-5**
REMOVE EXISTING STRIP DRAIN AND INSTALL GIBSON CATCH BASIN DETAIL T5 OR APPROVED EQUIVALENT
RIM EL.=198.65
6" I.E. OUT=197.15 (W)
- SDCO-1**
STA. 4+39.00±
INSTALL 10"x8" CLEANOUT
RIM TO MATCH A.C.
- SDCO-2**
STA. 5+03.00±
INSTALL 10"x8" CLEANOUT
RIM TO MATCH A.C.
- (P1)** INSTALL 12" PVC D3034, SDR 35 STORM SEWER
L=284.8' S=0.50%
- (P2)** INSTALL 10" PVC D3034, SDR 35 STORM SEWER
L=161.0' S=0.50%
- (P3)** INSTALL 6" PVC D3034, SDR 35 STORM SEWER
L=21.0' (OVERALL LENGTH)
S=2.00% TO VERTICAL BEND
- (P4)** INSTALL 8" PVC D3034, SDR 35 STORM SEWER
L=50.0' (OVERALL LENGTH)
S=0.50% TO VERTICAL BEND

GENERAL KEY NOTES

- (T1)** UNCOVER EXISTING STORM SEWER END PRIOR TO CONSTRUCTION AND VERIFY PIPE SIZE, ELEVATION AND MATERIAL TYPE. CONTACT ENGINEER WITH DATA.
- (T2)** EXISTING REINFORCED CONCRETE PAD TO BE REMOVED BY PUBLIC WORKS PRIOR TO CONSTRUCTION.
- (T3)** NEW STORM SEWER TO CROSS BENEATH EXISTING STORM SEWER. CONTRACTOR TO PROTECT EXISTING STORM SEWER. ANY DAMAGE TO EXISTING STORM SEWER TO BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (T4)** EXISTING STORM SEWER TO BE REMOVED AS NECESSARY TO INSTALL NEW STORM SEWER. PLUG END OF EXISTING STORM SEWER TO REMAIN IN PLACE. (INCIDENTAL)
- (T5)** CONTACT RICH SATTLER (503) 925-2319 FOR GATE ACCESS.
- (T6)** REMOVE AND REPLACE EXISTING BARRICADE AND FENCE. FENCE NEEDS TO BE IN PLACE AT THE END OF EACH WORK DAY.
- (T7)** REMOVE EXISTING ASPHALT AND BASE ROCK AS NECESSARY AND INSTALL 4" COMPACTED THICKNESS OF LEVEL 2, 1/2-INCH ACP (COMPACT TO 92% OF RICE METHOD). GRADE TO DRAIN TO CATCH BASINS. CONFIRM SAW CUT LIMITS WITH INSPECTOR.

SURFACING NOTE

CONTRACTOR MAY BACKFILL TRENCHES TO SURFACE WITH GRANULAR MATERIAL UNTIL READY FOR PERMANENT LEVEL 2, 1/2-INCH ACP PLACEMENT (4" THICK IN 2 LIFTS).



**STORM SEWER
(PLAN AND PROFILE)**

2019 PAVEMENT REHAB PROJECTS

LOCATED IN SECTIONS 32B AND 32A, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

**CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140**

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

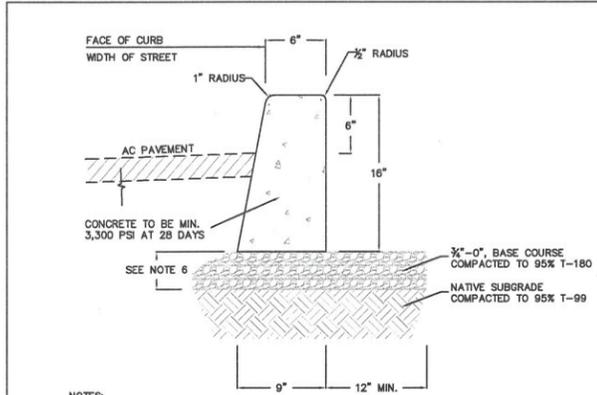
3-19-19

DESIGNED BY: CCC
DRAWN BY: CCC/TB
CHECKED BY: RSV/S
FULL SIZE SCALE: 1"=20'
DATE: MARCH, 2019
2019 PAVING_SHEETS.DWG

NO.	DATE	REVISIONS

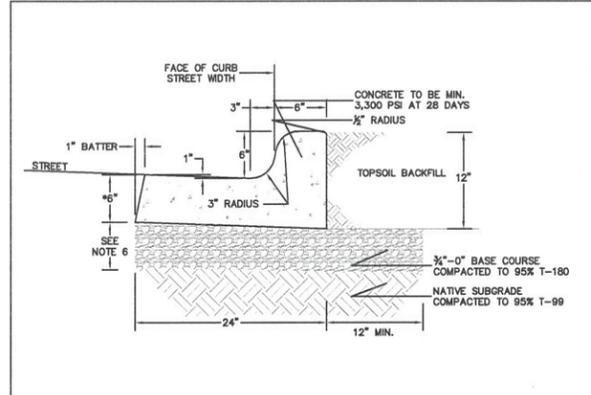
JOB NO. _____

SHEET NO. **6** OF **9**



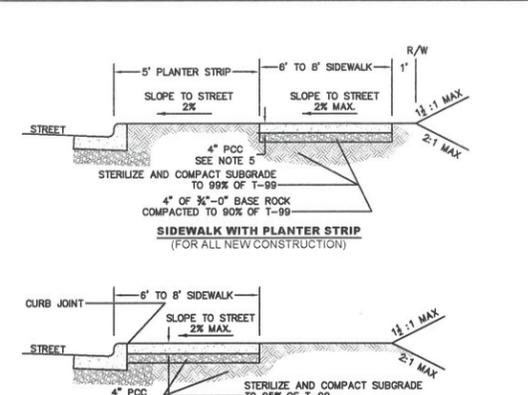
- NOTES:
- VERTICAL CURB TO BE USED AT MEDIANS AND MEDIAN PLANTING STRIPS, OR IN REPLACEMENT OF DAMAGED EXISTING VERTICAL CURBS.
 - CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI AT 28 DAYS.
 - EXPANSION JOINTS TO BE PROVIDED: AT POINT OF TANGENCY OF THE CURB, AT EACH COLD JOINT, AT THE SIDE OF INLET STRUCTURES, AT THE ENDS OF DRIVEWAYS AND AT LOCATIONS NECESSARY TO LIMIT SPACING TO 45 FEET.
 - MATERIAL TO BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2 INCH.
 - CONTRACTION JOINTS SHALL NOT BE SPACED MORE THAN 15 FEET AND SHALL BE 1/2" IN DEPTH.
 - BASE ROCK: 3/4"-0", COMPACTED TO 95% MAX DENSITY. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURE OR 6" IN DEPTH, WHICHEVER IS GREATER.

STANDARD DRAWING TITLE		DRAWING NUMBER
VERTICAL CURB		RD-21
SCALE	DATE	
N.T.S.	MAR '16	



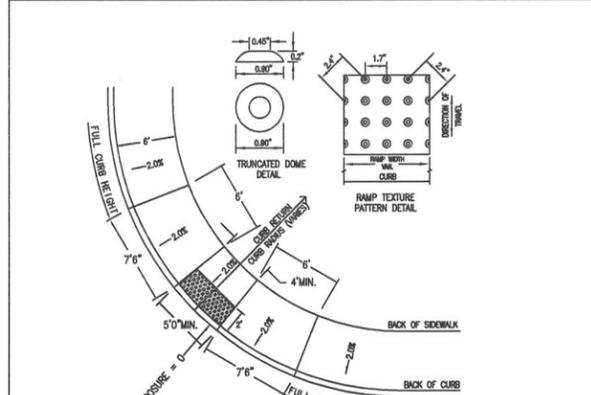
- NOTES:
- MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
 - CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3,300 PSI, WITH A 4" MAX SLUMP.
 - EXPANSION JOINTS TO BE PROVIDED AT EACH:
 - POINT OF TANGENCY.
 - COLD JOINT.
 - SIDE OF INLET STRUCTURES.
 - CONTRACTION JOINTS SHALL HAVE:
 - SPACING OF NOT MORE THAN 15 FEET.
 - DEPTH OF JOINT OF AT LEAST 1/2".
 - BASE ROCK SHALL BE 3/4"-0", COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 6", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.
 - FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23
 - COMMERCIAL DRIVEWAY DROPS SHALL BE 8" THICK, RE-BAR REINFORCED, AND 4,000 PSI AT 28 DAYS.

STANDARD DRAWING TITLE		DRAWING NUMBER
MONOLITHIC CURB AND GUTTER		RD-22
SCALE	DATE	
N.T.S.	MAR '16	



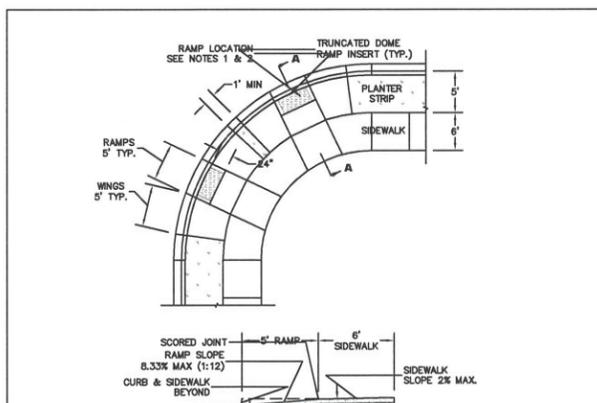
- NOTES:
- CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI @ 28 DAYS, WITH A 4" MAX SLUMP.
 - SIDEWALK PANELS TO BE SQUARE (6' LONG x 6' WIDE TYP.).
 - EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, CURB JOINTS, AND/OR POINTS OF TANGENCY IN CURB AS SHOWN ON THE STANDARD DRAWINGS FOR SIDEWALK RAMP, AND AT SPACING NOT TO EXCEED 45'.
 - FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROWELED JOINT WITH A MINIMUM 1/2" RADIUS.
 - SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4". IF MOUNTABLE CURB IS USED, OR IF SIDEWALK IS INTENDED AS PORTION OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6" MINIMUM THICKNESS, COMMERCIAL 8".
 - CONCRETE SHALL HAVE A BROOM FINISH. ALL JOINTS SHALL BE EDGED WITH 3" SHINE.
 - WIDTH OF PLANTER STRIP AND SIDEWALK IS MEASURED FROM FACE OF CURB.
 - IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 1" PAST BACK OF SIDEWALK WITH A 3" DIAMETER ADS PIPE. CONTRACTION JOINT SHALL BE PLACE OVER PIPE.

STANDARD DRAWING TITLE		DRAWING NUMBER
SIDEWALK DETAIL		RD-26
SCALE	DATE	
N.T.S.	MAR '16	



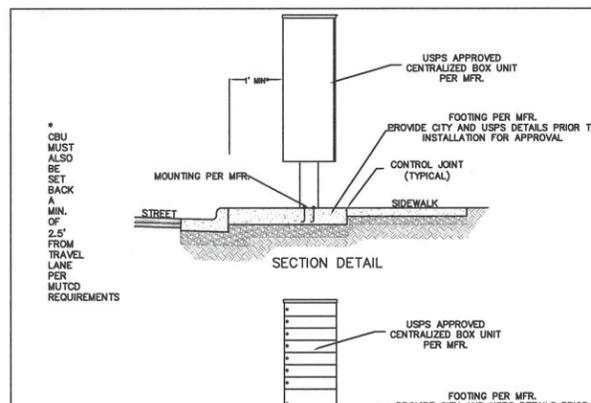
- NOTES:
- PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMP AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
 - PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
 - SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
 - PLANTER STRIP DETAIL - REFER TO STD STANDARD DRAWING RD 755 (SIDEWALK RAMP DETAIL).
 - DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
 - CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
 - CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI WITH A 4" MAX SLUMP.
 - 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
 - BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
 - SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
 - CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
 - PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

STANDARD DRAWING TITLE		DRAWING NUMBER
CURB-TIGHT CURB RETURN & RAMP DETAIL		RD-43
SCALE	DATE	
N.T.S.	MAR '16	



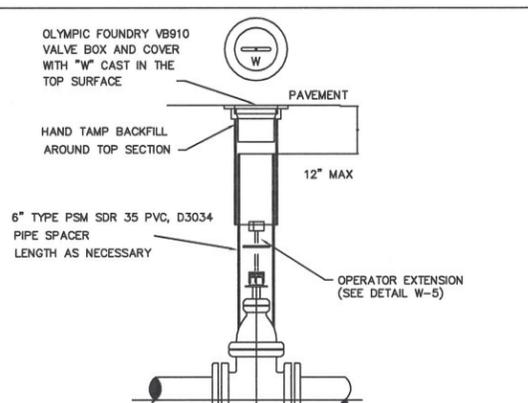
- NOTES:
- PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMP AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
 - PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
 - SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
 - DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
 - CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
 - CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH A 4" MAX SLUMP.
 - 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
 - BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
 - SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
 - CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
 - PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

STANDARD DRAWING TITLE		DRAWING NUMBER
DOUBLE CROSSING CURB RETURN & RAMP DETAIL		RD-44
SCALE	DATE	
N.T.S.	MAR '16	



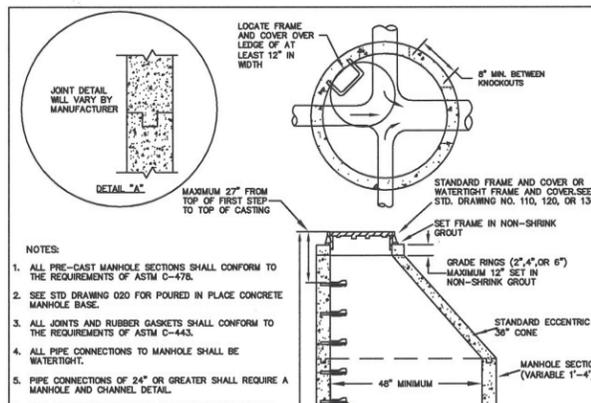
- NOTE:
- SHOULD A CENTRALIZED BOX UNIT (CBU) BE INSTALLED BEHIND A CURBTIGHT SIDEWALK, THE FRONT OF THE CBU SHALL NOT PROJECT BEHIND THE BACK OF SIDEWALK. A SIDEWALK CLEARANCE APRON SHALL BE INSTALLED PER RD-82.
 - SHOULD A CBU BE INSTALLED WITH THE FACE OF THE UNIT PERPENDICULAR TO SIDEWALK SUCH AS IN A PLANTER STRIP, THE CONCRETE FOOTING SHALL BE INSTALLED THE FULL WIDTH OF THE PLANTER STRIP AND LONG ENOUGH TO PROVIDE USERS A MIN. 3' OF PAVED CLEARANCE IN FRONT OF AND 1' PAVED CLEARANCE BEHIND THE CBU TO PROVIDE USER AND USPS ACCESS.
 - MAILBOX PLACEMENT SPECIFICATIONS:
 - PLACEMENT AND INSTALLATION SHALL MEET ALL CURRENT CITY, USPS, MUTED AND ADA GUIDELINES.
 - PLACEMENT SHALL BE APPROVED BY BOTH THE USPS AND CITY BEFORE INSTALLATION.
 - CBU SHOULD FACE SIDEWALK. CBU SHALL NOT PROTRUDE INTO THE VERTICAL PLANE OF THE SIDEWALK.
 - CBU SHALL NOT BE INSTALLED WITHIN THE LIMITS OF AN INTERSECTION SIGHT DISTANCE TRIANGLE.
 - WIDENED SIDEWALK CLEARANCE APRON OUTSIDE OF PUBLIC RIGHT OF WAY SHALL HAVE A SIDEWALK EASEMENT DEDICATED TO THE CITY OF SHERWOOD.
 - DEVELOPER SHALL REFERENCE THE LATEST EDITION OF THE USPS "DEVELOPERS GUIDE TO CENTRALIZED BOX UNITS" FOR CBU PLACEMENT GUIDELINES. IF MANUFACTURER'S SPECIFICATIONS FOR THE FOOTING AND MOUNTING GUIDELINES ARE NOT AVAILABLE, THE DEVELOPER SHALL REFER TO THE ABOVE MENTIONED USPS CBU GUIDE FOR GENERAL MOUNTING AND FOOTING SPECIFICATIONS.

STANDARD DRAWING TITLE		DRAWING NUMBER
MAILBOX - CENTRALIZED BOX UNITS		RD-60
SCALE	DATE	
N.T.S.	MAR '16	



- NOTES:
- VALVE BOX NOT TO REST ON OPERATING ASSEMBLY
 - OPERATOR EXTENSION REQUIRED WHEN VALVE NUT IS DEEPER THAN 3 FEET FROM FINISHED GRADE
 - CENTER VALVE BOX ON AXIS OF OPER NUT
 - PROVIDE 24" SQUARE BY 4" THICK CONCRETE PAD AROUND VALVE BOX OUTSIDE OF PAVED AREAS
 - VALVE BOX COVER SHALL BE A MINIMUM OF 2 1/2" IN LENGTH

STANDARD DRAWING TITLE		DRAWING NUMBER
TYPICAL GATE VALVE SETTING DETAIL		W-3
SCALE	DATE	
N.T.S.	JUL '09	



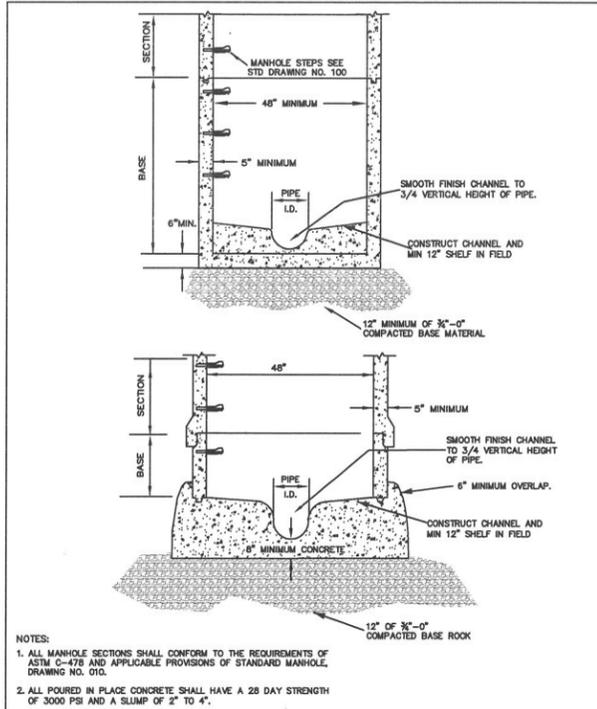
- NOTES:
- ALL PRE-CAST MANHOLE SECTIONS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C-478.
 - SEE STD DRAWING 020 FOR Poured IN PLACE CONCRETE MANHOLE BASE.
 - ALL JOINTS AND RUBBER GASKETS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C-443.
 - ALL PIPE CONNECTIONS TO MANHOLE SHALL BE WATERTIGHT.
 - PIPE CONNECTIONS OF 24" OR GREATER SHALL REQUIRE A MANHOLE CONNECTION AND CHANNEL DETAIL.
 - PIPE CONNECTIONS OF FOUR OR MORE MAINLINES SHALL REQUIRE A MANHOLE CONNECTION AND CHANNEL DETAIL.
 - PROVIDE A MINIMUM OF 8" OF INTACT (UNDISTURBED) MANHOLE WALL BETWEEN PIPE BREAKOUTS AS MEASURED ON THE INSIDE FACE OF THE MANHOLE.
 - BREAKOUT OF WALL FOR PIPE SHALL BE 2" MINIMUM AND 4" MAXIMUM CLEAR OF PIPE WALL.
 - THIS DETAIL LIMITED TO MAXIMUM INTERIOR DROP OF 12" FOR SANITARY CONNECTION AND 48" FOR STORM CONNECTION.
 - WATERTIGHT/TAMPER PROOF MANHOLE FRAME AND COVER SHALL BE USED IN ALL EASEMENT AND OFF STREET AREAS. SEE STD DRAWING 130.
 - ALL JOINTS AND RUBBER GASKETS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C-443.

STANDARD DRAWING TITLE		DRAWING NUMBER
STANDARD MANHOLE		W-3
SCALE	DATE	
N.T.S.	JUL '09	



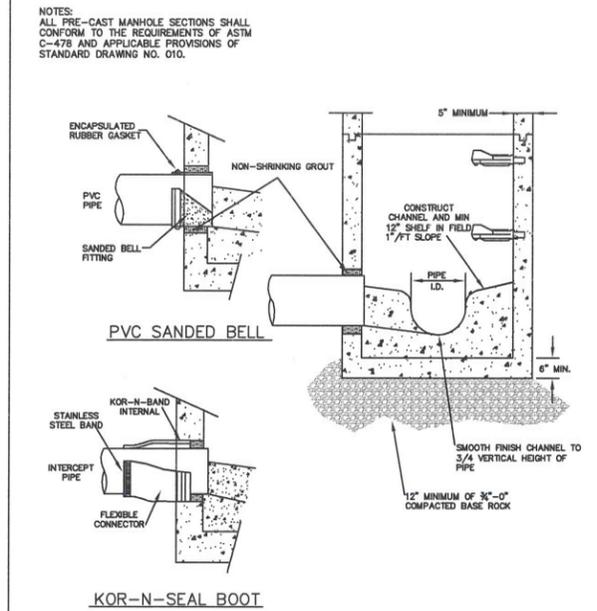
DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/VCS
FULL SIZE SCALE:	N/A
DATE:	MARCH, 2019
2019 PAVING SHEETS.DWG	

REVISIONS	
JOB NO.	
SHEET NO.	7
	9



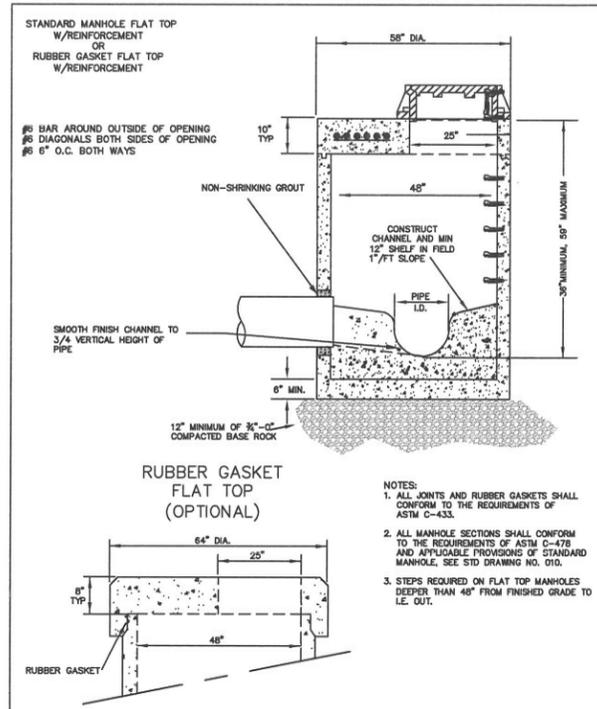
MANHOLE BASE

DRAWING NO. 020 REVISED 11-16



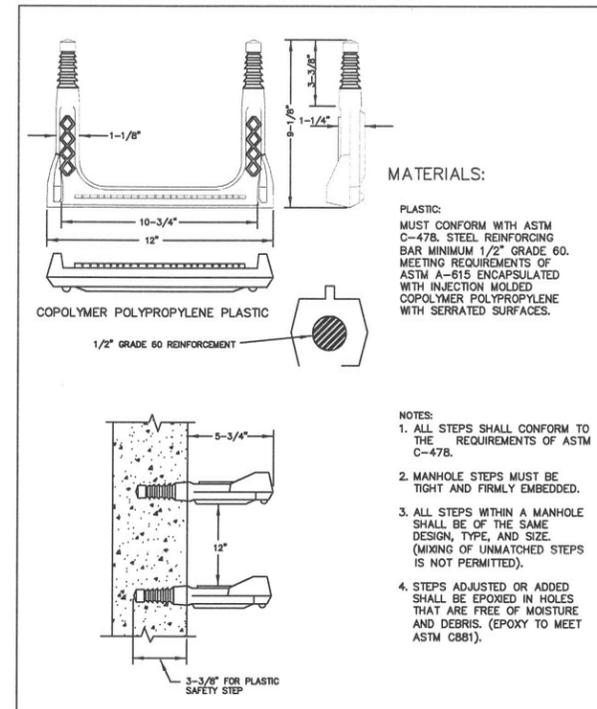
MANHOLE CONNECTIONS

DRAWING NO. 030 REVISED 12-16



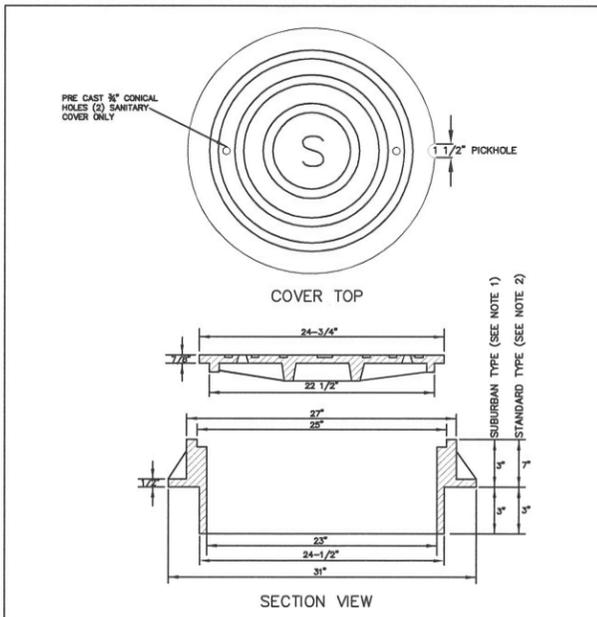
FLAT TOP MANHOLE

DRAWING NO. 050 REVISED 12-16



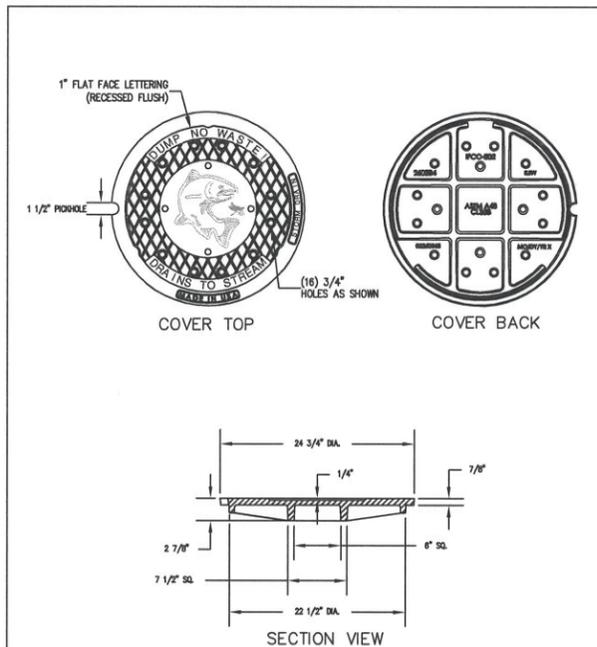
MANHOLE STEP

DRAWING NO. 100 REVISED 12-16



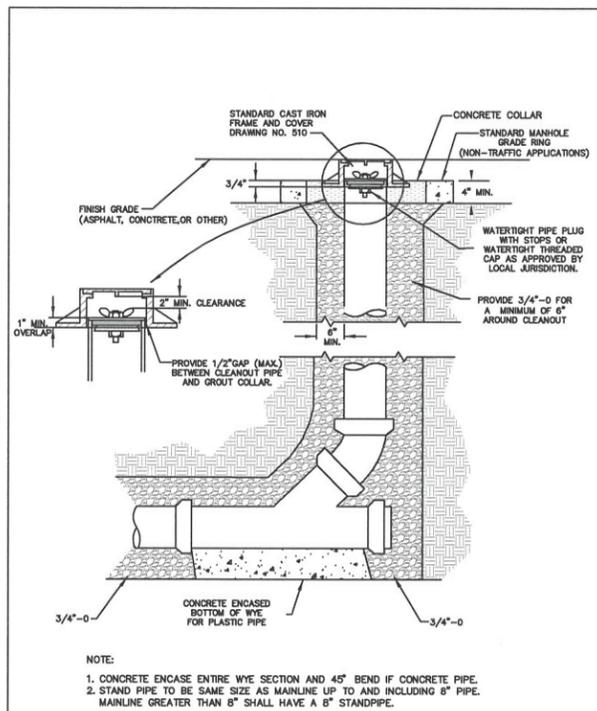
SUBURBAN AND STANDARD MANHOLE FRAME AND COVER SANITARY

DRAWING NO. 110 REVISED 02-17



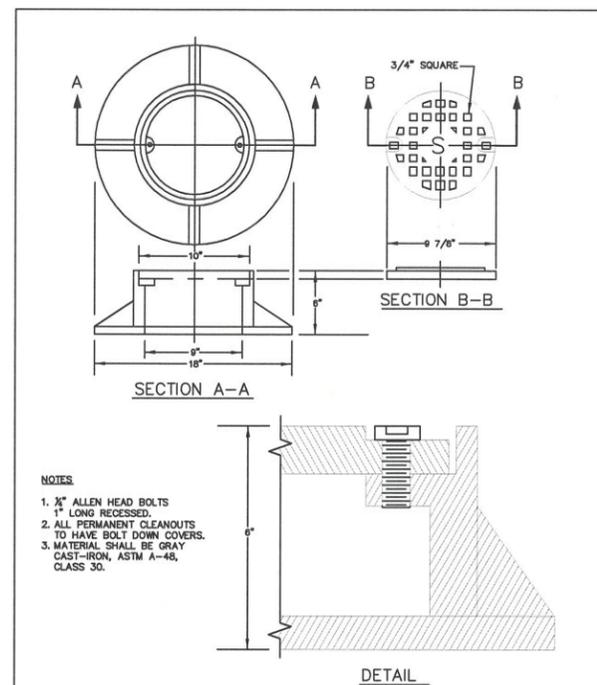
STORM WATER MANHOLE LID

DRAWING NO. 120 REVISED 12-08



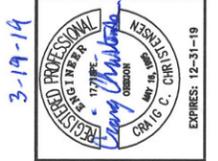
STANDARD CLEANOUT

DRAWING NO. 500 REVISED 01-13



CLEANOUT FRAME AND COVER

DRAWING NO. 510 REVISED 12-16



DESIGNED BY:	CCC	CCC/TB	RS/CS	N/A	MARCH, 2019
DRAWN BY:					
CHECKED BY:					
FULL SIZE SCALE:					
DATE:					

2019 PAVING_SHEETS.DWG	REVISIONS

