

CITY OF SHERWOOD

2018 PAVEMENT OVERLAY PROJECTS

AUGUST 2018

(BID DOCUMENT VOLUME 2 OF 2 - CONTRACT DRAWINGS)

COVER SHEET

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 288 AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov



8-23-18

DESIGNED BY:	CCC	CCC/TB	RS/CS	AS NOTED	AUGUST, 2018
DRAWN BY:					
CHECKED BY:					
FULL SIZE SCALE:					
DATE:					
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REVISIONS	
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JOB NO.	
SHEET NO.	1
	of 13

PROJECT LOCATION:

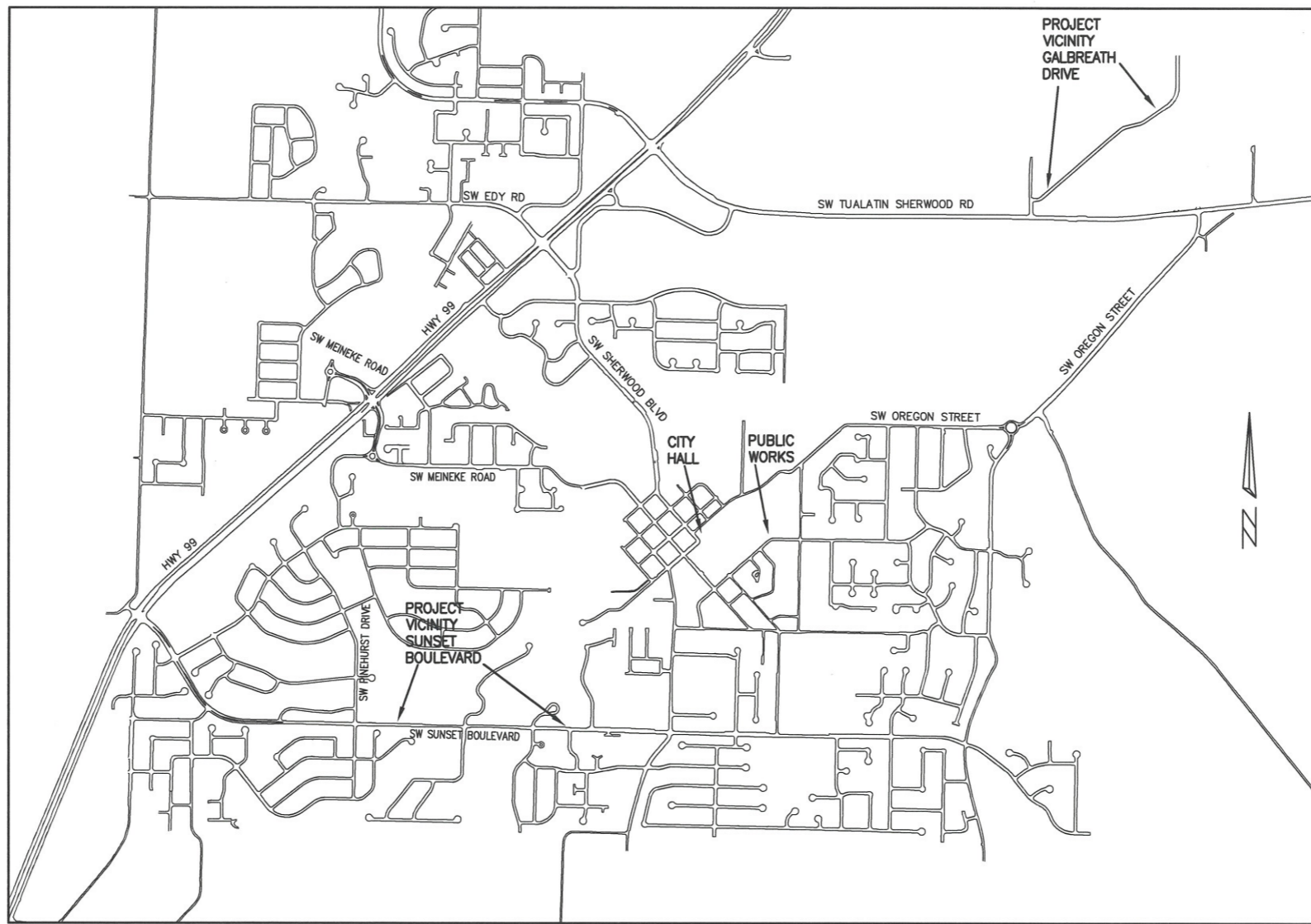
SW SUNSET BOULEVARD BETWEEN
SW RICHEN PARK TERRACE AND SW EUCALYPTUS TERRACE
AND SW GALBREATH DRIVE BETWEEN
SW GERDA LANE AND POWER LINES

DEVELOPER/OWNER:

CITY OF SHERWOOD
22560 SW PINE ST
SHERWOOD, OREGON 97140
CONTACT: RICH SATTLER
PH. 503-925-2319
SATTLERR@SHERWOODOREGON.GOV

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VICINITY MAP
NOT TO SCALE

INSPECTOR INFORMATION:

CITY OF SHERWOOD: RICH SATTLER (503) 925-2319
CONTACT INSPECTOR 48 HOURS PRIOR TO CONSTRUCTION.

SURVEY INFORMATION:

HORIZONTAL
AERIAL TOPOGRAPHIC MAP (2004) AND VISUAL OBSERVATION.

VERTICAL
AERIAL TOPOGRAPHIC MAP (2004) WITH ADDITIONAL LEVEL INFORMATION PERFORMED AT CRITICAL LOCATIONS.

THIS DESIGN COMPLIES WITH ORS 92.044 (7) IN THAT NO UTILITY INFRASTRUCTURE IS DESIGNED TO BE WITHIN ONE (1) FOOT OF A SURVEY MONUMENT LOCATION SHOWN ON A SUBDIVISION OR PARTITION PLAT. NO DESIGN EXCEPTIONS NOR FINAL FIELD LOCATION CHANGES SHALL BE PERMITTED IF THAT CHANGE WOULD CAUSE ANY UTILITY INFRASTRUCTURE TO BE PLACED WITHIN THE PROHIBITED AREA.

ATTENTION EXCAVATORS: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-011-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BEFORE COMMENCING ANY EXCAVATION. CALL (503) 246-6699.

GENERAL NOTES

- ALL CONSTRUCTION WORK, MATERIALS AND TESTING SHALL CONFORM TO THE APPLICABLE SECTION OF THE CITY OF SHERWOOD ENGINEERING DESIGN AND STANDARD DETAILS MANUAL. CONTRACTOR AND SUBCONTRACTOR(S) SHALL HAVE A MINIMUM OF ONE SET OF APPROVED PLANS AND CITY OF SHERWOOD CONSTRUCTION SPECIFICATIONS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROCURE ALL APPLICABLE PERMITS, LICENSES, AND CERTIFICATES RELATIVE TO THE TRADES TO COMPLETE THE PROJECT AND FOR THE USE OF SUCH WORK WHEN COMPLETED. COMPLIANCE SHALL BE AT ALL LEVELS, FEDERAL, STATE, COUNTY, AND LOCAL, RELATING TO THE PERFORMANCE OF THIS WORK. A CITY OF SHERWOOD OR A METRO BUSINESS LICENSE IS REQUIRED FOR THE CONTRACTOR AND ALL SUBCONTRACTORS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION AND ARRANGE FOR PROTECTION OF UTILITIES IN CONFLICT WITH PROPOSED WORK. THE LOCATIONS, DEPTH AND DESCRIPTION OF EXISTING UTILITIES SHOWN WERE COMPILED FROM AVAILABLE RECORDS AND/OR FIELD SURVEYS. THE ENGINEER OR UTILITY COMPANIES DO NOT GUARANTEE THE ACCURACY OR THE COMPLETENESS OF SUCH RECORDS. ADDITIONAL UTILITIES MAY EXIST WITHIN THE WORK AREA. POT-HOLING PRIOR TO CONSTRUCTION MAY BE NECESSARY. ALL WORK TO VERIFY UTILITY LOCATIONS AND DEPTHS IS INCIDENTAL.
- OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT WWW.STATE.OR.US/GOVERNMENT.HTM. THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL (503) 246-6699.
- THE CONTRACTOR SHALL MAKE PROVISIONS TO KEEP ALL EXISTING UTILITIES IN SERVICE AND PROTECT THEM DURING CONSTRUCTION. CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGED UTILITIES USING MATERIAL AND METHODS APPROVED BY THE UTILITY OWNER. NO SERVICE INTERRUPTIONS SHALL BE PERMITTED WITHOUT PRIOR WRITTEN AGREEMENT WITH THE UTILITY PROVIDER AND NOTIFICATION TO SERVICE CUSTOMER. PLEASE NOTE THE STORM SEWER IN THIS AREA IS SHALLOW - DO NOT DAMAGE. REPAIR TO UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PRUNE ALL VEGETATION, AS NECESSARY FOR EQUIPMENT CLEARANCES AWAY AND UP FROM THE STREET AND SIDEWALK. AS WELL AS ANY ROOT PRUNING AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL PROTECT ALL EXISTING LANDSCAPING THAT IS TO REMAIN. DAMAGE TO TREES AND SHRUBS IS THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR. COST OF TREE PRUNING IS INCIDENTAL.
- CONTRACTOR SHALL NOTIFY THE CITY OF SHERWOOD 48 HOURS IN ADVANCE OF STARTING CONSTRUCTION AND 24 HOURS BEFORE RESUMING WORK AFTER SHUTDOWNS, EXCEPT FOR NORMAL RESUMPTION OF WORK FOLLOWING SATURDAYS, SUNDAYS, OR HOLIDAYS.
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, EXCESS TOPSOIL, AND OTHER EXCESS MATERIAL OFF SITE. MATERIALS SHALL BE DISPOSED OF IN SUCH A MANNER AS TO MEET ALL APPLICABLE REGULATIONS. DISPOSAL LOCATION INFORMATION TO BE PROVIDED TO CITY PRIOR TO BEGINNING ANY WORK.
- CONSTRUCTION VEHICLES SHALL PARK AT A LOCATION APPROVED BY THE INSPECTOR OR AT A LOCATION(S) INDICATED ON AN APPROVED PLAN. HOURS OF CONSTRUCTION SHALL BE AS SPECIFIED IN THE PLANS - UNLESS OTHERWISE APPROVED BY THE INSPECTOR. CONSTRUCTION IS PROHIBITED ON SATURDAY AND SUNDAY UNLESS WRITTEN APPROVAL IS OBTAINED FROM THE PUBLIC WORKS DIRECTOR AND CITY MANAGER. CONSTRUCTION ACTIVITIES INCLUDE ALL FIELD MAINTENANCE OF EQUIPMENT, REFUELING, AND PICK UP AND DELIVERY OF EQUIPMENT AS WELL AS THE ACTUAL CONSTRUCTION ACTIVITY.
- AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN UP THE PROJECT AREA AND LEAVE IT IN A NEAT AND SECURED MANNER. UPON COMPLETION, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL.
- THE CONTRACTOR SHALL KEEP AN APPROVED SET OF PLANS ON THE PROJECT SITE AT ALL TIMES WITH ALL KNOWN CHANGES REDLINED ON THE WORKING PLAN SET FOR THE SOLE PURPOSE OF RECORDING AS BUILT INFORMATION OF IMPROVEMENTS AND LOCATION AND DEPTH OF ANY EXISTING UTILITIES ENCOUNTERED.
- ANY ALTERATIONS OR VARIATIONS FROM THESE PLANS EXCEPT MINOR FIELD ADJUSTMENTS NEEDED TO MEET EXISTING FIELD CONDITIONS, SHALL BE APPROVED BY THE ENGINEER AND APPLICABLE REGULATORY AGENCY REPRESENTATIVE. CONTRACTOR SHALL KEEP RECORD OF ALL CHANGES AND NOTE ON AS BUILT PLANS. CONTRACTOR SHALL SUBMIT ACCURATE AND COMPLETE "AS-BUILT" INFORMATION TO THE ENGINEER UPON COMPLETION OF CONSTRUCTION.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THIS PROJECT IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS INCLUDING SUCH INCIDENTALS AS MAY BE NECESSARY TO MEET THE INTENT OF THE PROJECT CONTRACT DOCUMENTS, APPLICABLE AGENCY REQUIREMENTS AND OTHER WORK AS NECESSARY TO PROVIDE A COMPLETE PROJECT.
- CONTRACTOR SHALL PROVIDE EFFECTIVE EROSION PROTECTION TO INCLUDE, BUT NOT BE LIMITED TO, GRADING, DITCHING, STRAW WATTLES, TRIANGULAR SILT DAMS, SILT FENCING, AND SEDIMENT BARRIERS TO MINIMIZE EROSION AND IMPACT TO ADJACENT PROPERTY. SEE EROSION AND SEDIMENT CONTROL NOTES AND PLANS.
- THE CONTRACTOR SHALL MAINTAIN AND COORDINATE ACCESS TO ALL AFFECTED PROPERTIES. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS A MINIMUM OF 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURES AND ASSIST PROPERTY OWNER WITH ALTERNATIVE PARKING AND ACCESS. PEDESTRIAN ACCESS TO ENTRANCES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL PROVIDE RAMPS OR OTHER APPROVED METHODS FOR MAINTAINING ACCESS TO ENTRANCES. RAMPS SHALL BE ADA COMPLIANT, APPROVED BY THE ENGINEER, AND INCIDENTAL TO THE CONTRACT. RESIDENTIAL DRIVEWAYS MAY ONLY BE CLOSED FOR A MAXIMUM OF ONE DAY (7:00 AM TO 6:00 PM).
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND VERIFY ALL EXISTING CONDITIONS BEFORE THE START OF WORK. THE CONTRACTOR SHALL TAKE ALL NECESSARY FIELD MEASUREMENTS AND OTHERWISE VERIFY ALL DIMENSIONS AND EXISTING CONSTRUCTION CONDITIONS INDICATED AND/OR SHOWN ON THE PLANS. SHOULD ANY ERROR OR INCONSISTENCY EXIST, THE CONTRACTOR SHALL NOT PROCEED WITH THE WORK AFFECTED UNTIL REPORTED TO THE PROJECT ENGINEER FOR CLARIFICATION OR CORRECTION.
- ANY INSPECTION BY THE CITY, COUNTY, STATE, FEDERAL AGENCY OR PROJECT ENGINEER SHALL NOT, IN ANY WAY, RELIEVE THE CONTRACTOR FROM ANY OBLIGATION TO PERFORM THE WORK IN COMPLIANCE WITH THE APPLICABLE CODES, REGULATIONS, CITY STANDARDS AND PROJECT CONTRACT DOCUMENTS.
- CONTRACTOR TO VERIFY CENTERLINE AND GUTTER SLOPES PRIOR TO CONSTRUCTION TO ENSURE COMPLIANCE WITH THE CONSTRUCTION DRAWINGS AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN CONFORMING TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION AND ACCORDING TO PROJECT SPECIFICATIONS. TRAFFIC CONTROL PLAN SHALL MATCH APPROVED PHASING/SEQUENCING PLAN AND SHALL BE SUBJECT TO THE APPROVAL BY THE CITY, WASHINGTON COUNTY AND/OR ODOT AS APPLICABLE.
- THE CONTRACTOR SHALL PROVIDE TO THE CITY PROJECT MANAGER AND INSPECTOR A 24 HOUR CONTACT PERSON AND CELL PHONE NUMBER.
- NO TRENCHES OR PITS WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT. ALL TRENCHES AND PITS SHALL BE COVERED WITH STEEL PLATES OR FILLED IN AT NIGHT.
- PROPERTY LINES SHOWN ON ALL PLAN SHEETS ARE FOR GENERAL DELINEATION ONLY AND ARE, BY NO MEANS, MEANT TO REPRESENT THE ACTUAL BOUNDARIES.

- CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL CONSTRUCTION SAFETY, HEALTH AND OTHER RULES AND REGULATIONS FROM OSHA, DEQ, STATE, AND LOCAL REGULATING AGENCIES FOR SAFETY AND INSTALLATION OF THE WORK INCLUDING BUT NOT LIMITED TO SHORING, BRACING, ERECTION/INSTALLATION, FALL PROTECTION, GUARDRAILS, ETC.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO REMOVE EXISTING AGGREGATE BASE AS NECESSARY TO CEMENT TREAT BASE AND ACHIEVE SURFACE BASE LEVEL FOR PLACEMENT OF NEW HMAC WHILE MEETING THE CURB AND DRIVEWAY EXPOSURE REQUIREMENTS IN THE PLANS.
- WHEN MATCHING EXISTING PAVEMENT, SAWCUT TO FIRM PAVEMENT. MINIMUM SAWCUT IS 1' FROM EXISTING EDGE OF PAVEMENT OR AS SHOWN ON PLANS. SAWCUT LOCATION TO BE APPROVED BY INSPECTOR.
- CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING STRUCTURES AND UTILITIES NOT SHOWN TO BE REMOVED. CONTRACTOR SHALL REPLACE OR REPAIR ANY EXISTING STRUCTURES (SIDEWALKS, DRIVEWAYS, CURB, FENCE, STREET TREES, ETC.) DAMAGED DURING CONSTRUCTION, IN ACCORDANCE WITH CITY STANDARDS.
- ALL LINENWORK SHOWN ON THE PLAN IS FROM A 2004 AERIAL MAP AND FROM VISUAL OBSERVATION AND IS THEREFORE NOT GUARANTEED FOR ACCURACY AND SHOULD NOT BE USED FOR OTHER PURPOSES.

EROSION CONTROL NOTES

- THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE IMPLEMENTATION OF THESE ESC PLANS AND CONSTRUCTION MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL JURISDICTION, AND VEGETATION/LANDSCAPING IS ESTABLISHED.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DOES NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE MORE THAN 1/3 THE BARRIER HEIGHT. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED AFTER PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSIDE DRAINAGE SYSTEM.
- STORM DRAIN INLETS, BASINS, AND AREA DRAINS SHALL BE PROTECTED UNTIL PAVEMENT SURFACES ARE COMPLETED AND/OR VEGETATION IS RE-ESTABLISHED.
- IF THERE ARE EXPOSED SOILS OR SOILS NOT FULLY ESTABLISHED FROM OCTOBER 1ST THROUGH MAY 31ST, THE WET WEATHER EROSION PREVENTION MEASURES WILL BE IN EFFECT. SEE THE EROSION PREVENTION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL (CHAPTER 4) OF CLEAN WATER SERVICES FOR REQUIREMENTS.
- THE CONTRACTOR SHALL REMOVE ESC MEASURES WHEN VEGETATION IS FULLY ESTABLISHED.
- ALL PUMPING OF SEDIMENT-LADEN WATER SHALL BE DISCHARGED OVER AN UNDISTURBED, PREFERABLY VEGETATED AREA, AND THROUGH A SEDIMENT CONTROL BMP (FILTER BAG, DIRT BAG, ETC.)
- SITE EROSION CONTROL PLAN AND BMP'S MEETING CWS STANDARDS TO BE IN PLACE AND APPROVED PRIOR TO CONSTRUCTION.
- TRACKING OF MATERIALS OUTSIDE OF THE WORK ZONE, ONTO ADJACENT STREETS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE AND CLEAN TO THE SATISFACTION OF THE INSPECTOR. THIS INCLUDES BUT IS NOT LIMITED TO SOIL, AGGREGATE, ASPHALT DEBRIS, ETC.
- CONTRACTOR TO PROVIDE FOR CONCRETE WASHOUT. SUBMIT PLAN FOR CONCRETE WASHOUT TO INSPECTOR FOR APPROVAL (INCIDENTAL).

WORK HOURS/TRAFFIC CONTROL NOTES

- WORK HOURS ARE FROM 8:00AM TO 6:00PM MONDAY THROUGH FRIDAY.
- SW SUNSET BOULEVARD AND SW CALBREATH DRIVE SHALL HAVE TWO LANES OPEN TO TRAFFIC AT ALL TIMES UNLESS FLAGGERS ARE PROVIDED FOR A SINGLE LANE CLOSURE. LANE CLOSURE MAY ONLY OCCUR BETWEEN THE HOURS OF 8:30AM TO 4:30PM.
- ALL DEAD END SIDE STREETS (GREENGATE DRIVE, GREENGATE PLACE AND REDFERN PLACE) SHALL HAVE TWO LANES OPEN TO TRAFFIC AT ALL TIMES UNLESS FLAGGERS ARE PROVIDED FOR A SINGLE LANE CLOSURE. LANE CLOSURE MAY ONLY OCCUR BETWEEN THE HOURS OF 8:30AM TO 4:30PM. REDFERN DRIVE MAY BE CLOSED WITH ADEQUATE ADVANCED SIGNAGE AND NOTIFICATION WITH THE APPROVAL OF THE INSPECTOR.
- TRENCHES WITHIN EXISTING ASPHALT PAVING SHALL HAVE EITHER ASPHALT COLD PATCH OR ASPHALT HOT PATCH (1" MIN. THICKNESS) AT THE END OF EACH WORK DAY PRIOR TO REOPENING THE STREET. PLATING IS ACCEPTABLE WITH THE APPROVAL OF THE INSPECTOR. COST IS INCIDENTAL.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EMERGENCY VEHICLE ACCESS TO ALL PROPERTIES AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS FOR MAIL AND TRASH (FRIDAY) AT ALL TIMES.
- TRAFFIC CONTROL SHALL INCLUDE ALL PAVEMENT MARKING REMOVAL, TEMPORARY PAVEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAVEMENT MARKINGS AND ALL INCIDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".
- CONTRACTOR IS RESPONSIBLE FOR THE IMPLEMENTATION OF THIS TRAFFIC CONTROL PLAN AND ITS CONTINUED FUNCTIONING FOR THE PROTECTION OF CONSTRUCTION WORKERS, VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIANS. ALL TRAFFIC CONTROL DEVICES/SIGNAGE SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. APPROVAL OF THE TRAFFIC CONTROL PLAN BY THE CITY OF SHERWOOD DOES NOT NEGATE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN A SAFE WORK ZONE. THE CITY OF SHERWOOD BEARS NO LIABILITY FOR THE CONTRACTOR'S IMPLEMENTATION OF THIS TRAFFIC CONTROL PLAN.

STREET NOTES

- IN AREAS WHERE CURB AND GUTTER EXISTS, NEW ASPHALT WILL MATCH FRONT EDGE OF CONCRETE GUTTER.
- TACK COAT SHALL BE APPLIED AGAINST CONCRETE GUTTER EDGE, CURB FACE, CONCRETE STRUCTURES, EXISTING ASPHALT EDGE, MANHOLE FRAMES, BLOW-OFF FRAMES, CLEANOUT FRAMES AND OTHER SURFACES OR STRUCTURES THAT WILL BE PAVED AGAINST WITH NEW ASPHALT (INCIDENTAL) PRIOR TO PAVING.
- HOT RUBBERIZED ASPHALT SEALER REQUIRED AT ASPHALT JOINTS (INCIDENTAL).
- PAVEMENT CROSS SLOPES SHALL BE AT LEAST 2.0%.
- NEW ASPHALT SHALL MATCH EDGE OF CONCRETE CATCH BASINS. THERE WILL BE NO ADJUSTING OF CATCH BASIN GRATES ALLOWED.
- NEW ASPHALT SHALL NOT COVER ANY WEEP HOLES (AT I.E. OR LOWER). CITY WILL PHOTO DOCUMENT PROJECT AREA INCLUDING EXISTING CURBS. COVERED WEEP HOLES SHALL BE CLEARED AT CONTRACTORS EXPENSE AND STREET REPAVED.
- TYPICAL FULL CURB EXPOSURE IS 6" AT TOP FACE OF CURB UNLESS OTHERWISE NOTED.
- TYPICAL DRIVEWAY CURB EXPOSURE IS 3/4" AT TOP FACE OF CURB.
- ADJUST ALL EXISTING VALVES, BLOW-OFFS AND OTHER MISCELLANEOUS BOXES AS NECESSARY TO MATCH NEW ASPHALT SURFACE GRADE - TO BE PAID FOR UNDER THE BID ITEM OF "ADJUSTING BOXES".
- MANHOLES THAT REQUIRE REMOVAL OF LOWER LEVEL OF ASPHALT TO ADD CONCRETE RINGS OR OTHERWISE ADJUST THE FRAME WILL BE PAID FOR UNDER THE BID ITEM "MINOR ADJUSTMENT OF MANHOLES". THERE WILL BE NO PAYMENT FOR MANHOLE FRAMES THAT ONLY NEED TO HAVE THE ASPHALT GRINDED AROUND AND NEW ASPHALT OVERLAY MATCHING THE FRAME.
- CONTRACTOR TO TEST ASPHALT SURFACE WITH 12-FOOT STRAIGHT EDGE IN TRAVEL LANES PARALLEL TO AND PERPENDICULAR TO THE CENTER LINE, AS DIRECTED BY THE INSPECTOR. THE ASPHALT SURFACE SHALL NOT VARY BY MORE THAN 1/4 INCH.
- EQUIPMENT PARKED ON SITE SHALL BE PARKED AWAY FROM AN INTERSECTION AT A LOCATION APPROVED BY THE INSPECTOR.
- WATER FOR CONSTRUCTION MAY BE OBTAINED FROM CITY FIRE HYDRANTS. INSTALL FIRE HYDRANT METER PRIOR TO DRAWING WATER. FIRE HYDRANT METER TO BE OBTAINED FROM CITY OF SHERWOOD WATER DEPARTMENT. COORDINATE WITH INSPECTOR. WATER FOR CONSTRUCTION IS INCIDENTAL.
- REPAIR ALL DISTURBED NEIGHBORING PROPERTY BACK TO ORIGINAL OR BETTER CONDITION.
- ANY SEGMENT OF STREET OPEN TO TRAFFIC THAT HAS A VERTICAL DROP (INCLUDING DRIVEWAYS) SHALL HAVE A TEMPORARY WEDGE INSTALLED. (INCIDENTAL).
- ANY EXISTING SURFACE TO REMAIN (CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, ETC.) THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED AND THE CONTRACTOR'S EXPENSE.
- TRAFFIC CONTROL SHALL INCLUDE BUT IS NOT LIMITED TO PAVEMENT MARKING REMOVAL, TEMPORARY PAVEMENT MARKING, BARRICADES, DELINEATORS, SIGNAGE, FLAGGERS, TEMPORARY PAVEMENT MARKINGS AND ALL INCIDENTALS NECESSARY TO COMPLETE THE WORK. TO BE PAID FOR UNDER THE BID ITEM "TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE".
- CONTRACTOR TO CLEAN UP JAGGED CONCRETE EDGES MEETING THE APPROVAL OF THE INSPECTOR. SOME SAWCUTTING MAY BE REQUIRED. COST IS INCIDENTAL.
- ANY HOLES IN THE EXISTING ASPHALT BELOW THE SURFACE LEVEL OF THE GRIND SURFACE SHALL BE FILLED WITH ASPHALT AND COMPACTED PRIOR TO OVERLAY (INCIDENTAL).
- CONTRACTOR TO CLEAN AND TACK COAT GRINDED ASPHALT SURFACE PRIOR TO OVERLAY (INCIDENTAL).
- CONTRACTOR TO SHAPE EXISTING GRAVEL SHOULDERS TO MATCH NEW ASPHALT EDGE (INCIDENTAL).

STORM SEWER NOTES

- STORM SEWER MATERIALS AND TESTING SHALL MEET CLEAN WATER SERVICES (CWS) DESIGN AND CONSTRUCTION SPECIFICATIONS AND THE CITY OF SHERWOOD'S ENGINEERING DESIGN MANUAL.
- WATER QUALITY SWALE PLANTING WATERING/ESTABLISHMENT WILL BE DONE BY THE CITY OF SHERWOOD.

RAILROAD NOTES

- CONTRACTOR TO OBTAIN PERMIT FROM RAILROAD PRIOR TO WORKING WITHIN RAILROAD RIGHT-OF-WAY. CITY HAS MADE INITIAL CONTACT AND PERMIT SUBMITTAL.
- CONTRACTOR TO PERFORM WORK IN COMPLIANCE WITH THE RAILROAD PERMIT AND IS RESPONSIBLE TO MEET RAILROAD'S INSURANCE REQUIREMENTS AND PROVIDE FOR RAILROAD INSPECTOR/FLAGGER IF REQUIRED BY THE RAILROAD (COST IS INCIDENTAL).

LEGEND

- ===== = EXISTING CURB AND GUTTER
- ===== = EXISTING/FINAL PAVEMENT MARKING
- ⊗_{WV} = EXISTING WATER VALVE
- W— = EXISTING WATER LINE
- ST— = EXISTING STORM LINE
- SS— = EXISTING STORM LINE
- G— = EXISTING GAS LINE
- T— = EXISTING TELECOMMUNICATION LINE
- = EXISTING MANHOLE
- ≡ = EXISTING CATCH BASIN
- S/W = SIDEWALK

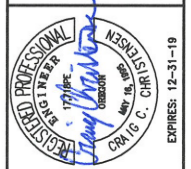
GENERAL NOTES

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
1000 SHERWOOD AVENUE
SHERWOOD, OREGON 97140

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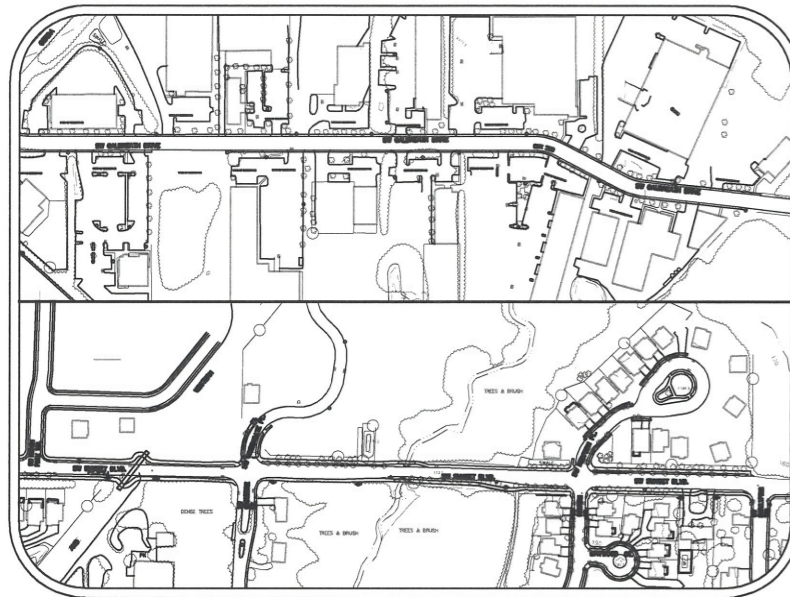


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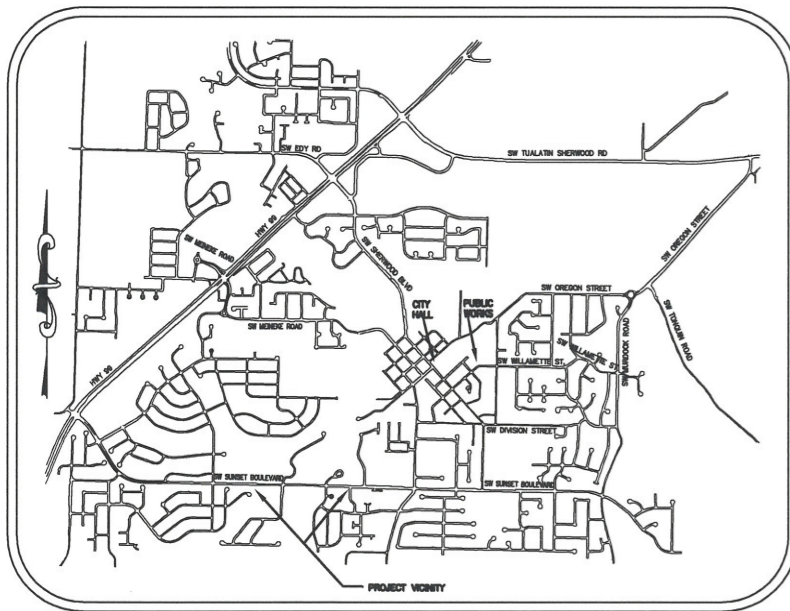
REVISIONS	SHEET NO.
	2
	13

JOB NO.	
SHEET NO.	2
OF	13

ESC PLAN FOR SITES 1 TO 5 ACRES



SITE MAP NOT TO SCALE



VICINITY MAP NOT TO SCALE

PROJECT LOCATION:

SW SUNSET BOULEVARD BETWEEN SW RICHEN PARK TERRACE AND SW EUCALYPTUS TERRACE AND SW GALBREATH DRIVE BETWEEN SW GERDA LANE AND BONNEVILLE POWER LINES, WASHINGTON COUNTY, OREGON
 SUNSET BLVD. LATITUDE = 45.349766, LONGITUDE = -122.849877
 GALBREATH DR. LATITUDE = 45.370300, LONGITUDE = -122.821417

PROPERTY DESCRIPTION:

LOCATED IN SECTIONS 28B AND 31D, TOWNSHIP 1 SOUTH, RANGE 1 WEST, WILLAMETTE MERIDIAN, WASHINGTON COUNTY, OREGON

ATTENTION EXCAVATORS:

OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY CALLING 503-232-1987. IF YOU HAVE ANY QUESTIONS ABOUT THE RULES, YOU MAY CONTACT THE CENTER. YOU MUST NOTIFY THE CENTER AT LEAST TWO BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION. CALL 503-246-6699.

DEVELOPER

DEVELOPER/COMPANY: CITY OF SHERWOOD
 CONTACT: CRAIG CHRISTENSEN, PE
 ADDRESS 22560 SW PINE STREET
 ADDRESS SHERWOOD, OR 97140
 PHONE: 503-925-2301
 FAX: 503-625-0629

PLANNING / ENGINEERING / SURVEYING FIRM

DEVELOPER/COMPANY: CITY OF SHERWOOD
 CONTACT: CRAIG CHRISTENSEN, PE
 ADDRESS 22560 SW PINE STREET
 ADDRESS SHERWOOD, OR 97140
 PHONE: 503-925-2301
 FAX: 503-625-0629

NARRATIVE DESCRIPTIONS

EXISTING SITE CONDITIONS

* PAVED STREETS

DEVELOPED CONDITIONS

* PAVED STREETS

NATURE OF CONSTRUCTION ACTIVITY AND ESTIMATED TIME TABLE

- * CLEARING (N/A)
- * MASS GRADING (N/A)
- * UTILITY INSTALLATION (OCTOBER, 2018)
- * STREET CONSTRUCTION (OCTOBER, 2018)
- * FINAL STABILIZATION (OCTOBER, 2018)

TOTAL SITE AREA = ±117,600 SF = ±2.70 ACRES

TOTAL DISTURBED AREA = ±117,600 SF = ±2.70 ACRES

SITE SOIL CLASSIFICATION:

GENERALLY HILLSBORO AND QUATAMA

ON-SITE SOILS HAVE A MODERATE TO HIGH EROSION POTENTIAL. ALL FILL MATERIAL SHALL BE GENERATED ON-SITE FROM GRADING EXCAVATION AND UTILITY TRENCH SPOILS.

RECEIVING WATER BODIES:

NEAREST WATER BODY: SUNSET BLVD. - CEDAR CREEK
 GALBREATH DRIVE - ROCK CREEK

STANDARD EROSION AND SEDIMENT CONTROL PLAN DRAWING NOTES:

1. All permit registrants must implement the ESCP. Failure to implement any of the control measures or practices described in the ESCP is a violation of the permit.
2. The ESCP measures shown on this plan are minimum requirements for anticipated site conditions. During the construction period, upgrade these measures as needed to comply with all applicable local, state, and federal erosion and sediment control regulations.
3. Submission of all ESCP revisions is not required. Submittal of the ESCP revisions is only under specific conditions. Submit all necessary revision to DEQ or Agent.
4. Phase clearing and grading to the maximum extent practical to prevent exposed inactive areas from becoming a source of erosion.
5. Identify, mark, and protect (by fencing off or other means) critical riparian areas and vegetation including important trees and associated rooting zones, and vegetation areas to be preserved. Identify vegetative buffer zones between the site and sensitive areas (e.g., wetlands), and other areas to be preserved, especially in perimeter areas.
6. Preserve existing vegetation when practical and re-vegetate open areas. Re-vegetate open areas when practicable before and after grading or construction. Identify the type of vegetative seed mix used.
7. Erosion and sediment control measures including perimeter sediment control must be in place before vegetation is disturbed and must remain in place and be maintained, repaired, and promptly implemented following procedures established for the duration of construction, including protection for active storm drain inlets and catch basins and appropriate non-stormwater pollution controls.
8. Establish concrete truck and other concrete equipment washout areas before beginning concrete work. Direct all wash water into a pit or leak-proof container. Handle wash water as waste, concrete discharge to waters of the state is prohibited.
9. Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses and for all roadways including gravel roadways.
10. Establish material and waste storage areas, and other non-stormwater controls.
11. Prevent tracking of sediment onto public or private roads using BMPs such as: graveled (or paved) exits and parking areas, gravel oil unpaved roads located onsite, or use an exit tire wash. These BMPs must be in place prior to land-disturbing activities.
12. When trucking saturated soils from the site, either use water-tight trucks or drain loads on site.
13. Use BMPs to prevent or minimize stormwater exposure to pollutants from spills; vehicle and equipment fueling, maintenance, and storage; other cleaning and maintenance activities; and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, leftover paints, solvents, and glues from construction operations.
14. Implement the following BMPs when applicable: written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures, spill kits in all vehicles, regular maintenance schedule for vehicles and machinery, material delivery and storage controls, training and signage, and covered storage areas for waste and supplies.
15. Use water, soil-binding agent or other dust control technique as needed to avoid wind-blown soil.
16. The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient releases to surface waters. Exercise caution when using time-release fertilizers within any waterway riparian zone.
17. If a stormwater treatment system (for example, electro-coagulation, flocculation, filtration, etc.) for sediment or other pollutant removal is employed, submit an operation and maintenance plan (including system schematic, location of system, location of inlet, location of discharge, discharge dispersion device design, and a sampling plan and frequency) before operating the treatment system. Obtain plan approval before operating the treatment system. Operate and maintain the treatment system according to manufacturer's specifications.
18. At the end of each workday soil stockpiles must be stabilized or covered, or other BMPs must be implemented to prevent discharges to surface waters or conveyance systems leading to surface waters.
19. Construction activities must avoid or minimize excavation and creation of bare ground during wet weather October 01 - May 31.
20. Sediment fence: remove trapped sediment before it reaches one third of the above ground fence height and before fence removal.
21. Other sediment barriers (such as biobags): remove sediment before it reaches two inches depth above ground height. and before BMP removal.
22. Catch basins: clean before retention capacity has been reduced by fifty percent. Sediment basins and sediment traps: remove trapped sediments before design capacity has been reduced by fifty percent and at completion of project.
23. Within 24 hours, significant sediment that has left the construction site, must be remediated. Investigate the cause of the sediment release and implement steps to prevent a recurrence of the discharge within the same 24 hours. Any in-stream clean up of sediment shall be performed according to the Oregon Division of State Lands required timeframe.
24. The intentional washing of sediment into storm sewers or drainage ways must not occur. Vacuuming or dry sweeping and material pickup must be used to cleanup released sediments.
25. Provide permanent erosion control measures on all exposed areas. Do not remove temporary sediment control practices until permanent vegetation or other cover of exposed areas is established. However, do remove all temporary erosion control measures as exposed areas become stabilized, unless doing so conflicts with local requirements. Properly dispose of construction materials and waste, including sediment retained by temporary BMPs.
26. If vegetative seed mixes are specified, seeding must take place no later than September 1; the type and percentages of seed in the mix must be identified on the plans.
27. All pumping of sediment laden water shall be discharged over an undisturbed, preferably vegetated area, and through a sediment control BMP i.e. (filter bag).
28. All exposed soils must be covered during the wet weather period, October 01 - May 31.
29. If water of the state is within the project site or within 50 feet of the project boundary, maintain the existing natural buffer within the 50-foot zone for the duration of the permit coverage, or maintain less than the entire existing natural buffer and provide additional erosion and sediment control BMPs.

INSPECTION FREQUENCY:

SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	WEEKLY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOW MELT, IS OCCURRING. AT LEAST ONCE EVERY MONTH, REGARDLESS OF WHETHER STORMWATER RUNOFF IS OCCURRING.
2. PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY.	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
3. INACTIVE PERIODS GREATER THAN FOURTEEN (14) CONSECUTIVE CALENDAR DAYS.	ONCE EVERY MONTH.
4. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.
5. PERIODS DURING WHICH DISCHARGE IS UNLIKELY DUE TO FROZEN CONDITIONS.	MONTHLY. RESUME MONITORING IMMEDIATELY UPON MELT, OR WHEN WEATHER CONDITIONS MAKE DISCHARGES LIKELY.

- * HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS.
- * ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200-CN PERMIT REQUIREMENTS.
- * INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-CN PERMIT REQUIREMENTS.
- * RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. DURING INACTIVE PERIODS OF GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS, RETAIN THE ESCP AT THE CONSTRUCTION SITE OR AT ANOTHER LOCATION.

THE PERMITTEE IS REQUIRED TO MEET ALL THE CONDITIONS OF THE 1200-CN PERMIT. THIS ESCP AND GENERAL CONDITIONS HAVE BEEN DEVELOPED TO FACILITATE COMPLIANCE WITH THE 1200-CN PERMIT REQUIREMENTS. IN CASES OF DISCREPANCIES OR OMISSIONS, THE 1200-CN PERMIT REQUIREMENTS SUPERCEDE REQUIREMENTS OF THIS PLAN.

BMP MATRIX FOR CONSTRUCTION PHASES

REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'S.

	CLEARING	MASS GRADING	UTILITY INSTALLATION	STREET CONSTRUCTION	FINAL STABILIZATION	WET WEATHER (OCT. 1 - MAY 31ST)
EROSION PREVENTION						
PRESERVE NATURAL VEGETATION					X	X
GROUND COVER						
HYDRAULIC APPLICATIONS						
PLASTIC SHEETING						
MATTING						
DUST CONTROL				X		
TEMPORARY PERMANENT SEEDING					X	X
BUFFER ZONE						
OTHER:						
SEDIMENT CONTROL						
SEDIMENT FENCE (PERIMETER)						
SEDIMENT FENCE (INTERIOR)						
STRAW WATTLES						
FILTER BERM						
INLET PROTECTION **			X	X	X	X
DEWATERING						
SEDIMENT TRAP						
NATURAL BUFFER ENCROACHMENT						
OTHER:						
RUN OFF CONTROL						
CONSTRUCTION ENTRANCE						
PIPE SLOPE DRAIN						
OUTLET PROTECTION						
SURFACE ROUGHENING						
CHECK DAMS **			X	X	X	X
OTHER:						
POLLUTION PREVENTION						
PROPER STORAGE			X	X	X	X
WASTE MANAGEMENT			X	X	X	X
SPILL KIT ON-SITE			X	X	X	X
CONCRETE WASHOUT AREA			X	X	X	X
OTHER:						

- * SIGNIFIES ADDITIONAL BMP'S REQUIRED FOR WORK WITHIN 50' OF WATER OF THE STATE.
- ** SIGNIFIES BMP THAT WILL BE INSTALLED PRIOR TO ANY GROUND DISTURBING ACTIVITY.

RATIONALE STATEMENT

A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S GUIDANCE MANUAL HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMP'S WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOIL CONDITIONS TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS, AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESC PLAN, AN ACTION PLAN WILL BE SUBMITTED.

ccc
INITIAL

PERMITTEE'S SITE INSPECTOR: THOMAS BENSON

COMPANY/AGENCY: CITY OF SHERWOOD
 PHONE: (503) 925-2305
 FAX: (503) 625-0629
 E-MAIL: BENSONT@SHERWOODOREGON.GOV
 DESCRIPTION OF EXPERIENCE:
 CITY EROSION CONTROL INSPECTOR

SHEET INDEX

EROSION AND SEDIMENT CONTROL PLANS

- C050 EROSION AND SEDIMENT CONTROL COVER SHEET
- C051 CLEARING AND DEMOLITION EROSION AND SEDIMENT CONTROL PLAN
- C052 GRADING, STREET AND UTILITY CONSTRUCTION EROSION AND SEDIMENT CONTROL PLAN
- C053 EROSION AND SEDIMENT CONTROL DETAILS
- C054 EROSION AND SEDIMENT CONTROL DETAILS

GENERAL NOTES

2018 PAVEMENT OVERLAY PROJECTS

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

DESIGNED BY: CCC
DRAWN BY: CCC/TB
CHECKED BY: RSV/S
FULL SIZE SCALE: AS NOTED
DATE: AUGUST, 2019
SUNSET_SHEETS

JOB NO.

SHEET 3

13

REVISIONS

PRE-CONSTRUCTION, CLEARING, AND DEMOLITION NOTES:

- ALL BASE ESC MEASURES (INLET PROTECTION, PERIMETER SEDIMENT CONTROL, GRAVEL CONSTRUCTION ENTRANCES, ETC.) MUST BE IN PLACE, FUNCTIONAL, AND APPROVED IN AN INITIAL INSPECTION, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
- SEDIMENT BARRIERS APPROVED FOR USE INCLUDE SEDIMENT FENCE, BERMS CONSTRUCTED OUT OF MULCH, CHIPPINGS, OR OTHER SUITABLE MATERIAL, STRAW WATTLES, OR OTHER APPROVED MATERIALS.
- SENSITIVE RESOURCES INCLUDING, BUT NOT LIMITED TO, TREES, WETLANDS, AND RIPARIAN PROTECTION AREAS SHALL BE CLEARLY DELINEATED WITH ORANGE CONSTRUCTION FENCING OR CHAIN LINK FENCING IN A MANNER THAT IS CLEARLY VISIBLE TO ANYONE IN THE AREA. NO ACTIVITIES ARE PERMITTED TO OCCUR BEYOND THE CONSTRUCTION BARRIER.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, STREET SWEEPING, AND VACUUMING, MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- RUN-ON AND RUN-OFF CONTROLS SHALL BE IN PLACE AND FUNCTIONING PRIOR TO BEGINNING SUBSTANTIAL CONSTRUCTION ACTIVITIES. RUN-ON AND RUN-OFF CONTROL MEASURES INCLUDE: SLOPE DRAINS (WITH OUTLET PROTECTION), CHECK DAMS, AND SURFACE ROUGHENING.

THESE EROSION AND SEDIMENT CONTROL PLANS ASSUME "DRY WEATHER" CONSTRUCTION. "WET WEATHER" CONSTRUCTION MEASURES NEED TO BE APPLIED BETWEEN OCTOBER 1ST AND MAY 31ST.

GRADING, STREET AND UTILITY EROSION AND SEDIMENT CONSTRUCTION NOTES:

- SEED USED FOR TEMPORARY OR PERMANENT SEEDING SHALL BE COMPOSED OF ONE OF THE FOLLOWING MIXTURES, UNLESS OTHERWISE AUTHORIZED:
 - VEGETATED CORRIDOR AREAS REQUIRE NATIVE SEED MIXES. SEE RESTORATION PLAN FOR APPROPRIATE SEED MIX.
 - DWARF GRASS MIX (MIN. 100 LB./AC.)
 - DWARF PERENNIAL RYEGRASS (80% BY WEIGHT)
 - CREeping RED FESCUE (20% BY WEIGHT)
 - STANDARD HEIGHT GRASS MIX (MIN. 100LB./AC.)
 - ANNUAL RYEGRASS (40% BY WEIGHT)
 - TURF-TYPE FESCUE (60% BY WEIGHT)
- SLOPE TO RECEIVE TEMPORARY OR PERMANENT SEEDING SHALL HAVE THE SURFACE ROUGHENED BY MEANS OF TRACK-WALKING OR THE USE OF OTHER APPROVED IMPLEMENTS. SURFACE ROUGHENING IMPROVES SEED BEDDING AND REDUCES RUN-OFF VELOCITY.

- LONG TERM SLOPE STABILIZATION MEASURES SHALL INCLUDE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER VIA SEEDING WITH APPROVED MIX AND APPLICATION RATE.
- TEMPORARY SLOPE STABILIZATION MEASURES SHALL INCLUDE: COVERING EXPOSED SOIL WITH PLASTIC SHEETING, STRAW MULCHING, WOOD CHIPS, OR OTHER APPROVED MEASURES.
- STOCKPILED SOIL OR STRIPPINGS SHALL BE PLACED IN A STABLE LOCATION AND CONFIGURATION. DURING "WET WEATHER" PERIODS, STOCKPILES SHALL BE COVERED WITH PLASTIC SHEETING OR STRAW MULCH. SEDIMENT FENCE IS REQUIRED AROUND THE PERIMETER OF THE STOCKPILE.
- EXPOSED CUT OR FILL AREAS SHALL BE STABILIZED THROUGH THE USE OF TEMPORARY SEEDING AND MULCHING, EROSION CONTROL BLANKETS OR MATS, MID-SLOPE SEDIMENT FENCES OR WATTLES, OR OTHER APPROPRIATE MEASURES. SLOPES EXCEEDING 25% MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES.
- AREAS SUBJECT TO WIND EROSION SHALL USE APPROPRIATE DUST CONTROL MEASURES INCLUDING THE APPLICATION OF A FINE SPRAY OF WATER, PLASTIC SHEETING, STRAW MULCHING, OR OTHER APPROVED MEASURES.
- CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES INCLUDING, BUT NOT LIMITED TO, TIRE WASHES, STREET SWEEPING, AND VACUUMING MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

- ACTIVE INLETS TO STORM WATER SYSTEMS SHALL BE PROTECTED THROUGH THE USE OF APPROVED INLET PROTECTION MEASURES. ALL INLET PROTECTION MEASURES ARE TO BE REGULARLY INSPECTED AND MAINTAINED AS NEEDED.
- SATURATED MATERIALS THAT ARE HAULED OFF-SITE MUST BE TRANSPORTED IN WATER-TIGHT TRUCKS TO ELIMINATE SPILLAGE OF SEDIMENT AND SEDIMENT-LADEN WATER.
- AN AREA SHALL BE PROVIDED FOR THE WASHING OUT OF CONCRETE TRUCKS IN A LOCATION THAT DOES NOT PROVIDE RUN-OFF THAT CAN ENTER THE STORM WATER SYSTEM OR SURFACE WATERS. IF THE CONCRETE WASH-OUT AREA CAN NOT BE CONSTRUCTED GREATER THAN 50' FROM ANY DISCHARGE POINT, SECONDARY MEASURES SUCH AS BERMS OR TEMPORARY SETTLING PITS MAY BE REQUIRED. THE WASH-OUT SHALL BE LOCATED WITHIN SIX FEET OF TRUCK ACCESS AND BE CLEANED WHEN IT REACHES 50% OF THE CAPACITY.
- SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE SHALL NOT BE TRANSFERRED TO THE STORM WATER SYSTEM. SWEEPINGS SHALL BE PICKED UP AND DISPOSED IN THE TRASH.
- AVOID PAVING IN WET WEATHER WHEN PAVING CHEMICALS CAN RUN-OFF INTO THE STORM WATER SYSTEM.
- USE BMPs SUCH AS CHECK-DAMS, BERMS, AND INLET PROTECTION TO PREVENT RUN-OFF FROM REACHING DISCHARGE POINTS.
- COVER CATCH BASINS, MANHOLES, AND OTHER DISCHARGE POINTS WHEN APPLYING SEAL COAT, TACK COAT, ETC. TO PREVENT INTRODUCING THESE MATERIALS TO THE STORM WATER SYSTEM.

EROSION CONTROL NOTES AND DETAILS

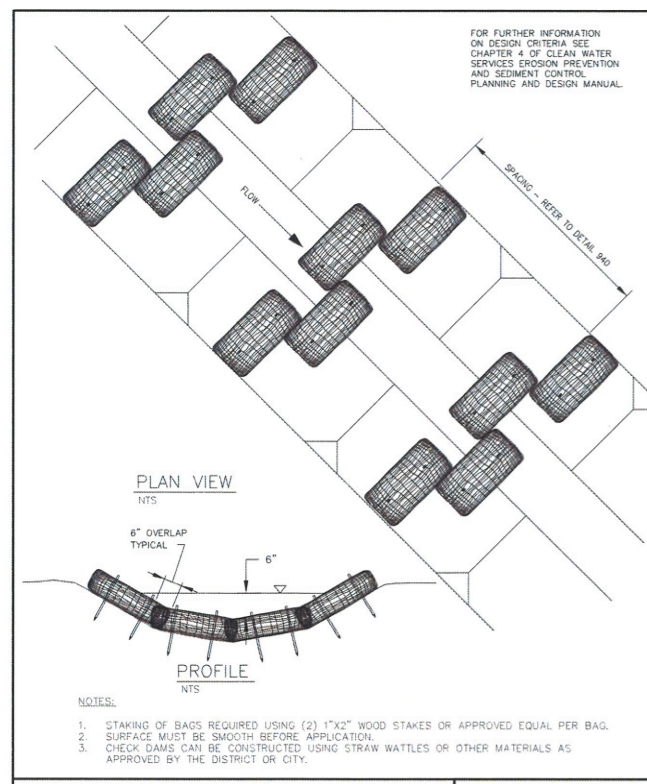
2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON

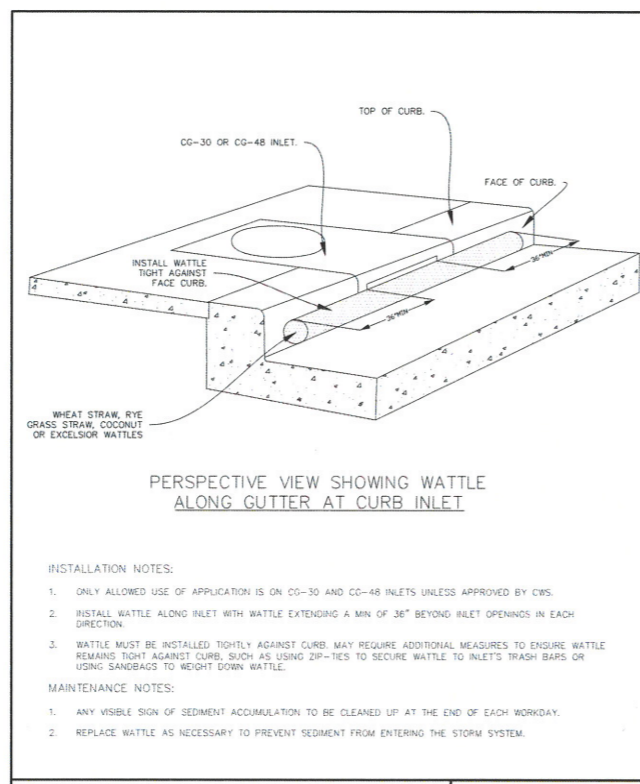
CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22550 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL ENGINEER
STATE OF OREGON
No. 17146
Craig C. Christensen
EXPIRES: 12-31-19

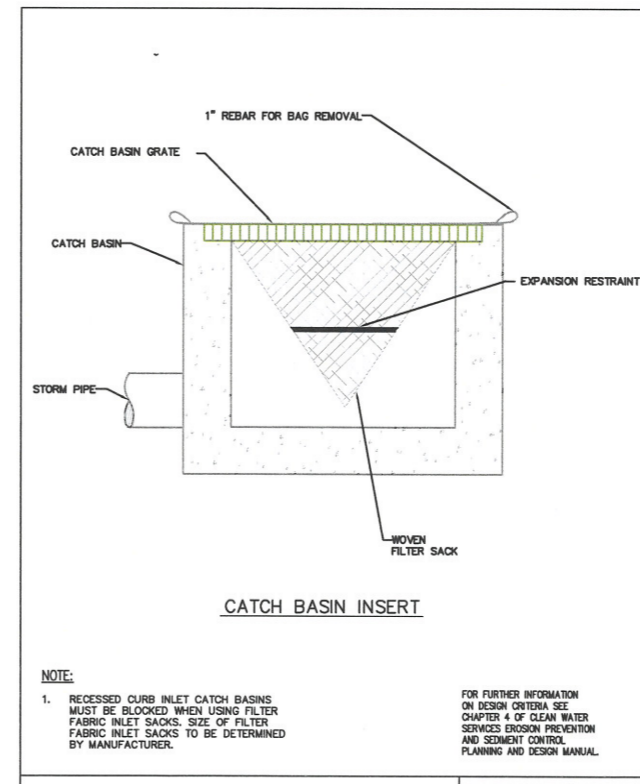
B-23-10



CHECK DAM BIO-FILTER BAG
DRAWING NO. 845 REVISED 12-16
CleanWater Services



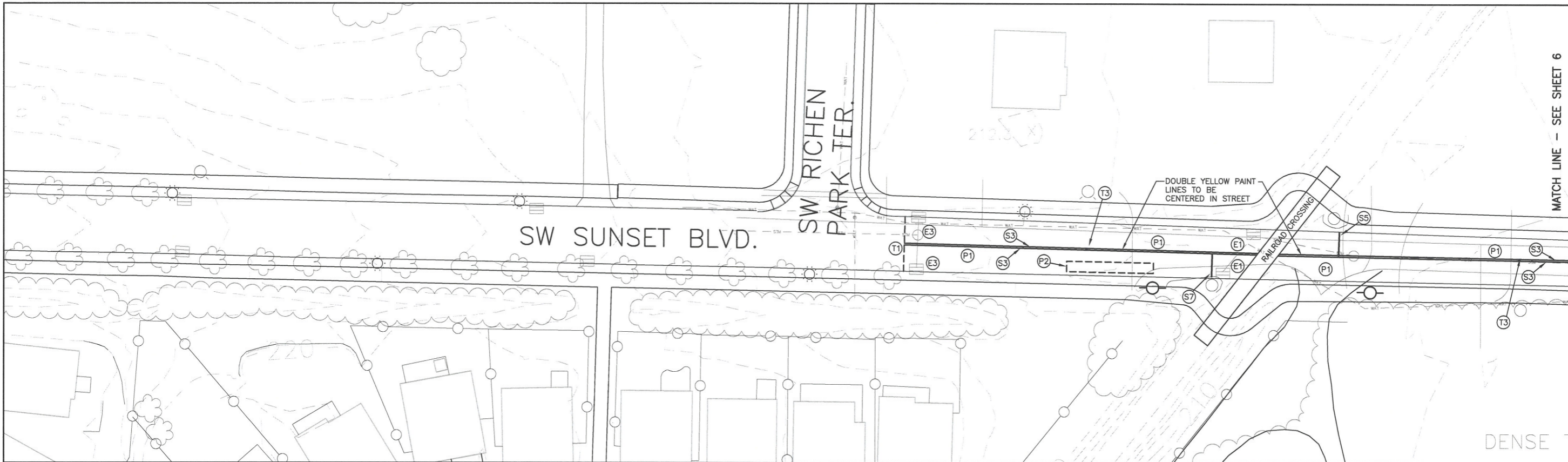
CURB AND GUTTER INLET PROTECTION
DRAWING NO. 905 REVISED 2-17
CleanWater Services



INLET PROTECTION TYPE 5
DRAWING NO. 920 REVISED 12-16
CleanWater Services

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	AS NOTED
DATE:	APRIL 7, 2015
SUNSET SHEETS	

REVISIONS	
JOB NO.	
SHEET NO.	4
	13



MATCH LINE - SEE SHEET 6

SW SUNSET BOULEVARD
(WEST PLAN VIEW)

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

STRIPING KEY NOTES

- (X') - APPROXIMATE LENGTH
- (S1) INSTALL WHITE THERMOPLASTIC BAR (12" WIDTH)
- (S2) INSTALL WHITE PAINT LINE (8" WIDTH)
- (S3) INSTALL YELLOW PAINT LINE (4" WIDTH)
- (S4) INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- (S5) INSTALL WHITE THERMOPLASTIC RAILROAD CROSSING SYMBOL WITH PAVEMENT BARS
- (S6) INSTALL WHITE PAINT LINE (4" WIDTH)
- (S7) INSTALL WHITE THERMOPLASTIC BAR (24" WIDTH)

STREET KEY NOTES

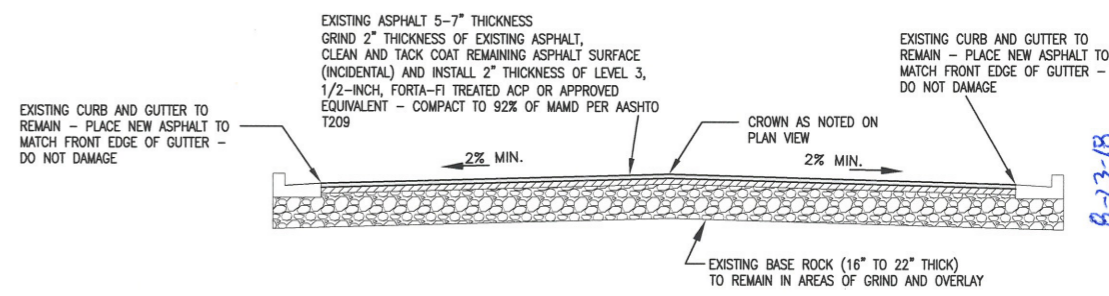
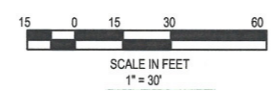
- (T1) SAWCUT (2" DEPTH) - CONFIRM LOCATION WITH INSPECTOR
- (T2) EXISTING SURVEY MONUMENT TO REMAIN - DO NOT DISTURB - ANY SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RE-ESTABLISHED AT THE CONTRACTOR'S EXPENSE
- (T3) INSTALL BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS

PAVING KEY NOTES

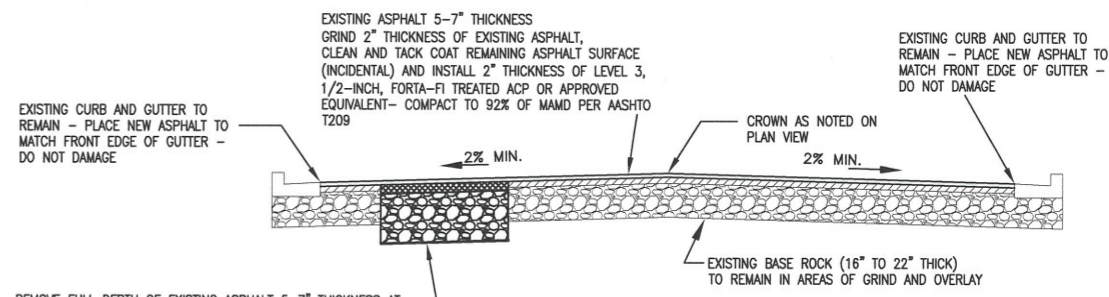
- (P1) AREA OF 2" ASPHALT GRIND WITH OVERLAY
- (P2) APPROXIMATE LOCATION OF FULL DEPTH PAVEMENT STRUCTURE RECONSTRUCTION - CONFIRM LIMITS WITH INSPECTOR.

EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- (E3) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS IN FRONT OF CURB OPENING.



TYPICAL GRIND AND INLAY STREET SECTION
NOT TO SCALE



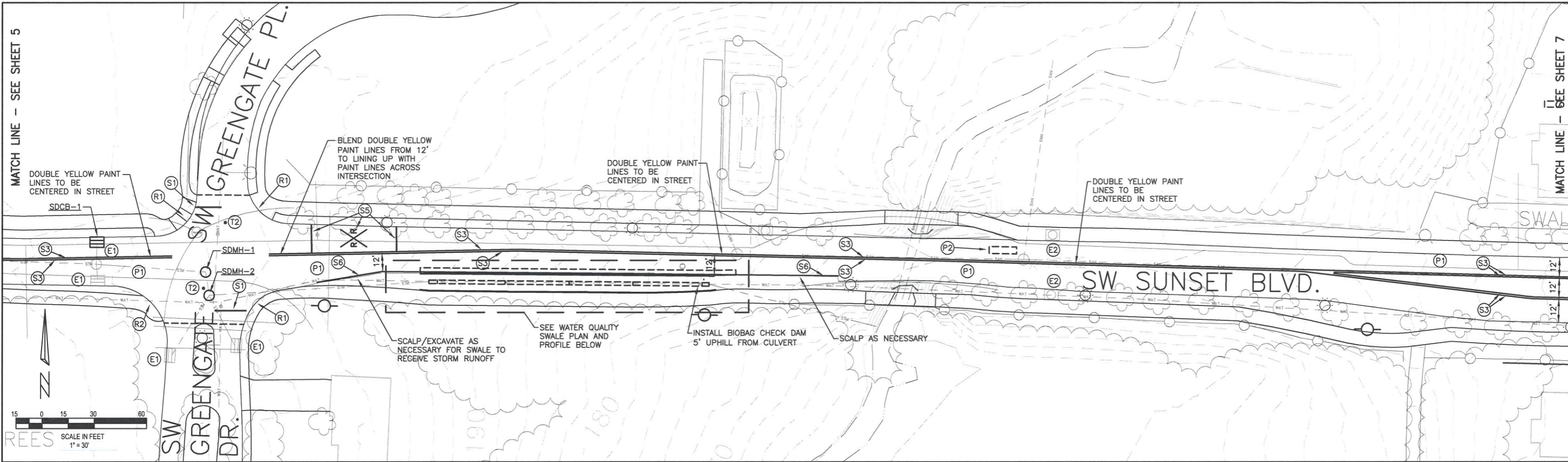
TYPICAL STREET RECONSTRUCTION SECTION
NOT TO SCALE

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22860 SW PINE STREET
SHERWOOD, OREGON 97140

PHONE: (503) 925-2309
FAX: (503) 625-0629
E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL ENGINEER
17779 SE
CRAIG C. CHIRK
EXPIRES: 12-31-19

DESIGNED BY:	CCC/TB	DESIGNED BY:	CCC/TB
DRAWN BY:	RS	CHECKED BY:	RS
FULL SIZE SCALE:	1"=30'	DATE:	AUGUST, 2019
SUNSET_SHEETS.DWG			
REVISIONS			
JOB NO.			
SHEET NO. 5			
13			



STORM SEWER KEY NOTES

- SDCB-1
REMOVE EXISTING CATCH BASIN (3.9' DEEP TO I.E.) AND INSTALL NEW CATCH BASIN CG-2 IS PREFERABLE HOWEVER IF GAS LINE DOES NOT ALLOW ENOUGH ROOM TO INSTALL CATCH BASIN (NO CONCRETE AROUND GAS LINE) THEN A CG-1 OR NARROWER SHALL BE USED - CONTACT INSPECTOR ONCE HOLE IS OPEN FOR DETERMINATION OF CATCH BASIN TO BE USED.
- SDMH-1
EXISTING MANHOLE TO REMAIN RIM 197.00
12" I.E. OUT (S) 192.83 (CONCRETE)
±4.5' SUMP BELOW I.E. OUT
FILL IN SUMP WITH CONTROLLED LOW STRENGTH MATERIAL AND CHANNEL PER DETAIL
- SDMH-2
REMOVE EXISTING MANHOLE AND REPLACE WITH NEW WATER QUALITY MANHOLE AND RECONNECT PIPES TO NEW MANHOLE
MANHOLE TO HAVE MINIMUM 3-FOOT SUMP BELOW BOTTOM OF SNOUT
RIM 196.68 (ASSUMED ELEV. FROM AS-BUILT)
12" I.E. IN (N) 193.52± (CONCRETE)
12" I.E. IN (SW) 192.35± (ADS N-12)
12" I.E. IN (SE) 192.18± (C900)
12" I.E. OUT (E) 191.64± (ADS N-12)
CONTRACTOR TO VERIFY SIZE, DEPTHS AND MATERIAL OF PIPES CONNECTED TO EXISTING MANHOLE PRIOR TO ORDERING OF MATERIALS

STRIPING KEY NOTES

- (X') - APPROXIMATE LENGTH
- (S1) INSTALL WHITE THERMOPLASTIC BAR (12" WIDTH)
- (S2) INSTALL WHITE PAINT LINE (8" WIDTH)
- (S3) INSTALL YELLOW PAINT LINE (4" WIDTH)
- (S4) INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- (S5) INSTALL WHITE THERMOPLASTIC RAILROAD CROSSING SYMBOL WITH PAVEMENT BARS
- (S6) INSTALL WHITE PAINT LINE (4" WIDTH)

SIDEWALK RAMP KEY NOTES

- (R1) REMOVE EXISTING CURB, ASPHALT AND SIDEWALK AND REPLACE WITH NEW TO CREATE SIDEWALK RAMP MEETING ADA REQUIREMENT - MEET WITH ENGINEER IN FIELD TO ESTABLISH LIMITS OF REMOVAL
- (R2) REMOVE EXISTING SIDEWALK IN LOCATION OF SIDEWALK RAMP (1 PANEL) AND REPLACE WITH NEW SIDEWALK AND RAMP TREATMENT

STREET KEY NOTES

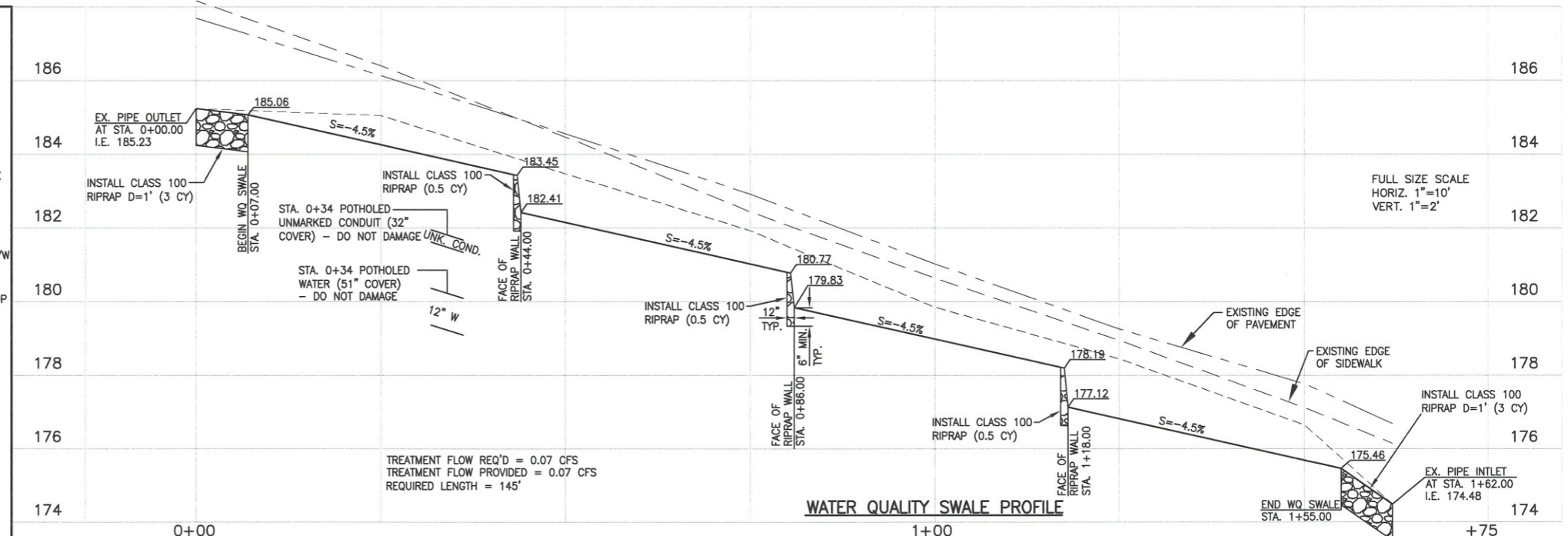
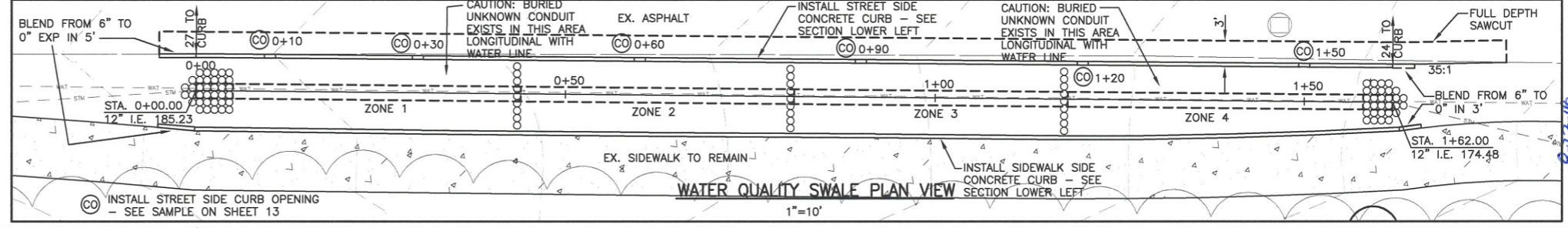
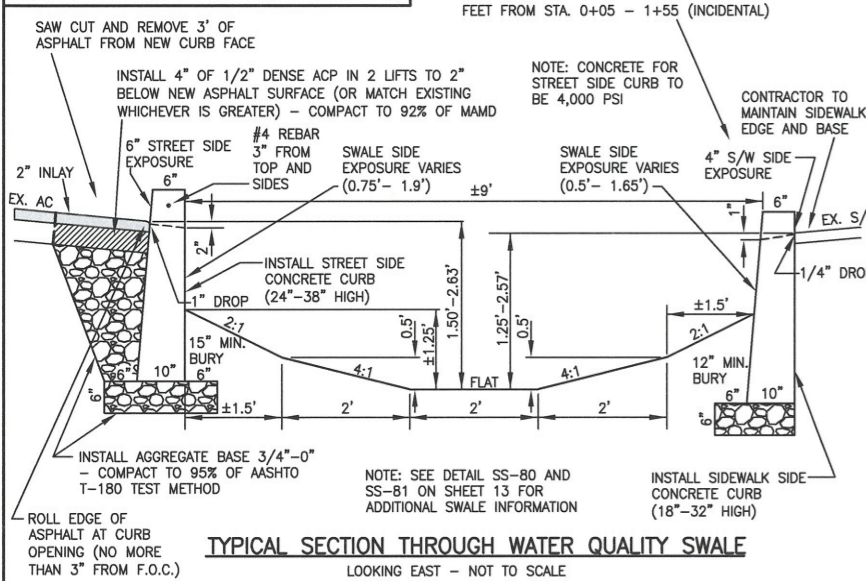
- (T1) SAWCUT (2" DEPTH) - CONFIRM LOCATION WITH INSPECTOR
- (T2) EXISTING SURVEY MONUMENT TO REMAIN - DO NOT DISTURB - ANY SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RE-ESTABLISHED AT THE CONTRACTOR'S EXPENSE

PAVING KEY NOTES

- (P1) AREA OF 2" ASPHALT GRIND WITH OVERLAY
- (P2) APPROXIMATE LOCATION OF FULL DEPTH PAVEMENT STRUCTURE RECONSTRUCTION - CONFIRM LIMITS WITH INSPECTOR.

EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- (E3) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS IN FRONT OF CURB OPENING.



SW SUNSET BOULEVARD (MIDDLE PLAN VIEW)
 SUNSET BOULEVARD AND APRIL COURT PAVEMENT REHABILITATION
 LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, OREGON

CITY OF SHERWOOD
 ENGINEERING DEPARTMENT
 22600 SW FIRE STREET
 SHERWOOD, OREGON 97140
 PHONE: (503) 925-2309
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 E-MAIL: engineering@sherwoodoregon.gov

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/CS
FULL SIZE SCALE:	1"=30'
DATE:	AUGUST, 2018
SUNSET_SHEETS.DWG	
EXPIRES: 12-31-19	
JOB NO.	
SHEET NO. 6	
OF 13	



MATCH LINE - SEE SHEET 6

STRIPING KEY NOTES

- (X') - APPROXIMATE LENGTH
- (S1) INSTALL WHITE THERMOPLASTIC BAR (12" WIDTH)
- (S2) INSTALL WHITE PAINT LINE (8" WIDTH)
- (S3) INSTALL YELLOW PAINT LINE (4" WIDTH)
- (S4) INSTALL WHITE THERMOPLASTIC LEFT TURN ARROW
- (S5) INSTALL WHITE THERMOPLASTIC RAILROAD CROSSING SYMBOL WITH PAVEMENT BARS
- (S6) INSTALL WHITE PAINT LINE (4" WIDTH)

STREET KEY NOTES

- (T1) SAWCUT (2" DEPTH) - CONFIRM LOCATION WITH INSPECTOR
- (T2) EXISTING SURVEY MONUMENT TO REMAIN - DO NOT DISTURB - ANY SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RE-ESTABLISHED AT THE CONTRACTOR'S EXPENSE

PAVING KEY NOTES

- (P1) AREA OF 2" ASPHALT GRIND WITH OVERLAY
- (P2) APPROXIMATE LOCATION OF FULL DEPTH PAVEMENT STRUCTURE RECONSTRUCTION - CONFIRM LIMITS WITH INSPECTOR.

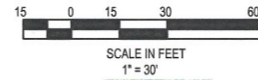
SIDEWALK RAMP KEY NOTES

- (R1) REMOVE EXISTING CURB, ASPHALT AND SIDEWALK AND REPLACE WITH NEW TO CREATE SIDEWALK RAMP MEETING ADA REQUIREMENT - MEET WITH ENGINEER IN FIELD TO ESTABLISH LIMITS OF REMOVAL

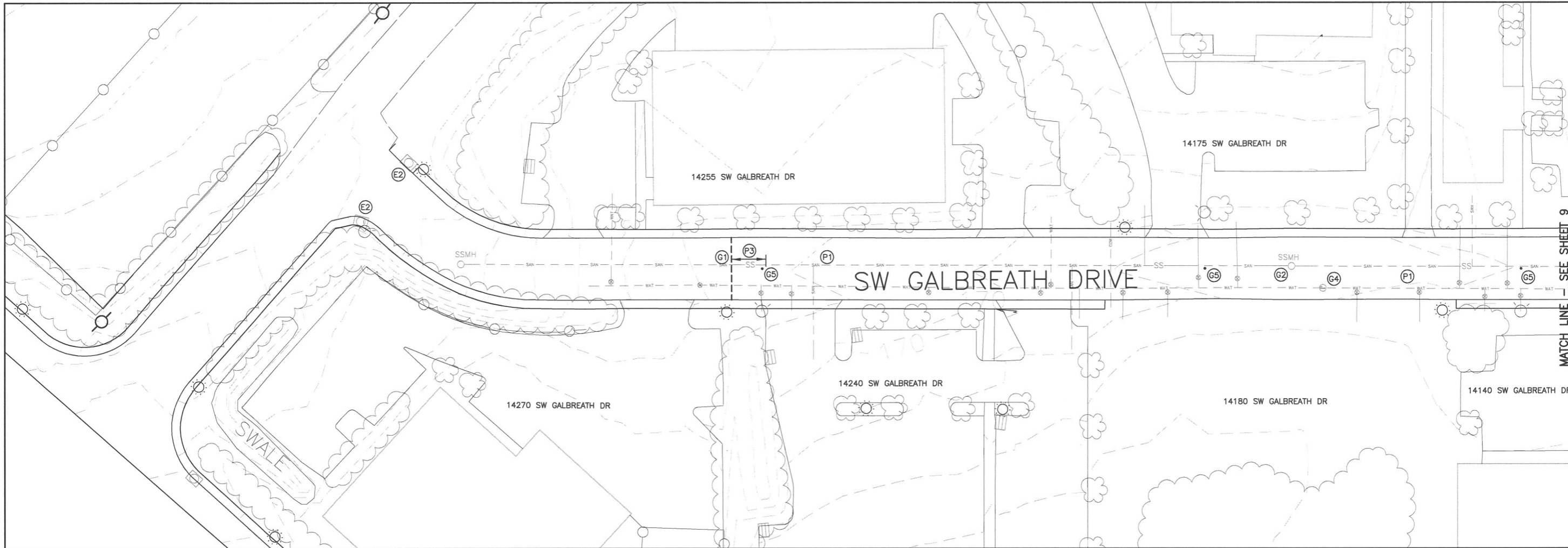
EROSION CONTROL KEY NOTES

- (E1) INSTALL SILT SACK IN CATCH BASIN.
- (E2) INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- (E3) INSTALL SILT SACK IN CATCH BASIN AND BIOBAGS IN FRONT OF CURB OPENING.

DOUBLE YELLOW PAINT LINES TO BE CENTERED IN STREET



SW SUNSET BOULEVARD (EAST PLAN VIEW)																
2018 PAVEMENT OVERLAY PROJECTS																
LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W4M. IN THE CITY OF SHERWOOD, WASHINGTON COUNTY, STATE OF OREGON																
<p>CITY OF SHERWOOD ENGINEERING DEPARTMENT 22560 SW PINE STREET SHERWOOD, OREGON 97140</p> <p>PHONE: (503) 925-2309 FAX: (503) 625-0629 E-MAIL: engineering@sherwoodoregon.gov</p>	<p style="text-align: right;">8-23-18</p>															
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>DESIGNED BY:</td> <td>CCC</td> </tr> <tr> <td>DRAWN BY:</td> <td>CCC/TB</td> </tr> <tr> <td>CHECKED BY:</td> <td>LRSCS</td> </tr> <tr> <td>FULL SIZE SCALE:</td> <td>1"=30'</td> </tr> <tr> <td>DATE:</td> <td>AUGUST 2018</td> </tr> </table>	DESIGNED BY:	CCC	DRAWN BY:	CCC/TB	CHECKED BY:	LRSCS	FULL SIZE SCALE:	1"=30'	DATE:	AUGUST 2018	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;">REVISIONS</td> </tr> <tr> <td style="height: 40px;"> </td> </tr> <tr> <td style="height: 40px;"> </td> </tr> <tr> <td style="height: 40px;"> </td> </tr> <tr> <td style="height: 40px;"> </td> </tr> </table>	REVISIONS				
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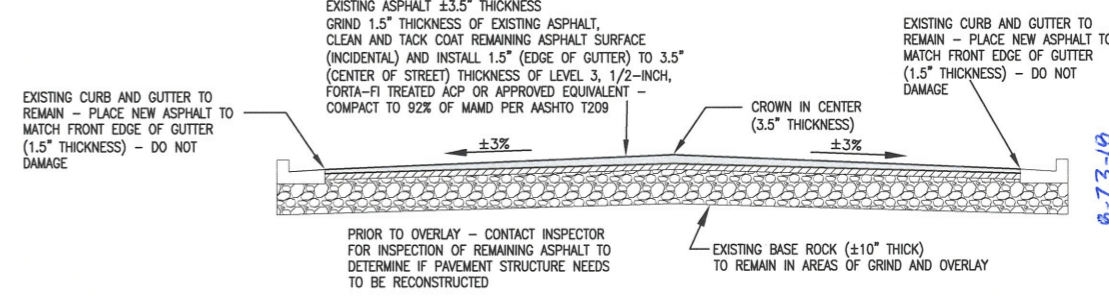
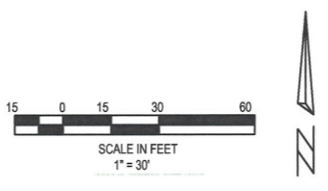
**SW GALBREATH DRIVE
(WEST PLAN VIEW)**

2018 PAVEMENT OVERLAY PROJECTS

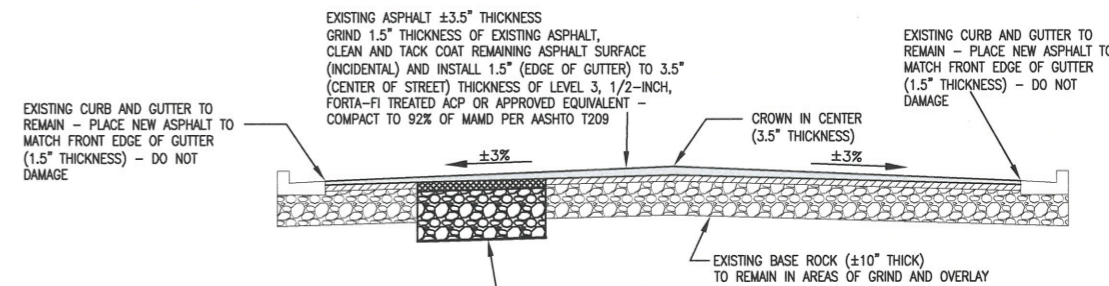
 LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
 IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
 IN THE STATE OF OREGON

- NOTES**
- G1 SAWCUT (1-1/2" DEPTH) - CONFIRM LOCATION WITH INSPECTOR
 - G2 SAWCUT DIAMOND AROUND EXISTING MANHOLE FRAME AND REMOVE ASPHALT, ADD CONCRETE GRADE RING AND REINSTALL EXISTING MANHOLE FRAME AND INSTALL LEVEL 3, ACP MATCHING EXISTING ASPHALT THICKNESS TO SURFACE OF GRINDED ASPHALT PRIOR TO OVERLAY.
 - G3 EXISTING SURVEY MONUMENT TO REMAIN - DO NOT DISTURB - ANY SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RE-ESTABLISHED AT THE CONTRACTOR'S EXPENSE
 - G4 SAWCUT DIAMOND AROUND EXISTING MANHOLE FRAME AND REMOVE ASPHALT, ADJUST AND REINSTALL EXISTING MANHOLE FRAME AND INSTALL LEVEL 3, ACP MATCHING EXISTING ASPHALT THICKNESS TO SURFACE OF GRINDED ASPHALT PRIOR TO OVERLAY.
 - G5 INSTALL BLUE REFLECTIVE MARKERS ON CENTERLINE AT FIRE HYDRANTS

- EROSION CONTROL NOTES**
- E1 INSTALL SILT SACK IN CATCH BASIN.
 - E2 INSTALL BIOBAGS IN FRONT OF CURB OPENING.
- PAVING NOTES**
- P1 AREA OF 1-1/2" ASPHALT GRIND WITH OVERLAY
 - P2 APPROXIMATE LOCATION OF FULL DEPTH PAVEMENT STRUCTURE RECONSTRUCTION - CONFIRM LIMITS WITH INSPECTOR.
 - P3 BLEND CROWN DOWN TO MATCH EXISTING PAVEMENT IN 20 FEET.



TYPICAL GRIND AND OVERLAY STREET SECTION
NOT TO SCALE

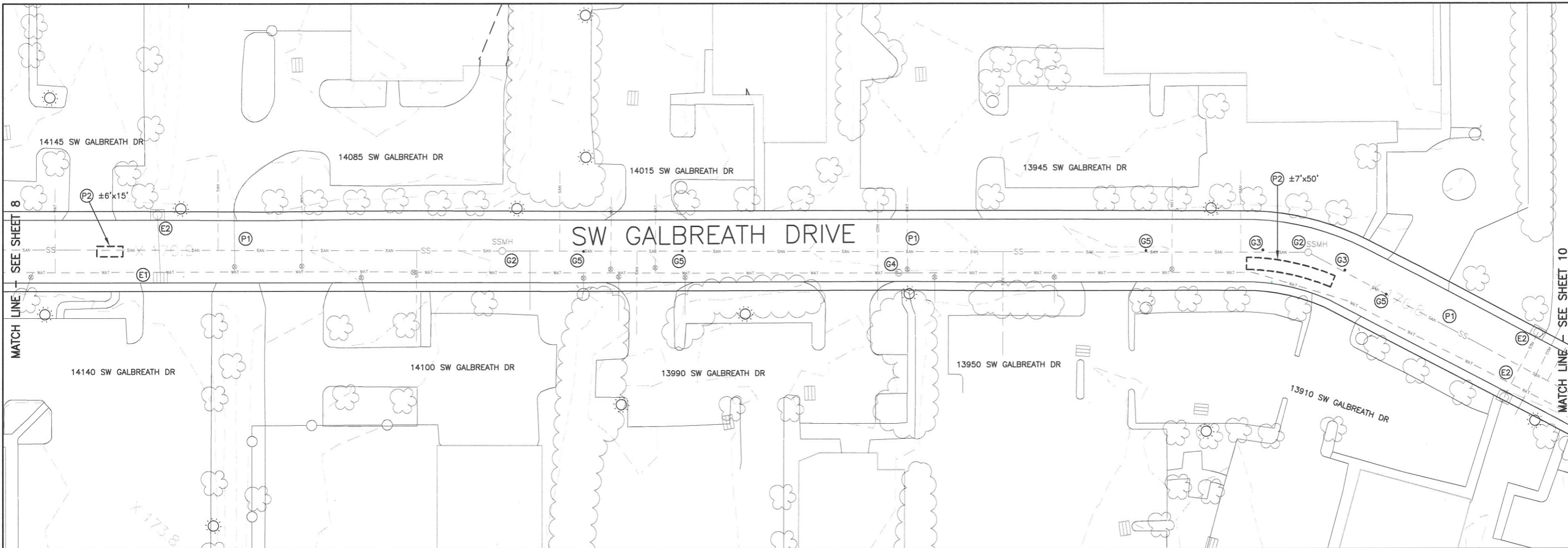


TYPICAL STREET RECONSTRUCTION SECTION
NOT TO SCALE

CITY OF SHERWOOD
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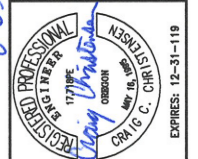
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CHECKED BY:	RS/CS
FULL SIZE SCALE:	1"=30'
DATE:	AUGUST, 2018
FILE NAME:	GALBREATH_SHEETS.DWG
EXP. DATE:	12-31-19
REVISIONS	
JOB NO.	
SHEET NO.	8
OF	13



SW GALBREATH DRIVE
(MIDDLE PLAN VIEW)

2018 PAVEMENT OVERLAY PROJECTS
LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

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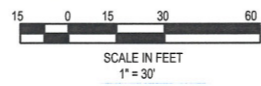


8-23-18

- NOTES**
- G1 SAWCUT (1-1/2" DEPTH) - CONFIRM LOCATION WITH INSPECTOR
 - G2 SAWCUT DIAMOND AROUND EXISTING MANHOLE FRAME AND REMOVE ASPHALT, ADD CONCRETE GRADE RING AND REINSTALL EXISTING MANHOLE FRAME AND INSTALL LEVEL 3, ACP MATCHING EXISTING ASPHALT THICKNESS TO SURFACE OF GRIDED ASPHALT PRIOR TO OVERLAY.
 - G3 EXISTING SURVEY MONUMENT TO REMAIN - DO NOT DISTURB - ANY SURVEY MONUMENTS DISTURBED BY THE CONTRACTOR SHALL BE RE-ESTABLISHED AT THE CONTRACTOR'S EXPENSE
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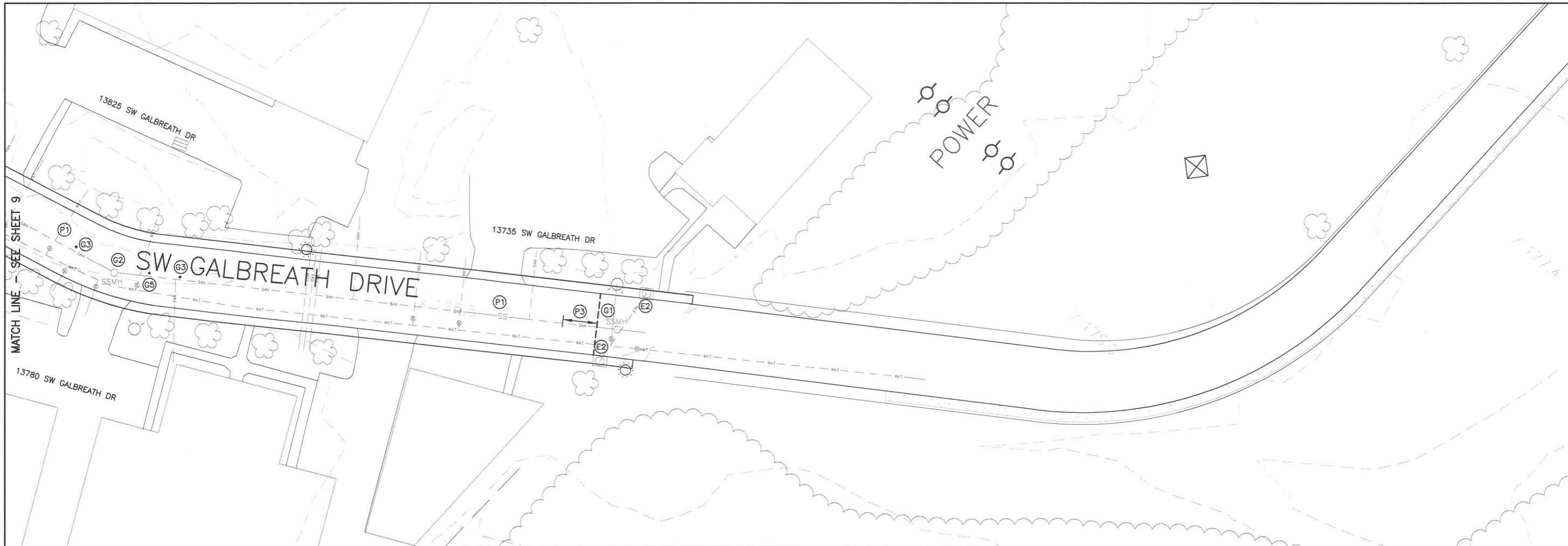
- PAVING NOTES**
- P1 AREA OF 1-1/2" ASPHALT GRIND WITH OVERLAY
 - P2 APPROXIMATE LOCATION OF FULL DEPTH PAVEMENT STRUCTURE RECONSTRUCTION - CONFIRM LIMITS WITH INSPECTOR.
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FULL SIZE SCALE:	1"=30'
DATE:	AUGUST, 2018
GALBREATH_SHEETS.DWG	

JOB NO.	
SHEET NO.	9
	13

REVISIONS

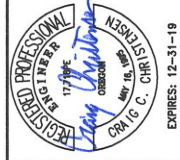


SW GALBREATH DRIVE
(EAST PLAN VIEW)

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W.M.
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON.

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
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NOTES

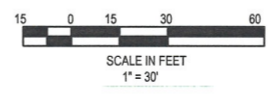
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- G2 SAWCUT DIAMOND AROUND EXISTING MANHOLE FRAME AND REMOVE ASPHALT, ADD CONCRETE GRADE RING AND REINSTALL EXISTING MANHOLE FRAME AND INSTALL LEVEL 3, ACP MATCHING EXISTING ASPHALT THICKNESS TO SURFACE OF GRIEDED ASPHALT PRIOR TO OVERLAY.
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PAVING NOTES

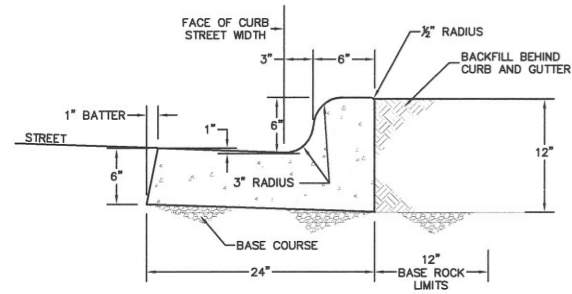
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8-23-18

DESIGNED BY:	CCC
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DATE:	AUGUST, 2018
GALBREATH_SHEETS	

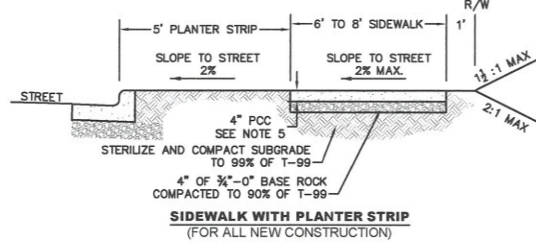
NO.	REVISIONS



NOTES:

- MONOLITHIC CURB AND GUTTER SHALL BE USED ON ALL NEW ROADWAY SECTIONS, EXCEPT AT ROADWAY MEDIANS AND AT MOUNTABLE CURB SECTIONS (SEE STD DET RD-21 & RD-24 FOR THESE CONDITIONS).
- CONCRETE SHALL BE COMMERCIAL MIX, WITH A 28-DAY COMPRESSIVE STRENGTH OF 3300 PSI, WITH A SLUMP RANGE OF 1 1/2" TO 3" MAX.
- EXPANSION JOINTS TO BE PROVIDED AT EACH:
 - POINT OF TANGENCY.
 - COLD JOINT.
 - SIDE OF INLET STRUCTURES.
 - SIDE OF DRIVEWAYS.
- EXPANSION JOINT MATERIAL SHALL BE PRE-MOLDED, ASPHALT IMPREGNATED, NON-EXTRUDING, WITH A THICKNESS OF 1/2".
- CONTRACTION JOINTS SHALL HAVE:
 - SPACING OF NOT MORE THAN 15 FEET.
 - DEPTH OF JOINT OF AT LEAST 1 1/2".
- BASE ROCK SHALL BE 3/4"-0" COMPACTED TO 95% OF MAXIMUM DENSITY PER AASHTO T-180. BASE ROCK SHALL BE TO SUBGRADE OF STREET STRUCTURES OR 4", WHICHEVER IS GREATER, AND SHALL EXTEND 12" BEHIND CURB.
- FOR CURB AND GUTTER REQUIREMENTS ON SHED AND SUPERELEVATED ROAD SECTIONS, SEE STD DET RD-23.

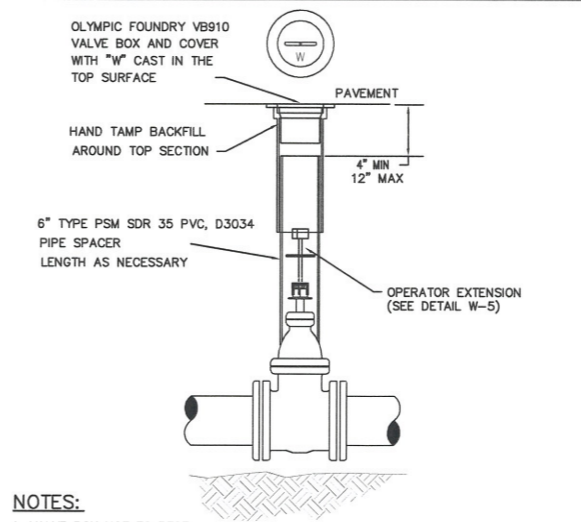
STANDARD DRAWING TITLE	DRAWING NUMBER
MONOLITHIC CURB AND GUTTER	RD-22
SCALE	DATE
N.T.S.	MAR '16



NOTES:

- CONCRETE SHALL BE COMMERCIAL MIX, MIN. COMPRESSIVE STRENGTH OF 3,300 PSI @ 28 DAYS, WITH A 4" MAX SLUMP.
- SIDEWALK PANELS TO BE SQUARE (6' LONG x 6' WIDE TYP.).
- EXPANSION JOINTS TO BE PLACED AT SIDES OF DRIVEWAY APPROACHES, UTILITY VAULTS, CURB RAMPS, AND/OR POINTS OF TANGENCY IN CURBS AS SHOWN ON THE STANDARD DRAWINGS FOR SIDEWALK RAMPS, AND AT SPACING NOT TO EXCEED 45'.
- FOR SIDEWALKS ADJACENT TO THE CURB AND POURED AT THE SAME TIME AS THE CURB, THE JOINT BETWEEN THEM SHALL BE A TROVELED JOINT WITH A MINIMUM 1/2" RADIUS.
- SIDEWALKS SHALL HAVE A MINIMUM THICKNESS OF 4". IF MOUNTABLE CURB IS USED, OR IF SIDEWALK IS INTENDED AS PORTION OF A RESIDENTIAL DRIVEWAY IT SHALL HAVE A 6" MINIMUM THICKNESS, COMMERCIAL 8".
- CONCRETE SHALL HAVE A BROOM FINISH, ALL JOINTS SHALL BE EDGED WITH 3" SHINE.
- WIDTH OF PLANTER STRIP AND SIDEWALK IS MEASURED FROM FACE OF CURB.
- IF DRAIN BLOCKOUTS IN CURBS ARE APPROVED, THEY SHALL BE EXTENDED PERPENDICULAR TO CURB TO 1" PAST BACK OF SIDEWALK WITH A 3" DIAMETER ADS PIPE. CONTRACTION JOINT SHALL BE PLACED OVER PIPE.

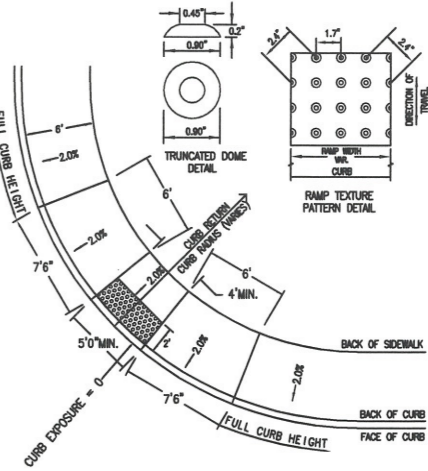
STANDARD DRAWING TITLE	DRAWING NUMBER
SIDEWALK DETAIL	RD-26
SCALE	DATE
N.T.S.	MAR '16



NOTES:

- VALVE BOX NOT TO REST ON OPERATING ASSEMBLY
- OPERATOR EXTENSION REQUIRED WHEN VALVE NUT IS DEEPER THAN 3 FEET FROM FINISHED GRADE
- CENTER VALVE BOX ON AXIS OF OPER NUT
- PROVIDE 24" SQUARE BY 4" THICK CONCRETE PAD AROUND VALVE BOX OUTSIDE OF PAVED AREAS
- VALVE BOX COVER SHALL BE A MINIMUM OF 2 1/2" IN LENGTH

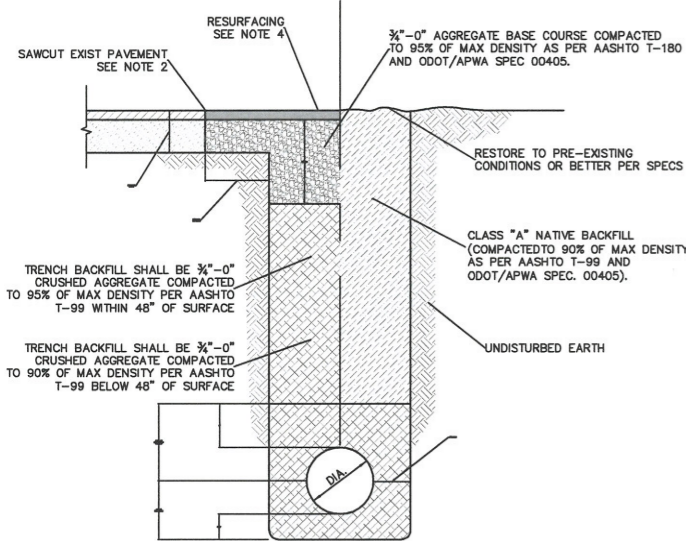
STANDARD DRAWING TITLE	DRAWING NUMBER
TYPICAL GATE VALVE SETTING DETAIL	W-3
SCALE	DATE
N.T.S.	JUL '09



NOTES:

- PROJECT ENGINEER SHALL USE THIS DRAWING AS A GUIDE FOR DESIGNING RAMPS AND SHALL PREPARE A SITE SPECIFIC DRAWING FOR EACH RAMP.
- PROJECT ENGINEER SHALL VERIFY APPLICABILITY OF THIS DRAWING TO SPECIFIC LOCATIONS WITHIN THE PROJECT BEFORE USING IT AS A DESIGN GUIDE AND SHALL LOCATE EACH RAMP RELATIVE TO CROSSWALK OR STOP LINE.
- SIDEWALK RAMP GRADES SHALL MEET CURRENT ADA STANDARDS.
- PLANTER STRIP DETAIL - REFER TO ODOT STANDARD DRAWING RD 755 (SIDEWALK RAMP DETAIL).
- DETECTABLE WARNING SHALL BE TRUNCATED DOME TYPE, 24" LONG IN DIRECTION OF TRAVEL AND FULL WIDTH OF RAMP, WITH DOMES ALIGNED ON A SQUARE GRID WITH ITS GRIDLINES PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE RAMP. COLOR OF DETECTABLE WARNING SHALL BE SAFETY YELLOW.
- CURB INLET OR CATCH BASIN SHALL NOT BE ALLOWED IN FRONT OF RAMP.
- CONCRETE SHALL BE A COMMERCIAL MIX, WITH A 28 DAY COMPRESSIVE STRENGTH OF 3300 PSI WITH A 4" MAX SLUMP.
- 3/4"-0" BASE ROCK SHALL BE COMPACTED TO 90% OF MAXIMUM DENSITY.
- BEVEL THE CURB CUT FROM GUTTER TO BACK OF CURB AT 8.33% (1:12) MAX.
- SCORE CONCRETE AT GRADE CHANGES, SURFACE TEXTURE CHANGES AND AT ALL OTHER POINTS SHOWN.
- CONCRETE SURFACE SHALL HAVE BROOM FINISH, AND EDGE ALL JOINTS.
- PROJECT ENGINEER SHALL ACCEPT FULL RESPONSIBILITY FOR CORRECTING ALL UNACCEPTABLE RAMP CONSTRUCTION RESULTING FROM APPLYING THIS DRAWING "AS IS" AND NOT PROVIDING A SITE-SPECIFIC DRAWING FOR EACH RAMP.

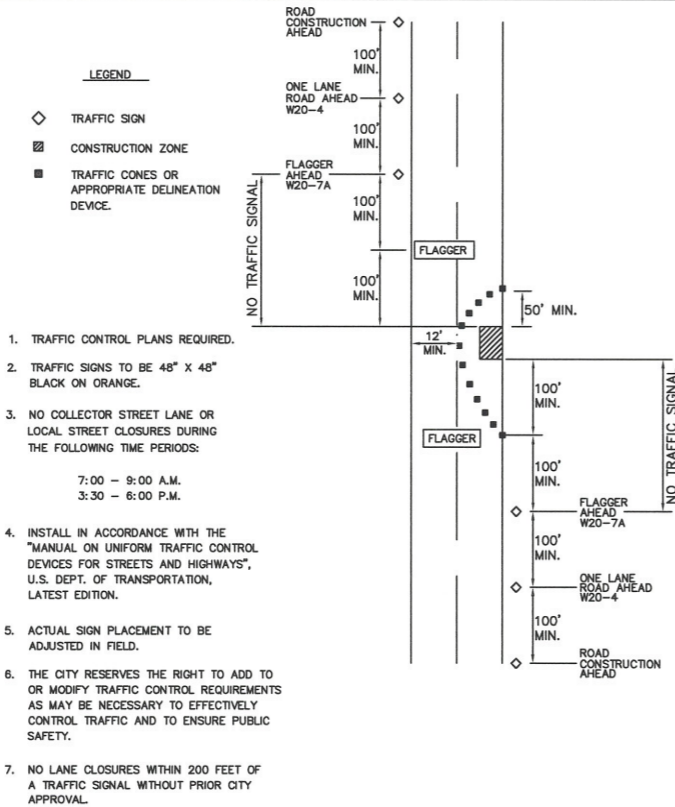
STANDARD DRAWING TITLE	DRAWING NUMBER
CURB-TIGHT CURB RETURN & RAMP DETAIL	RD-43
SCALE	DATE
N.T.S.	MAR '16



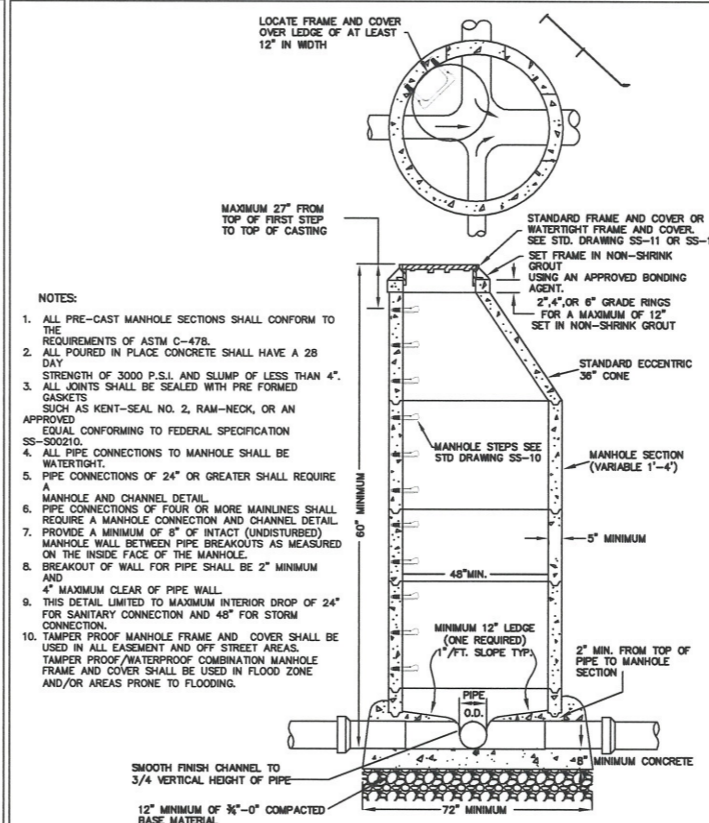
NOTES:

- THESE TRENCH BACKFILL REQUIREMENTS APPLY TO ALL PUBLIC UTILITY PIPES. FOR ADDITIONAL REQUIREMENTS, SEE CITY STANDARD DESIGN MANUAL SECTION 210.19.
- SAWCUT EXISTING HMAc PAVEMENT FULL DEPTH. SAWCUT EXISTING PCC PAVEMENT ACCORDING TO CITY STANDARD DETAILS.
- 12" FOR TRENCHES WIDER THAN 12". 6" FOR TRENCHES LESS THAN 12".
- MATCH EXISTING PAVEMENT MATERIAL(S). THICKNESS SHALL BE AS FOLLOWS:
 - FOR EXISTING HMAc: RESURFACE TO A MINIMUM OF 3" OF LEVEL 2, 1/2" DENSE HMAc OR EXISTING AC THICKNESS PLUS 2", WHICHEVER IS GREATER, BUT DO NOT EXCEED 6". COMPACT AC IN 2" MAX LIFTS TO 92% OF MAXIMUM DENSITY (RICE).
 - FOR EXISTING PCC: EXISTING PAVEMENT THICKNESS PLUS 2", BUT NOT LESS THAN 6". ON ARTERIAL AND COLLECTOR STREETS, CONCRETE PATCHING MATERIAL SHALL BE HIGH EARLY STRENGTH CLASS 5000 PSI PCC APPROVED BY CITY ENGINEER.
- ALL CUT EDGES OF AC SHALL BE SAND SEALED WITH CRS-1 OR CRS-2 EMULSIFIED ASPHALT OR EQUIVALENT.

STANDARD DRAWING TITLE	DRAWING NUMBER
PIPE TRENCH BACKFILL	RD-47
SCALE	DATE
N.T.S.	MAR '16



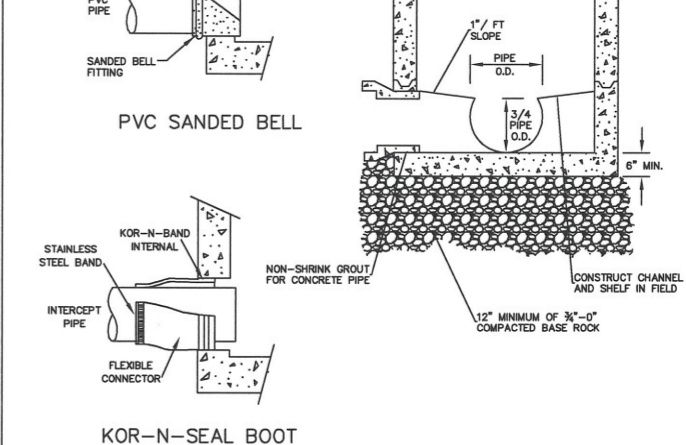
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TYPICAL TRAFFIC CONTROL PLAN	RD-70
SCALE	DATE
N.T.S.	MAR '16



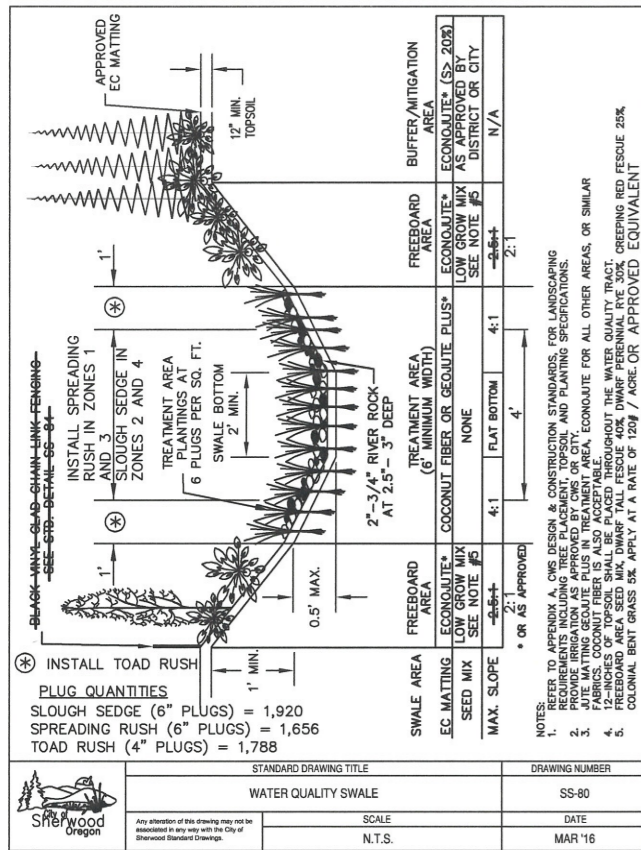
STANDARD DRAWING TITLE	DRAWING NUMBER
STANDARD MANHOLE	SS-1
SCALE	DATE
N.T.S.	MAR '16

NOTES:

- ALL PRE-CAST MANHOLE SECTIONS SHALL CONFORM TO THE REQUIREMENTS OF ASTM C-478.
- ALL JOINTS SHALL BE SEALED WITH PRE-FORMED GASKETS SUCH AS KENT-SEAL NO. 2, RAM-NECK, OR AN APPROVED EQUAL CONFORMING TO FEDERAL SPECIFICATION SS-900210.
- ALL PIPE CONNECTIONS TO MANHOLE SHALL BE WATERTIGHT.
- PIPE CONNECTIONS OF 24" OR GREATER SHALL REQUIRE A MANHOLE AND CHANNEL DETAIL.
- PIPE CONNECTIONS OF FOUR OR MORE MAINLINES SHALL REQUIRE A MANHOLE CONNECTION AND CHANNEL DETAIL.
- PROVIDE A MINIMUM OF 8" OF INTACT (UNDISTURBED) MANHOLE WALL BETWEEN PIPE BREAKOUTS AS MEASURED ON THE INSIDE FACE OF THE MANHOLE.
- BREAKOUT OF WALL FOR PIPE SHALL BE 2" MINIMUM AND 4" MAXIMUM CLEAR OF PIPE WALL.
- THIS DETAIL LIMITED TO MAXIMUM INTERIOR DROP OF 24" FOR SANITARY CONNECTION AND 48" FOR STORM CONNECTION.
- TAMPER PROOF MANHOLE FRAME AND COVER SHALL BE USED IN ALL EASEMENT AND OFF STREET AREAS. TAMPER PROOF/WATERPROOF COMBINATION MANHOLE FRAME AND COVER SHALL BE USED IN FLOOD ZONE AND/OR AREAS PRONE TO FLOODING.



STANDARD DRAWING TITLE	DRAWING NUMBER
PRECAST CONCRETE MANHOLE BASE	SS-2
SCALE	DATE
N.T.S.	MAR '16



CONSTRUCTION

- Water Quality Swale shall be over-excavated and filled to final grade with 12-inch amended topsoil. Topsoil amendments shall be garden compost, not conventional fertilizer amendments.
- A biodegradable Erosion Control Matting shall be placed over the topsoil throughout the swale cross section, fabric shall be held in place in accordance with the manufacturer's installation requirements. Anchor spacing shall be based on 3 fps flow over the fabric.
 - Treatment area - high-density jute matting (Geojute Plus or other approved equal)
 - All other areas - low-density jute matting (Ecojute or other approved equal)
- 2.5-3 inches of 2"-3" river run rock shall be placed over the matting evenly throughout the length and width of the swale.
- Plant materials shall be placed in accordance with the plan and plant table as shown on approved plans.
- The water quality swale treatment area plantings can be deemed "substantially complete" once active green growth has occurred to an average growth of 3" and plant density is an average of approx. 6 plants (minimum 1-inch plugs or equivalent) per square foot.
- The facility shall be deemed acceptable to begin the maintenance period when plant growth and density matches the engineer's design as shown on the approved plans and all other requirements have been met. The engineer must certify the facility to be functional, in accordance with the approved plan design to begin the two-year maintenance period.

MAINTENANCE

- The permittee is responsible for the maintenance of this facility for a minimum of two years following construction and acceptance of this facility per Chapter 2.
- Irrigation is to be provided per separate irrigation plan as approved. Irrigation needs are to be met using a temporary irrigation system with a timer during the dry season. Systems should be winterized during the wet season to assure longevity and guard against damage from freezing temperatures. Water source shall be as shown on the approved plans.
- Engineer or Owners Representative is to visit and evaluate the site a minimum of twice annually (Spring and Fall). The landscaping shall be evaluated and replanted as necessary to ensure a minimum of 80% survival rate of the required plantings and 90% aerial coverage of herbaceous plugs. Non-native, invasive plant species shall be removed when occupying more than 20% of the site.
- The facility shall be re-excavated and planted if siltation greater than 3 inches in depth occurs within the two-year maintenance period.

RIPRAP:

- ROCK FOR RIPRAP SHALL BE ANGULAR IN SHAPE.
- THICKNESS OF A SINGLE ROCK SHALL NOT BE LESS THAN ONE-THIRD ITS LENGTH.
- ROUNDED ROCK WILL NOT BE ACCEPTED UNLESS APPROVED BY THE CITY OR DISTRICT.

RIPRAP INSTALLATION:

- EXCAVATE BELOW FINISH GRADE TO DEPTH & DIMENSIONS SHOWN ON APPROVED PLANS.
- INSTALL WOVEN GEOTEXTILE FABRIC.
- PLACE RIP RAP TO FINISH GRADE.

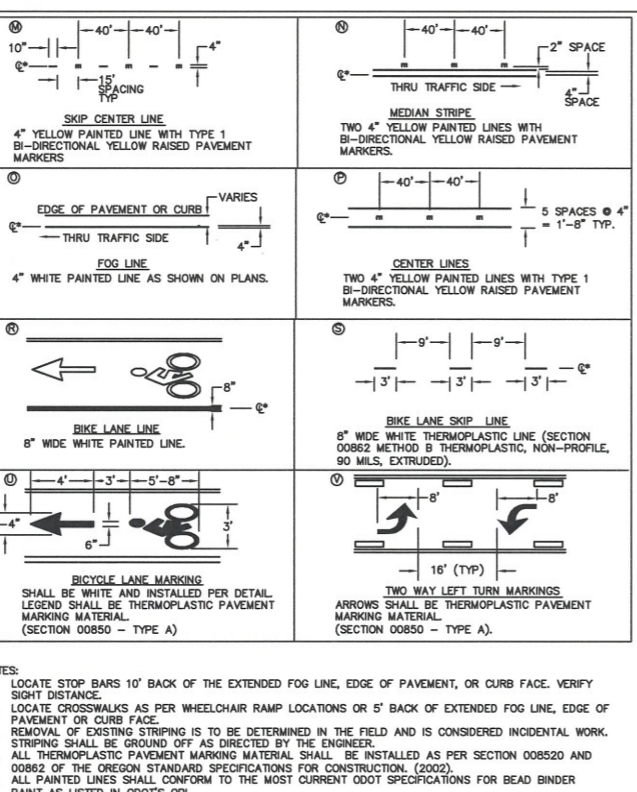
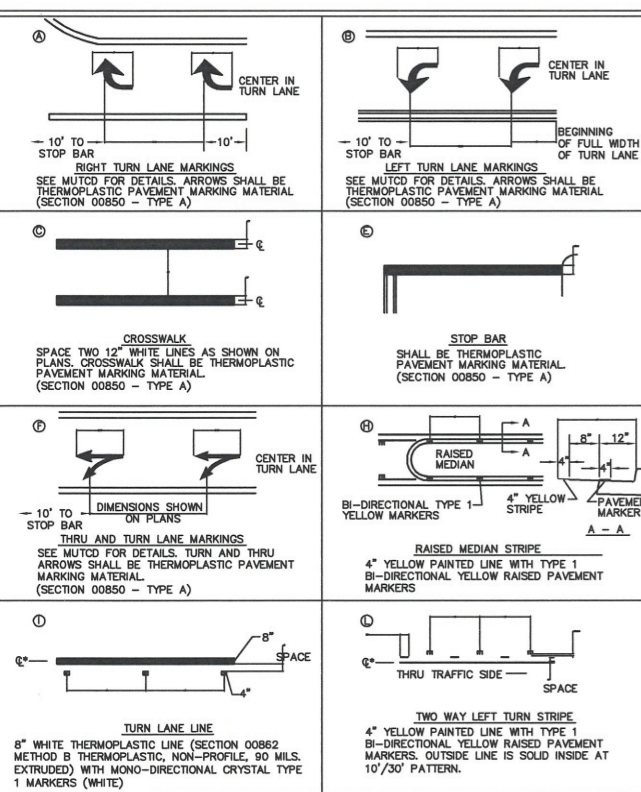
GRADE RIPRAP SHALL BE THE CLASS AND SIZE OF ROCK ACCORDING TO THE FOLLOWING:

CLASS	100	200	700	2000	PERCENT (BY WEIGHT)
50-30	100-60	200-140	700-500	2000-1400	20
30-15	60-25	140-80	500-200	1400-700	30
15-2	25-2	80-8	200-20	700-40	40
2-0	2-0	8-0	20-0	40-0	10

STANDARD DRAWING TITLE	DRAWING NUMBER
WATER QUALITY SWALE	SS-80
SCALE	DATE
N.T.S.	MAR '16

STANDARD DRAWING TITLE	DRAWING NUMBER
WATER QUALITY SWALE CONSTRUCTION & MAINTENANCE NOTES	SS-81
SCALE	DATE
N.T.S.	MAR '16

STANDARD DRAWING TITLE	DRAWING NUMBER
RIP RAP DETAILS	SS-87
SCALE	DATE
N.T.S.	MAR '16



STANDARD DRAWING TITLE	DRAWING NUMBER
STRIPING DETAILS	S-3
SCALE	DATE
N.T.S.	MAR '16

STANDARD DRAWING TITLE	DRAWING NUMBER
STRIPING DETAILS II	S-4
SCALE	DATE
N.T.S.	JUL '09



SAMPLE CURB OPENING
CURB OPENING TO BE MADE OF GALVANIZED STEEL WITH 18" OPENING AND 2" DEEP TOP PLATE COVER. GET SUBMITTAL TO INSPECTOR FOR APPROVAL PRIOR TO PURCHASE.

CITY OF SHERWOOD DETAILS

2018 PAVEMENT OVERLAY PROJECTS

LOCATED IN SECTIONS 28B AND 31D, T2S, R1W, W4M,
IN THE CITY OF SHERWOOD, WASHINGTON COUNTY,
STATE OF OREGON

CITY OF SHERWOOD
ENGINEERING DEPARTMENT
22560 SW PINE STREET
SHERWOOD, OREGON 97140
PHONE: (503) 925-2309
E-MAIL: engineering@sherwoodoregon.gov

PROFESSIONAL ENGINEER
JAY CHRISTENSEN
No. 10000
OREGON
EXPIRES: 12-31-19

DESIGNED BY:	CCC
DRAWN BY:	CCC/TB
CHECKED BY:	RS/VS
FULL SIZE SCALE:	AS NOTED
DATE:	APRIL 7, 2015

SHEET 13 OF 13